

**AGENDA**  
**HAILEY PLANNING & ZONING COMMISSION**  
**Monday, September 12, 2016**  
**Hailey City Hall**  
**5:30 p.m.**

**Call to Order**

**Public Comment** for items not on the agenda

**Consent Agenda**

[CA 1](#) Motion to approve minutes of August 22, 2016

[CA 2](#) Motion to approve Findings of Fact and Conclusions of Law for an application submitted by D.L. Evans Bank, represented by Tom Lennon and Andy Erstad of Erstad Architects, for a new 4,595 square foot bank branch to be located at 609 South Main Street (Lots 1-8, Block 5 Hailey Townsite) and 611 South Main Street in the Business (B) and Townsite Overlay (TO) Zoning Districts.

[CA 3](#) Motion to approve the Findings of Fact and Conclusions of Law for a Conditional Use Permit Application from UPS for a temporary structure to be located at 111 South River Street (Lots 1-7 of Block 31, Hailey Townsite) within the Business (B) and Townsite Overlay (TO) Zoning Districts.

**New Business and Public Hearings**

[NB 1](#) Consideration of a Request for Reconsideration pursuant to Hailey Municipal Code 17.03.050(D) by Wise Guy Pizza, located at 411 Main Street (S. ½ of Lot 3, Lots, 4, 5, Block 56 Hailey Townsite). The applicant is requesting reconsideration of a condition imposed by the Commission. That condition states that a City street tree located in the City right-of-way should remain.

[NB 2](#) Consideration of the Design Review Application submitted by Benny Rod Shop, represented by Susan Scovell, for a new 3,685 square foot addition to an existing 4,352 square foot building located at 1131 Airport Way (Lot 2A, Block 1, Friedman Park Subdivision) in the Light Industrial (LI) Zoning District.

[NB 3](#) Consideration of an application for a Subdivision of Lot 2, Block 4, Old Cutters, by creating seven (7) sublots, to be known as Sonitalena Cottages. Sublot 1 consists of 0.06 acre, Sublot 2 consists of 0.04 acre, Sublot 3 consists of 0.04 acre, Sublot 4 consists of 0.03 acre, Sublot 5 consists of 0.04 acre, Sublot 6 consists of 0.04 acre and Sublot 7 consists of 0.07 acre. The total development comprises of 0.89 acre. Current zoning is General Residential (GR). Design review was approved for this project on July 11, 2016.

[NB 4](#) Continuation of a Planned Unit Development (PUD) and Subdivision Preliminary Plat, the Quigley Farm PUD. This is an application to Blaine County by Quigley Farm & Conservation Community, LLC, represented by Hennessy Company and SERA Architects for a 51-unit mixed use project consisting of:

- 7 Large Lot Dwellings
- 9 Medium Lot Dwellings
- 10 Cottage Unit Dwellings (4 of which are identified for community housing)
- 6 Town House Sublot Dwellings (2 of which are identified for community housing)
- 9 Live/Work Mixed Use Dwellings
- 10 Multi-Family Dwellings (2 of which are identified for community housing)
- Future hospitality site of 1.38 acres
- Future Sage School site of 6.17 acres
- Open Space and common area
- Bike/pedestrian pathways and trails
- Working Agriculture

## Old Business

### Commission Reports and Discussion

### Staff Reports and Discussion

- SR 1** Discussion of current building activity, upcoming projects, and zoning code changes.  
(no documents)
- **Design Review Exemption:** On August 18, 2016, a Design Review Exemption Application was submitted by Thomas and Ann Helms, represented by Errin Bliss of Bliss Architecture, for a 96 square foot addition, to be located at 213 North 4<sup>th</sup> Avenue (Hailey Townsite, Lots 6, 7, 8, Block 48), within the Limited Residential 1 (LR-1) and Townsite Overlay (TO) Zoning Districts. The Chair and Administrator, having been presented with all information and testimony in favor and in opposition to the proposal, hereby determine that the project is minor, will not conflict with the design review standards, will not adversely impact adjacent properties, will not alter any structural or architectural elements of the building, and is not an addition of floor area equal to or greater than 50% of the original structure.
- SR 2** Discussion of the next Planning and Zoning meeting: Monday, September 26, 2016.  
(no documents)

### Adjourn

**Return to Agenda**

**MINUTES OF THE MEETING OF THE  
HAILEY PLANNING & ZONING COMMISSION**

**Monday, August 22, 2016**

**Meeting convened at 609 and 611 South Main Street at 5:30pm  
Meeting reconvened at Hailey City Hall at approximately 6:30pm**

**Present: Richard Pogue, Janet Fugate, Owen Scanlon, Jeff Engelhardt, Dan Smith**

**Staff: Lisa Horowitz, Robyn Davis**

***Call to Order***

At approximately 5:30pm, Chair Fugate called the meeting to order. Meeting convened at 609 and 611 South Main Street, the proposed site for D.L. Evans Bank.

**5:32:00 PM** Lisa Horowitz opened the meeting and discussed the conclusions reached among staff and the Tree Committee regarding City street trees. Horowitz noted this unique opportunity to plan for an entire City block; the species of the trees, the need to increase the width of sidewalk, and the grade change which affects over half of the trees. For this combination of factors, staff and Tree Commissioners support the idea of having all of the street trees on the block replaced, with the exception of the specimen oak tree on the corner. Horowitz also noted that the Design Team would speak to the grade change, dumpster location, light and banner pole, trees – both private and City street trees, and parking.

**5:39:00 PM** Dan Smith questioned whether or not elevation markers were located on site to show the difference in grade. Jessica Aguilar stated that no grade stakes were put in place. Jessica Aguilar noted that the stakes currently in place mark the edge the proposed building.

**5:42:00 PM** Lisa Horowitz discussed a few items that still need to be addressed with the landscape architect: will the sidewalk get pulled back to meet the ½ to 2/3 of the drip line requirement for the specimen oak, or do we not disturb the grade of the site to consider the health of longevity of the trees. Horowitz also noted that the light pole, on the corner of Main and Chestnut, will be sawed off at grade, as not to disturb the roots of the tree, and replaced 1 block to the south.

Jeff Engelhardt inquired about whether or not the revised plans had the tree well scaled out on site plan. Engelhardt also inquired about the dimensions of the well. Jessica Aguilar noted that the initial plans called for a five foot tree grate; however, D.L. Evans has plans to modify the size of the tree grate to accommodate the tree as suggested in the staff report. Jeff Engelhardt inquired about whether or not the well could be large enough for the grate to come up to the tree, to be able to utilize that portion of the sidewalk. Terry King noted that the grates are modular and have varying sized rings, which can be removed and enlarged to accommodate for the growth of the tree.

Owen Scanlon inquired about the light pole and whether or not it would be replaced and the location of the new one. Horowitz noted that staff suggested that the light and banner pole be relocated to the south, on the corner of Main and Maple.

Chair Fugate requested that Lisa Horowitz speak to the current proposal of the street trees, as well as the recommendation of the Tree Committee regarding said trees. Horowitz noted that the entire block

of City street trees, nine (9) trees are recommended for replacement due to the factors mentioned; however, nine (9) new street trees will be added in a similar location with similar spacing, but with the required 10' sidewalk, tree grates and all at the grade of the new building. Horowitz confirmed that the trees will be replanted at grade with the sidewalk, and will be approx. 30 feet on center. Horowitz also noted that the Tree Committee suggested that the street trees be planted toward the back of the sidewalk to reduce bus conflicts.

**5:49:00 PM** Jessica Aguilar showed the Commissioners where the bus stop location would be. Aguilar noted that the structure would be built behind the property line, not to be located on the City sidewalk. Aguilar also noted that a solar light kit will be added to the structure. Horowitz noted two items that staff suggested: a) a trash receptacle to be located near bus stop and b) pedestrian access from bus stop to bank building. Aguilar also noted that staff recommended that bike racks be added. Aguilar confirmed that D.L. Evans would be placing bike racks on site.

Chair Fugate inquired about the location of the trash receptacle. Horowitz noted that she would work with Applicant to determine a good location.

Jason Miller from Mountain Rides confirmed that all comments from Applicant are accurate and Mountain Rides is very excited to add bus stop to location. Miller would also like to see a larger red curb, as well as no parallel parking from the bus stop to the corner.

Richard Pogue inquired about the property line and whether or not the street trees would be planted in a similar location as the current street trees. Aguilar noted that the new street trees would be pushed back to the west, as the Applicant will be increasing the sidewalk from five (5) feet to 10 feet.

Chair Fugate inquired about the metal railing along the sidewalk. Aguilar confirmed that metal railing would be removed and replaced with landscaping.

Russell Moreno inquired about the street crossing and whether or not a flashing light would put in place. Horowitz noted that the flashing light is in the City's Capital Improvement Plan.

**5:56:00 PM** Horowitz inquired about the whereabouts of the landscape beds. King pointed out where each would be located. Terry King also noted that a landscape strip would be added to separate the two properties at the sound end of the lot.

**5:59:00 PM** Terry King pointed out locations for the drive thru, trash enclosure and parking area. Chair Fugate questioned the access of the drive thru. King noted that the drive thru would only be accessed one way – traffic will enter off of Chestnut and exit through the alley.

Russell Moreno shared his concerns regarding the dumpster being located across the street from a residential area. Moreno suggested moving the dumpster location. Jessica Aguilar noted that the dumpster could not be relocated near the bus stop, as Clear Creek Disposal would not be able to service it at said location. Aguilar noted the D.L. Evans would be willing to relocate or consider another dumpster location. Chair Fugate clarified that the dumpsters are residential cans, not commercial dumpster bins. Moreno noted that his concern is the location of the dumpster, not the size of the can. Commissioners agreed to revisit matter when meeting reconvened at City Hall.

**6:08:00 PM** Horowitz presented the Tree Committee's concerns for the trees on private property: the

mature crab apple tree and the maple were noted. The Tree Committee recommended retaining both trees, if possible. Aguilar noted that the grade of the lot is not adequate and the interior trees present a security issue. Aguilar also noted that removing the interior trees would allow for a clear view of the parking lot. Aguilar noted that eventually a future expansion would take place and both trees hinder expansion. Aguilar noted that benches would be built in the open space and D.L. Evans would like to utilize large open area as a gathering place.

Jay Cone inquired about the Tree Committee's suggestions and whether or not they are with authority or with recommendation. Horowitz clarified that suggestions are only recommendations when pertaining to private trees. Dan Smith inquired about caliper of the trees. Horowitz noted that staff recommended a caliper of 2 ½ to 3 inch.

Chair Fugate inquired about the expansion of the building and the visibility of the lot once the expansion was built. Aguilar stated that they are working to accommodate City's requirements and may or may not include more interior trees, mostly for security purposes. King added that D.L. Evans proposed the Columnar Oak for this purpose, as their spread isn't as wide and they are taller in height, which would help keep the lot open and more visible.

Chair Fugate adjourned site visit. Meeting reconvened at Hailey City Hall.

#### **Call to Order**

[6:25:02 PM](#) Chair Fugate called the meeting to order. Meeting reconvened at Hailey City Hall.

#### **Public Comment**

None

#### **Consent Agenda**

No public comments.

#### **CA 1** Motion to approve minutes of August 1, 2016

[6:25:14 PM](#) Dan Smith motioned to approve all Consent Agenda items. Richard Pogue seconded and all were in favor.

#### **New Business and Public Hearings**

#### **NB 1** *Continuation of the Design Review Application by D.L. Evans Bank, represented by Tom Lennon and Andy Erstad of Erstad Architects, for a new 4,595 square foot bank branch to be located at 609 South Main Street (Lots 1-8, Block 5 Hailey Townsite) and 611 South Main Street in the Business (B) and Townsite Overlay (TO) Zoning Districts.*

[6:26:56 PM](#) Lisa Horowitz presented the application by D.L. Evans Bank and noted only one issue that was not covered during the site visit. Horowitz noted the issue as being whether or not the code calls for alley loaded parking in the Business District or whether or not the code calls for screening of parking areas adjacent to residential zones in the Business District. Horowitz informed the Commission that per the City Attorney, alley loaded parking is still wanted in the Business District. Horowitz noted that the applicant modified the amount and location of the alley loaded parking from the previous site plan.

Horowitz stated that the applicant also modified the roof lines on the drive thru, as requested at the last Planning and Zoning Meeting. .

[6:28:21 PM](#) Chair Fugate questioned the alley parking and whom it's intended for: customers or employees? Jessica Aguilar noted that designated parking currently does not exist; however, D.L. Evans has no issues designating parking for their employees. Aguilar also noted that the parking lot will be signed to accommodate customer parking only; D.L. Evans will tow abandoned vehicles away at the owner's expense.

[6:30:00 PM](#) Richard Pogue inquired about size of trash can. Jessica Aguilar noted that the trash can is smaller and plastic, not large and metal. Per Aguilar, D.L. Evans does not generate a large amount of trash. Aguilar noted that one or two recycle bins may be utilized; however, most documents are shredded. Aguilar noted that a separate company will visit once a month to empty confidential materials. Owen Scanlon inquired about the look of truck, as dumpsters on site plan are facing an angle, meaning a garbage truck will back up to the dumpsters. Aguilar was not aware of other plans or information regarding dumpsters, as Design Team developed the plan and could not attend meeting.

[6:32:22 PM](#) Chair Fugate questioned whether or not D.L. Evans could see requiring a large dumpster at any point. Jessica Aguilar didn't believe so. Richard Pogue inquired about how materials would be recycled. Aguilar isn't sure what the recycling plan is and noted that they will accommodate local services requests or needs.

[6:34:17 PM](#) Aguilar noted that the recommended conditions of approval by staff were reviewed and D.L. Evans is in agreement with all conditions listed.

[6:35:05 PM](#) Chair Fugate opened the item for public hearing. Jon Evans, Jr., CEO of DL Evans Bank, thanked the Commissioners for their time, effort and support of project.

[6:36:00 PM](#) Steven Crosser believes that the landscaping plan looks good. Crosser also believes the Commissioners should move ahead with project, as it is a good opportunity for Hailey.

[6:37:26 PM](#) Russell Moreno expressed his concern with the position of the dumpsters, which are shown in the site plan to be placed directly across the street from the residential neighborhood, just to the west of D.L. Evans Bank. Moreno noted that he would prefer to see that the dumpsters be relocated in front of the driver teller, as it could provide more shade to the area and would be directly across the street from the residential neighborhood.

[6:38:45 PM](#) Peter Lobb likes the landscaping design; however, believes the two Spruce trees will cause uproar among residents. Lobb believes that the Applicant has every right to cut the trees down; however, would prefer to see at least one of the trees remain intact.

[6:39:56 PM](#) Steve Crosser questioned whether or not the project would go in front of City Council. Lisa Horowitz noted that the Planning and Zoning Commissioners will make final decision on project; however, Horowitz also explained that the replacement of an entire block of City street trees would go in front of City Council.

[6:40:58 PM](#) Chair Fugate closed the item for public hearing. Jessica Aguilar commented on the dumpster location, as well as any fencing on site and noted that D.L. Evans will work with staff to modify and minimize the visual impact of the dumpster area. Aguilar also noted that the plan was modified to include fencing. The proposed fencing would be slightly staggered and in varying heights. The fence would also include a tree/mountain design; an artistic panel relevant to the community of Hailey.

[6:43:01 PM](#) Lisa Horowitz informed the Commissioners that one public comment came in via email. Chair Fugate added that Gary Sleddy, author of said public comment, supports the project.

[6:43:49 PM](#) Chair Fugate believes it would be beneficial to designate alley parking to employee parking, as there would be less conflict with drive thru traffic.

[6:45:48 PM](#) Dan Smith noted that if D.L. Evans has plans to utilize roll out bins, Clear Creek usually picks those up on the street. Smith would like to see them moved to a different location, not the alley way. Chair Fugate would like to confirm with Clear Creek where the truck will pick up trash and recycling. Jeff

Engelhardt noted that his office is nearby and he rolls his bin out on Main Street. Engelhardt also noted that nearby townhouses roll their bins out to River Street.

[6:49:24 PM](#) Horowitz noted that she would follow up with Clear Creek Disposal on the location of where the trash will be collected. Horowitz also suggested moving the dumpster area closer to the corner to easily accommodate for a larger dumpster in the future, if need be. Horowitz suggested adding another Conditional of Approval that D.L. Evans will need to rebuild dumpster at time of the upgrade and switch orientation back to the alley.

[6:50:23 PM](#) Chair Fugate noted that moving the trash bins further to the corner could work as long as it could accommodate a dumpster in the future, if need be. Jessica Aguilar would like to take the opportunity to review matter with Clear Creek Disposal and Staff, and figure out the best solution. Chair Fugate questioned whether or not the Commissioners were comfortable putting this item on hold until they had all of the details. Richard Pogue added that the trash bin can be smaller and believes it should be. Aguilar noted that D.L. Evans will work with staff and adjust as need be.

[6:52:28 PM](#) Dan Smith suggested that the area be developed for a larger dumpster now and utilize a smaller bin until a larger dumpster is needed.

[6:53:28 PM](#) Chair Fugate suggested that the item be put on hold until matter is discussed with Clear Creek Disposal. Horowitz suggested that the dumpster enclosure be moved to the corner and the landscaping plan change slightly, due to power pole in area. Jessica Aguilar would like to get input from Clear Creek before finalizing plans.

[6:55:14 PM](#) Dan Smith noted that Clear Creek will likely have overhead issues with power lines, if dumpster is located on the corner.

[6:56:05 PM](#) Jeff Engelhardt doesn't believe it will make much of a difference if the dumpster is located on the corner or 70 feet away. Owen Scanlon recommended that Staff and the Applicant work with Clear Creek to determine the best location for the dumpster area. Scanlon also noted that he would like to see the radius increase, as cars will drive over curb all of the time.

[6:57:27 PM](#) Horowitz suggested the following, 'the dumpster location shall be relocated closer to the corner of Chestnut Street, with final design to be approved by Clear Creek Disposal, Staff and the Chair.' Chair Fugate added, 'pending that location be workable for Clear Creek Disposal.'

[6:58:38 PM](#) Chair Fugate noted that she would like to see Jason Miller's comments included as a condition. Chair Fugate also encouraged the Applicant to recycle and wanted to confirm that the Applicant has agreed to include pedestrian access from the bus shelter back to the parking lot. Jessica Aguilar confirmed this. Chair Fugate would like to also see the alley parking designated as employee parking.

[7:01:06 PM](#) Dan Smith questioned whether or not the Applicant has satisfied at least four of the criteria required. Chair Fugate noted that the Applicant has satisfied three: bus stop, overflow for events, and parking strip that isolates main parking are from view. Horowitz noted that the fifth standard would be difficult to meet. Horowitz also noted that the fourth requirement, Overall Development, was already addressed by the Applicant at the last meeting.

[7:05:06 PM](#) Chair Fugate inquired about the parking area expansion and whether or not the parking could be limited until the expansion comes to fruition. Chair Fugate suggested partitioning off a portion of the parking. Horowitz noted that a partition would not be necessary and instead, the parking for the expansion phase could be noted in the Findings of Fact as being built at this time. Dan Smith inquired about square footage of expansion. Jessica Aguilar noted that the expansion would be approximately 880 square feet. Horowitz noted that the expansion square footage is equal to one parking stall.

[7:07:06 PM](#) Chair Fugate questioned what the Applicant must do to be compliant at the time of the expansion. Lisa Horowitz suggested that the Commissioners look at the regional nature of the bank and decide whether or not the remaining standard provide flexibility to consider it in the scope of the entire neighborhood.

[7:08:13 PM](#) Owen Scanlon requested that Terry King show the Commissioners where the two large evergreen trees are on the site. Chair Fugate questioned the location of the crab apple and maple trees. King showed the Commissioners where the trees are currently located.

[7:10:43 PM](#) Horowitz noted that the maple tree is currently in a landscape bed and may be moved slightly up to address the safety issues. Chair Fugate is wondering if the smaller parking could be eliminated to meet the fourth standard. Horowitz suggested that the Commissioners find four standards (that don't include parking) that would fulfill the requirements and concurrently decide on what is to happen with the trees. Horowitz noted that she will expand on this matter in the Findings of Fact.

[7:11:48 PM](#) Chair Fugate inquired about the regional nature and how it relates to criteria. Horowitz noted that the excess parking would be beneficial to area, as the south end of Hailey is limited in parking places. Horowitz noted that the way the requirement is written, four out of five standards will need to be identified. Horowitz suggested that the Commissioners identify a fourth standard and she will expand on parking matter in the Findings of Fact.

[7:15:18 PM](#) Owen Scanlon noted that the proposed parking at D.L. Evans Bank in Hailey will accommodate vehicles and customers from Hailey, Bellevue, Ketchum and Sun Valley, and doesn't feel it should be an issue. Richard Pogue agreed.

[7:16:04 PM](#) Chair Fugate noted that she liked the proposed fencing; however, she would like to see it before being installed.

[7:16:49 PM](#) Horowitz utilized the landscaping plan to show location of all trees currently on property. Horowitz added that Public Works has hired a Certified Arborist, whom has discussed options of new trees, such as Columnar Oak, with Lisa.

[7:19:19 PM](#) Chair Fugate questioned whether or not the Commissioners are in agreement with staff and Tree Committee as far as having trees replaced with 2.5 to 3 caliper tree, as well as having the railing pulled out, and sidewalks widened. Jeff Engelhardt noted that he would like to see a larger caliper tree, but will approve a smaller tree. Engelhardt also noted that he believes the interior trees are in the way and the owner has the right to remove them. In lieu of the interior trees, Engelhardt would like to see a larger caliper street tree.

[7:20:48 PM](#) Horowitz suggested that the Commissioners provide D.L. Evans with some wiggle room, as trees may not be available and Applicant may have no other option but to go with a larger caliper tree.

[7:21:18 PM](#) Chair Fugate and Commissioners are all in agreement with the street trees. Scanlon is pleased the oak tree is not being replaced.

[7:23:20 PM](#) Richard Pogue noted that he hates to lose the interior trees; however, with this new development, they will be lost. Dan Smith noted that with the current trees do not compliment the current topography of the lot. Chair Fugate agreed and complimented the Applicant on a job well done in accommodating staff suggestions.

[7:24:14 PM](#) Horowitz inquired about the maple tree and whether or not it would survive if they kept it and retained the grade coming in to the curb. Terry King noted that there will be lot of fill used on site and fears that the tree will not survive.

[7:26:00 PM](#) Scanlon noted that if all of the trees start off at the same scale, with the exception of the oak tree, the site would be visually more appealing. Chair Fugate agreed.

[7:27:32 PM](#) Chair Fugate questioned whether or not the Commissioners were all in agreement with the interior trees. Commissioners agreed.

[7:28:04 PM](#) Chair Fugate inquired about the banner pole and street light. Horowitz noted that this was made a condition of approval and that the Applicant would be responsible for purchasing and installing the new pole. Per Jessica Aguilar, D.L. Evans Bank was not aware that this was a City expense. Aguilar noted that D.L. Evans would not be opposed to putting in a street light similar to that of a downtown street light on the corner; however, is concerned with the impact that the construction will have on the oak tree.

[7:31:02 PM](#) Horowitz noted that the pole sign and street light would be pushed to the south. Aguilar noted that D.L. Evans would not be opposed, but would prefer to see the City install the light pole.

[7:31:51 PM](#) Horowitz presented the Conditions brought up by the Commissioners. Horowitz noted the following modifications: Condition (n) would be modified to read: the dumpster location shall be relocated, close to the corner of Chestnut Street, with the final design to be approved by staff and the Chair of Planning and Zoning, assuming the location is acceptable to Clear Creek Disposal with regards to and the power pole and line. New condition (t) would read: The parking stall adjacent to the alley shall be signed for employee parking only. Condition (u) would read: The final fence design shall be approved by staff and the Planning and Zoning Chair.

[7:33:13 PM](#) Owen Scanlon requested another look at the drawings for the bus enclosure. Scanlon questioned why the proposed bus stop is not patterned after the ones already located in Hailey. Jessica Aguilar noted that they asked Jason Miller to modify the design slightly for security purposes. The proposed design will allow for visibility on all three sides. Aguilar noted that the enclosure is made of wood with glass panes on three sides, and a roof that will accommodate the snow load.

[7:38:00 PM](#) Dan Smith concluded with one housekeeping item: For 22,800 square feet of hardscape plans are calling for 45/67. Smith noted that 57/16 is needed. Smith also questioned the drip line distance around the oak. Horowitz noted that per staff recommendation and the conditions of approval, ½ is suggested. Horowitz noted that the condition could be modified to include ½ to 2/3.

[7:39:33 PM](#) Smith inquired about the construction plan. Horowitz noted that a construction staging plan was included in the first set. If not, Horowitz will make it another conditional of approval. Smith also inquired about the energy plan.

[7:40:31 PM](#) Jessica Aguilar noted that D.L. Evans plans to meet the requirements of the energy code; however, does not care to complete the paperwork associated with LEED.

[7:41:10 PM](#) Dan Smith noted his appreciation for D.L. Evans wanting to invest in Hailey and believes it will be a nice compliment to the community. Chair Fugate agreed.

[7:42:09 PM](#) **Richard Pogue motioned to a Design Review Application by D.L. Evans Bank, represented by Tom Lennon and Andy Erstad of Erstad Architects, for a new 4,595 square foot bank branch to be located at 609 South Main Street (Lots 1-8, Block 5 Hailey Townsite) and 611 South Main Street in the Business (B) and Townsite Overlay (TO) Zoning Districts, finding that The project does not jeopardize the health, safety or welfare of the public, and the project Conforms to the applicable specifications outlined in the Design Review Guidelines, applicable requirements of the Zoning Ordinance, Title 18, and City Standards, provided conditions (a) through (v) are met. Conditions (h) and (n) have been modified. (T), (U) and (V) are new modifications. Dan Smith seconded and all were in favor.**

**NB 2** *Reconsideration of a Conditional Use Permit Application from UPS for a temporary structure to be located at 111 South River Street (Lots 1-7 of Block 31, Hailey Townsite) within the Business (B) and Townsite Overlay (TO) Zoning Districts.*

[7:44:46 PM](#) Horowitz presented the project and noted that when the Temporary Conditional Use Permit was approved, a requirement was to review the project in six (6) months. Horowitz presented the conditions from the Staff Report as: b) parking on the corner of the intersection to protect site visibility, c) idling and d) reviewing project within time period of six (6) months.

[7:45:45 PM](#) Judy Younce noted that all requirements have been met and believes that the unit is a piece of equipment, not temporary storage. Younce noted that without a Conditional Use Permit, UPS would have to relocate. At this time, UPS is not interested in leaving the community of Hailey and if forced to relocate, UPS may choose not to return to Hailey.

[7:48:24 PM](#) Younce noted that UPS is growing at a rapid rate and per Younce, there is a possibility that UPS will need to relocate in the future to accommodate growth. No timeframe is available at this time.

UPS would like to continue to work with Hailey; however, Younce noted that if the City of Hailey requires UPS to put in sidewalks, parking and other infrastructure, UPS will no longer be able to conduct business at location.

[7:49:29 PM](#) Chair Fugate disclosed that after the last Planning and Zoning Meeting where UPS was addressed, Julie Cord phoned Janet Fugate. Chair Fugate noted that due to the nature of the phone call, no details or information were discussed. Julie Cord understood and the conversation ended.

[7:50:45 PM](#) Horowitz added that when the issue of the Matrix goes before City Council, Ned Williamson, City Attorney, will study the code and make an opinion about whether or not the temporary storage facility is an expansion of the non-conforming use. Horowitz noted that Williamson believes it may fall into a different category, but needs to study the code a bit more to make a more accurate determination. Horowitz noted that a decision still needs to be made on the part of the City regarding the temporary structure.

[7:51:23 PM](#) Horowitz also noted that she believes the Applicant has done a good job on keeping the corner lot clean and suggested that, due to neighbor complaints, employees park on the property, as opposed to other City parking areas. Judy Ounce noted that UPS does not tell their employees where to park; however, are encouraged to park on property.

[7:52:19 PM](#) Chair Fugate opened the item for public comment. Peter Lobb noted that he does not have an attachment to UPS and is supportive of the Conditional Use Permit. Lobb believes that UPS has been a good business for Hailey. For now, Lobb believes the Conditional Use Permit is warranted and should be granted to UPS.

[7:53:48 PM](#) Robyn Lively noted that the structure is an additional extension to UPS. Lively explained that the volume doubles during the holidays and without it, UPS would not be able to efficiently process all packages during the season.

[7:55:08 PM](#) Chair Fugate closed the item for public hearing. Chair Fugate noted that the Matrix issue has gone past the Planning and Zoning Board; currently addressing the Conditional Use Permit. Jeff Engelhardt inquired about issuing a Conditional Use Permit for temporary building or equipment. Owen Scanlon agreed that it's a temporary structure – a piece of equipment that packages move through.

[7:56:33 PM](#) Chair Fugate questioned whether or not the application is incorrectly worded. Scanlon doesn't believe so, as the building is structure. Scanlon reiterated that the goal is to prevent another company or competition from building a large trucking center on Main Street or in downtown Hailey. Scanlon noted that it's not their intent to run UPS out of town and believes a Conditional Use Permit is warranted, as UPS is a key part of getting packages to/from community.

[7:57:26 PM](#) Chair Fugate noted from a previous discussion that the final decision be postponed until a later date. Horowitz reminded the Commissioners that a Conditional Use Permit for this temporary structure can be approved in perpetuity; Commissioners do not have to put a 12-month timeline on item, unless violation occurs.

[7:58:36 PM](#) Horowitz noted that the definitions need to be clarified. Richard Pogue added that UPS is extremely integral to community. Scanlon believes there are grandfather rights and should be considered.

[8:08:15 PM](#) Chair Fugate questioned Commissioners on next steps. Jeff Engelhardt agreed with Lisa Horowitz's suggestion in approving the Conditional Use Permit in perpetuity. Engelhardt noted that UPS will likely outgrow site soon. Chair Fugate inquired about whether or not the Conditional Use Permit is applicable only to the temporary structure. Horowitz confirmed and noted that so long as the conditions of approval are met and the site is kept clean; not disturbing neighbors. Chair Fugate inquired about whether or not another condition should be included, stating that employees should park on site.

[8:10:59 PM](#) Robyn Lively agreed with the suggestion that employees should park on the lot; however, also noted that the UPS pre-loaders usually start around 7:00am each morning and will staff anywhere from 5 to 18 pre-loaders. Truck drivers will usually start work around 10:30am. There are anywhere

from 8 to 18 drivers. Pre-loaders and truck drivers overlap in schedules; Lively noted that the lot cannot accommodate 32 vehicles at one time.

[8:11:44 PM](#) Chair Fugate questioned the number of parking spaces needed during peak season. Lively noted that employees park as close and as many as they can on the lot; however, during peak time, eight (8) more UPS trucks will be located on site.

[8:12:45 PM](#) Chair Fugate believes this to be a growing issue and noted that parking could be a reason to revisit issue in 12 months.

[8:13:34 PM](#) Owen Scanlon motioned to approve a Conditional Use Permit, for UPS for a temporary structure to be located at 111 South River Street (Lots 1-7 of Block 31, Hailey Townsite) within the Business (B) and Townsite Overlay (TO) Zoning Districts, finding that the project does not jeopardize the health, safety or welfare of the public and the project conforms to the applicable specifications outlined in the Design Review Guidelines , applicable requirements of the Zoning Ordinance, Title 18, and City Standards, provided conditions (a) through (c) are met.

[8:14:27 PM](#) Chair Fugate inquired about adding a request or condition that employees park on lot whenever possible. Richard Pogue and Owen Scanlon noted that it sounds like they are already actively trying to do that.

[8:15:02 PM](#) Richard Pogue seconded and all were in favor.

**NB 3** *Consideration of an Amendment to the Hailey Comprehensive Plan to:*

1. *Update Part 5, Capital Improvement Plan to reflect the 2016 TishlerBise report, "Development Impact Fees"*
2. *Repeal Appendix E, 2012 Capital Development Impact 2012 Update, and adopt the 2016/2017 Capital Expense Budget and 2016 Capital Improvement Plan as Appendix E to the Comprehensive Plan.*

[8:16:31 PM](#) Horowitz explained that this item would amend the Comprehensive Plan and updates the packet with the most current information. Dan Smith inquired about whether or not this item would replace the document completed in 2012. Horowitz confirmed this.

[8:17:27 PM](#) Chair Fugate opened the item for public comment.

[8:17:35 PM](#) Dan Smith motioned to approve *an Amendment to the Hailey Comprehensive Plan* to:

1. *Update Part 5, Capital Improvement Plan to reflect the 2016 TishlerBise report, "Development Impact Fees"*
2. *Repeal Appendix E, 2012 Capital Development Impact 2012 Update, and adopt the 2016/2017 Capital Expense Budget and 2016 Capital Improvement Plan as Appendix E to the Comprehensive Plan,*

*finding that the project does not jeopardize the health, safety and welfare of the public and the project conforms to the applicable specifications outlined in the Design Review Guidelines, applicable requirements of the Zoning Ordinance, Title 18, and City Standards, provided that all conditions are met.* Jeff Engelhardt seconded and all were in favor.

## **Old Business**

### **Commission Reports and Discussion**

### **Staff Reports and Discussion**

**SR 1** Discussion of current building activity, upcoming projects, and zoning code changes.  
(no documents)

▪ **Design Review Exemption:** On August 16, 2016, a Design Review Exemption Application was submitted by the Blaine County Historical Museum, located at 218 North Main Street (Lot 11, Block 45, Hailey Townsite) in the Business (B) and Townsite Overlay (TO) Zoning Districts, for approval of a 6' x 8' Detached Shed Structure, to be placed at the rear of the Blaine County Historical Museum, on Lot 12, Block 45. The Chair and Administrator, having been presented with all information and testimony in favor and in opposition to the proposal, hereby determine that the project is minor, will not conflict with the design review standards, will not adversely impact adjacent properties, will not alter any structural or architectural elements of the building, and is not a building and is not subject to design review.

**SR 2** Discussion of the next Planning and Zoning meeting: Monday, September 12, 2016.  
(no documents)

**Adjourn**

**8:20:36 PM** Owen Scanlon motion to adjourn. Richard Pogue seconded and all were in favor.

**MINUTES OF THE MEETING OF THE  
DEVELOPMENT IMPACT FEE ADVISORY COMMITTEE  
Monday, August 22, 2016**

**Call to Order**

**8:21:04 PM** Chair Fugate called the meeting to order. Call to order

**CA 1** Motion to approve the minutes of the Development Impact Fee Advisory Committee from August 1, 2016

**8:21:12 PM** Richard Pogue motioned to approve all Consent Agenda items. Owen Scanlon recused himself due to being absent from the August 1, 2016 meeting. Jeff Engelhardt seconded and four Advisory Committee members were in favor.

**Adjourn**

**8:21:45 PM** Dan Smith motioned to adjourn. Richard Pogue seconded and all were in favor.

**Return to Agenda**

## **FINDINGS OF FACT, CONCLUSIONS OF LAW AND DECISION**

On June 22, 2016, the Hailey Planning & Zoning Commission considered a Design Review application submitted by D.L. Evans Bank, represented by Tom Lennon and Andy Erstad of Erstad Architects, for a new 4,595 square foot bank branch to be located at 609 and 611 South Main Street (Lots 1-8, Block 5 Hailey Townsite) in the Business (B) and Townsite Overlay (TO) Zoning Districts.

### **FINDINGS OF FACT**

#### **Notice**

Notice for the public hearing was published in the Idaho Mountain Express on June 22, 2106 and mailed to property owners within 300 feet on June 22, 2016. The project was continued on the record from the July 11 P & Z meeting.

#### **Application**

D.L. Evans Bank plans to construct a new 4,595 square foot bank branch to be located at 609 and 611 South Main Street. The site plan includes a drive-through window off of Chestnut Street, a perennial bed area that could accommodate an approximately 855 square foot addition to the bank in the future, a 19-stall parking lot that can be used for transit riders and events at Werthheimer Park, 6 (employee) parking stalls off the alley, new bus stop and upgraded streetscape plan.

A 13,108 square foot building was proposed for this site in 2009, but never constructed.

#### **Procedural History**

The application was submitted on June 6, 2016 and certified complete on the same date. A public hearing before the Planning and Zoning Commission was held on July 11, 2016, in the Hailey City Council Chambers. The Commission continued the project until the August 22, 2016 meeting, which included a site visit. Items discussed at the August 22, 2016 hearing included:

- Street trees on Main Street
- Large Oak on the corner of Main Street (also a city tree)
- Interior trees
- Grade changes with regard to trees
- Parking adjacent to the alley and screening, if required by Code
- Size/scope of additional parking above 200% of requirement
- Roof design over the drive-through
- Trash enclosure screening and location
- Lighting details
- Bus shelter details and bus stop design

Revised drawings were submitted, and have been attached to this report. Upon receipt of these Findings of Fact, Conclusions of Law and Decision, the applicant may request a regulatory takings analysis pursuant to Idaho Code § 67-8003.

<b>General Requirements for all Design Review Applications</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.050	<b>Complete Application</b>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>Department Comments</b>	<b>Engineering:</b> See comments herein on the Main Street curb cut and banner pole
				<b>Life/Safety:</b> No comments
				<b>Water and Sewer:</b> The project will comply with all City water and sewer standards. Two water meter vaults exist for this property: applicant will either need to use both of them (i.e. one for in the building and the other for irrigation) or abandon one of them at the water main. The existing sewer service should be used for the new building. The existing sewer service should be inspected to verify integrity and type pipe used. If AC pipe is in use, wastewater suggests replacing with a PVC sewer pipe such as SDR35 at least from the alley in to new building, per plumber recommendations.
				<b>Building:</b> No comments <b>Streets:</b> See comments herein on the banner pole
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.08A Signs	<b>17.08A Signs:</b> The applicant is hereby advised that a sign permit is required for any signage exceeding four square feet in sign area. Approval of signage areas or signage plan in Design Review does not constitute approval of a sign permit.
			<b>Staff Comments</b>	Signs have not yet been submitted, but will conform to City regulations.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.09.040 On-site Parking Req.	<b>See Section 17.09.040 for applicable code.</b> 9.4.5, 1 space per 1,000 square feet. 9.4.A.1, fractional spaces are rounded down
			<b>Staff Comments</b>	The site plan has been revised to show a total 25 parking spaces parking spaces: 6 off of the alley, and them remainder in two configurations to the south of the building. Six parking spaces are required for this project. The section below regarding excess in permitted parking applies. See conditions of approval regarding parking uses and limitations.
X				<b>17.09.040.06: EXCESS OF PERMITTED PARKING:</b>    <b>A. Approval Required:</b> No use shall provide on-site parking for more than two hundred percent (200%) of the number of spaces required by this chapter unless permitted by specific action of the commission. Applications for parking in excess of that normally permitted will be heard by the commission as part of other applications, or, where no other application is pertinent, under the notice and hearing procedures set forth for design review. The parking requested is in excess of 200%; therefore the Commission must make specific findings on the standards below.  <b>B. Criteria:</b> The commission shall consider the following criteria when evaluating any

				<p><b>application for parking in excess of that normally permitted. Applicants are required to satisfy at least four (4) of the following criteria:</b></p> <p><b>1. The excess parking area will be commonly used for public interests such as park and ride or carpool lots. The property owner will be permitted to reserve the use of the parking area twelve (12) days in any calendar year.</b> <i>Mountain Rides has a bus stop in front of the subject property. They are also requesting a shelter in this location. A revised comment letter from Mountain Rides was attached to the August 22 staff report. The location of the 14-stall parking lot is convenient for the bus stop. Park and Ride or carpool parking will be permitted in this area to meet this standard. The addition of a path through the landscape area, bike rack and trash receptacle will be in the public interest.</i></p> <p><b>2. The excess parking area provided would relieve or help to relieve a substantial shortage of parking within an eight hundred foot (800') radius.</b> <i>A shortage of parking occurs seasonally at the Rodeo Grounds/Hailey Ice. While this location is across Main Street, it would function for large events such as hockey games. The Commission found that this parking area would address parking shortages within an 800' radius.</i></p> <p><b>3. The excess parking area will not be adjacent to a public right of way, and will be separated from the right of way by a building.</b> <i>The revised design show the parking well separated from Main Street by a substantial landscape strip, but not by a building. The Commission found that this landscape strip functions to separate the parking from the public right of way.</i></p> <p><b>4. The excess parking area is part of an overall development scheme which compensates for insufficient parking in other portions of the same development.</b> <i>The applicant described the regional nature of this bank branch at the previous meeting. The Commission found that this standard has been met. The Commission required that the parking adjacent to the alley be limited to employees of the bank, to decrease traffic in this alley adjacent to a residential zone district.</i></p> <p><b>5. The excess parking area will be used as an alternate facility, such as a basketball court or skateboard park, when not in use as an overflow parking area. The property owner will be permitted to reserve the use of the parking area twelve (12) days in any calendar year. No alternative use has been proposed.</b></p>
<input checked="" type="checkbox"/> ?	<input type="checkbox"/>	<input type="checkbox"/>	<b>17.08C.040 Outdoor Lighting Standards</b>	<p><b>17.08C.040 General Standards</b></p> <p>a. All exterior lighting shall be designed, located and lamped in order to prevent:</p> <ol style="list-style-type: none"> <li>1. Overlighting;</li> <li>2. Energy waste;</li> <li>3. Glare;</li> <li>4. Light Trespass;</li> <li>5. Skyglow.</li> </ol> <p>b. All non-essential exterior commercial and residential lighting is encouraged to be turned off after business hours and/or when not in use. Lights on a timer are encouraged. Sensor activated lights are encouraged to replace existing lighting that is desired for security purposes.</p> <p>c. Canopy lights, such as service station lighting shall be fully recessed or fully shielded so as to ensure that no light source is visible from or causes glare on public rights of way or adjacent properties.</p> <p>d. Area lights. All area lights are encouraged to be eighty-five (85) degree full cut-off type luminaires.</p> <p>e. Idaho Power shall not install any luminaires after the effective date of this Article that lights the public right of way without first receiving approval for any</p>

				<b>such application by the Lighting Administrator.</b>
			<b>Staff Comments</b>	<i>The applicant is proposing a variety of light fixtures. Cut sheets were attached, to the previous packet and the applicant has stated that final design will meet City standards. A photometric plan has been identified in the conditions of approval.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>Bulk Requirements</b>	<b>(Insert sections from applicable zoning district)</b>
			<b>Staff Comments</b>	<i>The District Use matrix has zero front, side and rear setbacks in the B Zone. The building is within all of these setbacks. The building is proposed to be 24'-8, which is within the 35' allowed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>17.06.070(A)1 Street Improvements Required</b>	<p><b>Sidewalks and drainage improvements are required in all zoning districts, except as otherwise provided herein.</b></p> <p><i>A 6' Sidewalk and street trees in a 4' (+/-) recessed tree well at the back of the sidewalk (but within the City right of Way) are existing on Main Street. The site rises to the south: these trees come back to sidewalk grade at the southern end of the property. Portions of the sidewalk will be reconstructed in the area of the bulb out. Staff has studied the sidewalk, grade changes and existing trees along this block in detail in collaboration with the Tree Committee, and there is consensus that a 10' sidewalk as per Title 18 Mobility Ordinance would be appropriate on Main Street. This could only be accomplished in conjunction with a revised tree planting plan for the block, which is discussed elsewhere in this report. A new 5' sidewalk (plus 1' curb line for a total of 6') will be added along the length of the property on Chestnut (from Main Street to the alley). This design is consistent with Title 18, Mobility Standards.</i></p> <p><i>A drainage plan has been submitted, prepared by a registered engineer.</i></p>
			<b>Staff Comments</b>	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>17.06.070(B) Required Water System Improvements</b>	<p><b>In the Townsite Overlay District, any proposal for new construction or addition of a garage accessing from the alley, where water main lines within the alley are less than six (6) feet deep, the developer shall install insulating material (blue board insulation or similar material) for each and every individual water service line and main line between and including the subject property and the nearest public street, as recommended by the City Engineer.</b></p>
			<b>Staff Comments</b>	<i>This has been made a condition of approval.</i>

<b>Design Review Requirements for Non-Residential, Multifamily, and/or Mixed Use Buildings within the City of Hailey</b>				
<b>1. Site Planning: 17.06.080(A)1, items (a) thru (n)</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1a	a. The location, orientation and surface of buildings shall maximize, to the greatest extent possible sun exposure in exterior spaces to create spaces around buildings that are usable by the residents and allow for safe access to

				<b>buildings</b>
			<b>Staff Comments</b>	<i>The proposed building follows the grid pattern in downtown Hailey. Outdoor seating is planned on the south side; sun angles were considered.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1b	<p><b>b. All existing plant material shall be inventoried and delineated, to scale, and noted whether it is to be preserved, relocated or removed. Removal of trees larger than 6-inch caliper proposed to be removed require an arborist review. Any tree destroyed or mortally injured after previously being identified to be preserved, or removed without authorization, shall be replaced with a species of tree found in the Tree Guide and shall be a minimum of 4 inch caliper.</b></p>
			<b>Staff Comments</b>	<p><i>The site contains a variety of landscaping, shown in a screened back version on the landscape plan. Staff requested a separate exhibit that better illustrates existing landscaping, which was provided. The site plan reviewed by the Commission in July showed ten (10) mature City street trees are proposed for removal, and nine (9) other mature trees from the southern half of the site. Much discussion occurred regarding the age and condition of the trees, the grade changes and the tree variety.</i></p> <p><i>Staff has conferred again with the Tree Committee to discuss the issues presented in this block. There is general consensus that, due to grade, tree species, age and condition and the fact that this project presents an opportunity to plan for an entire City block, that it is appropriate to re-think the entire block, and to develop a plan that is compatible with the chokecherries across the street, but accommodates pedestrians, the bus shelter, and the sidewalk and tree standards called for in Title 18, Mobility Ordinance. Per this line of thinking, the chokecherries would be allowed to be replaced with a new line of street trees that are designed in grates at the back of the sidewalk, at grade, and within a 10' sidewalk. The exception is the mature oak on the corner. This Oak is part of a pattern of corner oaks on this block and the next, which represent a mature street tree plan. These oaks are a highly valuable specimen tree. Staff and the Tree Committee recommend that the corner Oak be retained, and that the grade be protected under the tree for a distance of 1/2 to 2/3 of the tree canopy. (The tree is proposed to be retained, but the undisturbed area needs to be increased).</i></p> <p><i>The revised plan shows the following street trees:</i></p> <p><i>6- columnar oaks, 2" caliper          1-American Linden, 2" caliper          1-White Oak, 2" caliper</i></p> <p><i>Revised comments from Tree Committee members are attached to the August 22 staff report regarding the overall design. Public Works staff has been consulted regarding species type and size. City staff has indicated that the street trees should be a minimum of 2-1/2 to 3" caliper. Note that tree size and additional comments on soil and planting are incorporated into the conditions of approval. The site plan shows the specimen oak on the northeast corner as being retained. However, the undisturbed area under the tree needs to be increase to a minimum of 1/2 to 2/3 of the tree canopy. This has been made a condition of approval.</i></p> <p><i>Regarding the interior trees, further site evaluation with Tree Committee members suggested that the mature maple in the vicinity of the southern parking lot is a valuable mature specimen tree which could provide nice shade for the excess parking area. The Commission discussed this tree, and heard testimony from the project landscape architect that the combination of grade changes and hardscape associated with the parking lot may affect the ability to save this tree. The Commission determined that the interior trees</i></p>

				<i>could be removed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1c	<p><b>c. Site circulation shall be designed so pedestrians have safe access to and through the site and to building.</b></p> <p><i>Staff Comments</i></p> <p><i>Pedestrians access the building from the front sidewalk or the back parking area in a safe manner. The revised site plan meets the Mobility standards for a 10' sidewalk on Main Street. A striped walkway crosses the drive-through zone leading in to the perennial bed area.</i></p> <p><i>The applicant is proposing to remove the residential curb cut on to Main Street, which would become a commercial grade curb cut. Commercial curb cuts on Main Street are undesirable, and staff supports the elimination of this curb cut.</i></p> <p><i>Staff recommended an additional pedestrian access from the southerly parking lot to the bus shelter. This has been made a condition of approval.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1d	<p><b>d. Building services including loading areas, trash storage/pickup areas and utility boxes shall be located at the rear of a building; the side of the building adjacent to an internal lot line may be considered as an alternate location. These areas shall be designed in a manner to minimize conflict among uses and shall not interfere with other uses, such as snow storage. These areas shall be screened with landscaping, enclosures, fencing or by the principal building.</b></p> <p><i>Staff Comments</i></p> <p><i>Trash receptacles will be stored within an enclosure on the west side of the building adjacent to the alley for easy pick-up. The enclosure should be reviewed by Clearcreek Disposal. Elevations of the enclosure were provided in the packet. The location of the enclosure was discussed at the meeting, in addition to the fact that, at this time, the bank will not require a commercial dumpster, but will use residential-sized cans. The Commission recommended that, if possible, the dumpster enclosure be move to the north, closer to the corner, but still accessing off of the alley. See conditions of approval.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1e	<p><b>e. Where alleys exist, or are planned, they shall be utilized for building services.</b></p> <p><i>Staff Comments</i></p> <p><i>- The alley will be utilized for utilities and parking.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)1f	<p><b>f. Vending machines located on the exterior of a building shall not be visible from any street.</b></p> <p><i>Staff Comments</i></p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1g	<p><b>g. On-site parking areas shall be located at the rear of the building and screened from the street. Parking and access shall not be obstructed by snow accumulation. (NOTE: If project is located in Airport West Subdivision, certain standards may apply that are not listed here. See code for details.)</b></p> <p><b>i. Parking areas located within the SCI zoning district may be located at the side or rear of the building.</b></p> <p><b>ii. Parking areas may be considered at the side of buildings within the B, LB, TI and LI zoning districts provided a useable prominent entrance is located on the front of the building and the parking area is buffered from the sidewalk adjacent to the street.</b></p> <p><i>Staff Comments</i></p> <p><i>A total of 19 parking spaces are proposed in two parking lots south of the building, and an additional 6 spaces off of the alley. They are screened from Main Street by a substantial landscape strip. A bus shelter and improved bus stop is proposed in this area. The applicant is proposing to eliminate the residential curb cut on to Main Street. Commercial curb cuts on Main Street are undesirable, and staff recommends the elimination of this</i></p>

				<i>curb cut. The Commission concurred with this recommendation. The Commission required that the parking proposed off the alley be limited to bank employees. A Commissioner noted that the radius between the dumpster and the drive through separating landscaping from drive aisles may not be sufficient.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1h	<b>h. Access to on-site parking shall be from the alley or, if the site is not serviced by an alley, from a single approach to the street to confine vehicular/pedestrian conflict to limited locations, allow more buffering of the parking area and preserve the street frontage for pedestrian traffic.</b>
			<i>Staff Comments</i>	<i>The site is serviced by the alley, and all parking stalls are accessible from the alley. The Commission discussed additional parking standards from the zoning code that appear to be in conflict with this requirement. A memo from the City Attorney was attached to the August 22 staff report giving direction on this issue. Screening fences have been added between the southerly landscape bed and the alley. A concept plan for the fences, showing metal artwork was entered into the record at the August 22 hearing. Final designs have not been completed for the metal screen fences. The Commission determined that this standard has been met, and added conditions relative to the final design of the screening fences.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1i	<b>i. Snow storage areas shall be provided on-site where practical and sited in a manner that is accessible to all types of snow removal vehicles of a size that can accommodate moderate areas of snow.</b>
			<i>Staff Comments</i>	<i>The site plan proposes 22,864 square feet of hardscape (parking, vehicle and pedestrian areas). 25% of this (5,716) is required as a snow storage area.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1j	<b>j. Snow storage areas shall not be less than 25% of the improved parking and vehicle and pedestrian circulation areas.</b>
			<i>Staff Comments</i>	<i>See Standard (i) above.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1k	<b>k. A designated snow storage area shall not have any dimension less than 10 feet.</b>
			<i>Staff Comments</i>	<i>Snow storage areas vary in width: some are only 10' wide.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1l	<b>l. Hauling of snow from downtown areas is permissible where other options are not practical.</b>
			<i>Staff Comments</i>	<i>Snow will need to be hauled from the large parking areas proposed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1m	<b>m. Snow storage areas shall not impede parking spaces, vehicular and pedestrian circulation or line of sight, loading areas, trash storage/pickup areas, service areas or utilities.</b>
			<i>Staff Comments</i>	<i>Snow storage areas do not impede parking or pedestrian areas.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1n	<b>n. Snow storage areas shall be landscaped with vegetation that is salt-tolerant and resilient to heavy snow.</b>
			<i>Staff Comments</i>	<i>Snow storage areas will be seeded with grass only.</i>

**2. Building Design: 17.06.080(A)2, items (a) thru (m)**

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2a	<b>a. The proportion, size, shape and rooflines of new buildings shall be compatible with surrounding buildings.</b>
			<i>Staff Comments</i>	<i>The propose building uses a variety of flat roof line, canopies and parapets. Per feedback</i>

				<i>from the Commission, the roof line of the drive-through ahs been redesigned to be more reflective of the overall building design.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2b	<b>b. Standardized corporate building designs are prohibited.</b>
			<i>Staff Comments</i>	<i>Modifications have been made to the standard corporate design to reflect the character of Hailey.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2c	<b>c. At ground level, building design shall emphasize human scale, be pedestrian oriented and encourage human activity and interaction.</b>
			<i>Staff Comments</i>	<i>The building features a modest profile. Doors and canopies reflect human scale.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2d	<b>d. The front façade of buildings shall face the street and may include design features such as windows, pedestrian entrances, building off-sets, projections, architectural detailing, courtyards and change in materials or similar features to create human scale and break up large building surfaces and volumes.</b>
			<i>Staff Comments</i>	<i>The main entry / front façade faces Main Street and has multiple windows sheltered by a canopy and columns which breaks up the larger roof form above.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)2e	<b>e. Any addition onto or renovation of an existing building shall be designed to create a cohesive whole.</b>
			<i>Staff Comments</i>	<i>Plans for future expansion have been shown in a dashed line on the plans. No drawings have been submitted at this time, but the applicant has indicated that the design would be compatible with the existing building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2f	<b>f. All exterior walls of a building shall incorporate the use of varying materials, textures and colors.</b>
			<i>Staff Comments</i>	<i>A variety of materials will be used on the exterior: stucco, brick veneer, soldier course, exposed metal columns. Materials samples were submitted as part of the application.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2g	<b>g. Exterior buildings colors and materials shall be integrated appropriately into the architecture of the building and be harmonious within the project and with surrounding buildings.</b>
			<i>Staff Comments</i>	<i>Building colors are in the “brown” family: brick cladding, brown stucco, dark bronze parapet caps, desert buff stone, urbane bronze fascia. See color and material samples for additional information.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2h	<b>h. Flat-roofed buildings over two stories in height shall incorporate roof elements such as parapets, upper decks, balconies or other design elements.</b>
			<i>Staff Comments</i>	<i>The proposed building contains parapets and canopies.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2i	<b>i. All buildings shall minimize energy consumption by utilizing alternative energy sources and/or passive solar techniques. At least three (3) of the following techniques, or an approved alternative, shall be used to improve energy cost savings and provide a more comfortable and healthy living space:</b> <b>i) Solar Orientation.</b> If there is a longer wall plane, it shall be placed on an east-west axis. A building’s wall plane shall be oriented within 30 degrees of true south. <b>ii) South facing windows with eave coverage.</b> At least 40% of the building’s total glazing surface shall be oriented to the south, with roof overhang or awning coverage at the south. <b>iii) Double glazed windows.</b>

				<ul style="list-style-type: none"> <li>iv) Windows with Low Emissivity glazing.</li> <li>v) Earth berming against exterior walls</li> <li>vi) Alternative energy. Solar energy for electricity or water heating, wind energy or another approved alternative shall be installed on-site.</li> <li>vii) Exterior light shelves. All windows on the southernmost facing side of the building shall have external light shelves installed.</li> </ul>
			<i>Staff Comments</i>	<i>The applicant described energy plans in the meeting: the project will exceed all energy codes; it will have solar gain; windows will be energy compliant.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2j	j. Gabled coverings, appropriate roof pitch, or snow clips and/or gutters and downspouts shall be provided over all walkways and entries to prevent snow from falling directly onto adjacent sidewalks.
			<i>Staff Comments</i>	<i>The flat-roofed buildings do not shed snow on to the pedestrian areas.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2k	k. Downspouts and drains shall be located within landscape areas or other appropriate locations where freezing will not create pedestrian hazards.
			<i>Staff Comments</i>	<i>Downspouts from the building will be routed underground to a drywell. A second drywell system serves the proposed parking lot, as shown on the drainage plan.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)2l	l. Vehicle canopies associated with gas stations, convenience stores or drive-through facilities shall have a minimum roof pitch of 3/12 and be consistent with the colors, material and architectural design used on the principal building(s).
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)2m	m. A master plan for signage is required to ensure the design and location of signs is compatible with the building design and compliance with Article 8.
			<i>Staff Comments</i>	<i>A Master signage plan is not required of a single-tenant building.</i>

**3. Accessory Structures, Fences and Equipment/Utilities: 17.06.080(A)3, items (a) thru (i)**

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3a	a. Accessory structures shall be designed to be compatible with the principal building(s).
			<i>Staff Comments</i>	<i>No accessory structures are planned, with the exception of the dumpster enclosure. An elevation of the enclosure was submitted prior to the August 22 hearing.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3b	b. Accessory structures shall be located at the rear of the property.
			<i>Staff Comments</i>	<i>The dumpster is located to the rear of the property; the location has been modified to be compatible with neighboring development per the Conditions of Approval herein. A dumpster enclosure has been provided.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3c	c. Walls and fences shall be constructed of materials compatible with other materials used on the site.
			<i>Staff Comments</i>	<i>An elevation of the trash enclosure has been submitted, and was reviewed in the hearing.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3d	d. Walls and fencing shall not dominate the buildings or the landscape. Planting should be integrated with fencing in order to soften the visual impact.

			<b>Staff Comments</b>	<i>Fences have been added to the landscape area between the parking lot and the alley at the south end of the site, and adjacent to the drive-through near the alley. A concept design was brought to the August 22 hearing. See conditions of approval regarding final design for the fence areas. A low seating wall may be appropriate around the existing specimen oak at the northeast corner.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3e	<b>e. All roof projections including, roof-mounted mechanical equipment, such as heating and air conditioning units, but excluding solar panels and Wind Energy Systems that have received a Conditional Use Permit, shall be shielded and screened from view from the ground level of on-site parking areas, adjacent public streets and adjacent properties.</b>
			<b>Staff Comments</b>	<i>Staff has requested the locations of roof-mounted equipment, which will be shown as part of the building permit drawings.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)3f	<b>f. The hardware associated with alternative energy sources shall be incorporated into the building’s design and not detract from the building and its surroundings.</b>
			<b>Staff Comments</b>	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3g	<b>g. All ground-mounted mechanical equipment, including heating and air conditioning units, and trash receptacle areas shall be adequately screened from surrounding properties and streets by the use of a wall, fence, or landscaping, or shall be enclosed within a building.</b>
			<b>Staff Comments</b>	<i>Heating and air conditioning will be interior; the trash area will be screened and relocated if feasible.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3h	<b>i. All service lines into the subject property shall be installed underground.</b>
			<b>Staff Comments</b>	<i>All service lines are underground. The location of the pad-mounted transformer should be shown on the plans.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3i	<b>j. Additional appurtenances shall not be located on existing utility poles.</b>
			<b>Staff Comments</b>	<i>The existing wooden street pole is used for banners which are hung over Main Street. In order to accommodate the least disturbance to the existing specimen city street tree (Oak), the Commission concurred with a staff recommendation to relocate this banner pole to the southern corner of the block by the Hitchrack. A city street light standard with a banner extension would be installed across the street from an existing pole which would be used as a banner pole location, costs of this to be borne by the applicant.</i>

**4. Landscaping: 17.06.080(A)4, items (a) thru (n)**

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)4a	<b>a. Only drought tolerant plant species and/or xeriscape specific plant materials shall be used, as specified by the Hailey Landscaping Manual or an approved alternative.</b>
			<b>Staff Comments</b>	<i>Plant materials will be appropriate for the environment. Staff has concurred that the species identified are appropriate. Location of the trees is compatible with their recommendations. Staff recommends that the size of the proposed street trees be increased to 2-1/2-3” caliper, minimum.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)4b	<b>b. All plant species shall be hardy to the Zone 4 environment.</b>
			<b>Staff Comments</b>	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)4c	<b>c. At a minimum, a temporary irrigation system that fully operates for at least two complete growing seasons is required in order to establish drought</b>

				<b>tolerant plant species and/or xeriscape specific plant materials. Features that minimize water use, such as moisture sensors, are encouraged.</b>
			<i>Staff Comments</i>	<i>An automatic drip irrigation system on a timer is planned.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)4d	<b>d. Landscaped areas shall be planned as an integral part of the site with consideration of the urban environment. A combination of trees shrubs, vines, ground covers and ornamental grasses shall be used. New landscaped areas having more than 10 trees, a minimum of 10% of the trees shall be at least 4-inch caliper, 20% shall be at least 3-inch caliper, and 20% shall be at least 2½ inch caliper and a maximum of 20% of any single tree species may be used in any landscape plan (excluding street trees). New planting areas shall be designed to accommodate typical trees at maturity. Buildings within the LI and SCI-I zoning district are excluded from this standard.</b>
			<i>Staff Comments</i>	<i>See earlier discussion in this report of the city chokecherry trees and the signature city oak tree, and the specimen maple interior to the lot.</i>  <i>Regarding the remainder of the landscape plan, a variety of trees and shrubs are shown, and annual beds. As noted earlier, a pedestrian path from the bus stop through the landscape bed to the parking area is desirable.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4e	<b>e. Seasonal plantings in planter boxes, pots, and/or hanging baskets shall be provided to add color and interest to the outside of buildings in the LI and SCI-I zoning districts.</b>
			<i>Staff Comments</i>	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)4f	<b>f. Plantings for pedestrian areas within the B, LB, TN and SCI-O zoning districts shall be designed with attention to the details of color, texture and form. A variety of trees, shrubs, perennials, ground covers and seasonal plantings, with different shapes and distinctive foliage, bark and flowers shall be used in beds, planter boxes, pots, and/or hanging baskets.</b>
			<i>Staff Comments</i>	<i>Proposed landscaping is varied. A low seating wall located ½ to 2/3 out from the drip line of the existing signature City Oak tree could provide a nice pedestrian amenity on Main street.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)4g	<b>g. Storm water runoff should be retained on the site wherever possible and used to irrigate plant materials.</b>
			<i>Staff Comments</i>	<i>Runoff is within planting beds.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)4h	<b>h. A plan for maintenance of the landscaping areas is required to ensure that the project appears in a well maintained condition (i.e., all weeds and trash removed, dead plant materials removed and replaced).</b>
			<i>Staff Comments</i>	<i>The applicant will be responsible for maintaining plant material in healthy condition, with the exception of City street trees. The Commission has required the addition of a trash receptacle near the planned bus shelter.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4i	<b>i. Retaining walls shall be designed to minimize their impact on the site and the appearance of the site.</b>
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4j	<b>j. Retaining walls shall be constructed of materials that are utilized elsewhere on the site, or of natural or decorative materials.</b>
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4k	<b>k. Retaining walls, where visible to the public and/or to residents or employees of the project, shall be no higher than four feet or terraced with a three foot horizontal separation of walls.</b>
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4l	<b>l. Landscaping should be provided within or in front of extensive retaining walls.</b>
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4m	<b>m. Retaining walls over 24" high may require railings or planting buffers for</b>

				<b>safety.</b>
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4n	n. Low retaining walls may be used for seating if capped with a surface of at least 12 to 16 inches wide.
			<i>Staff Comments</i>	

### Additional Design Review Requirements for Non-Residential Buildings Located within B, LB, or TN

#### 1. Site Planning: 17.06.080(B)1, items (a) thru (b)

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(B)1a	a. The site shall support pedestrian circulation and provide pedestrian amenities. Sidewalks shall be provided along building fronts.
			<i>Staff Comments</i>	A sidewalk currently exists on the building front, and will be improved to be 10' wide, with trees at the back of the sidewalk. This is a preferred design due to the bus stop and grade considerations. Mountain Rides has provided comments on the bus stop, as described in the letter attached to the August 22 staff report. This is a heavily used transit location. The Commission required that a pedestrian path be added through the landscape area from the parking lot to the bus stop, and a trash receptacle.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(B)1b	b. Wider sidewalks are encouraged to provide additional amenities such as seating areas and bicycle racks.
			<i>Staff Comments</i>	Wider sidewalks are proposed along the length of Main Street, and at the corner. Bike racks are not shown, and are required to be added near the bus stop and near the building.

#### 2. Building Design: 17.06.080(B)2, items (a) thru (c)

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(B)2a	a. The main facade shall be oriented to the street. The main entrance(s) to the building shall be located on the street side of the building. If the building is located on a corner, entrances shall be provided on both street frontages. If the design includes a courtyard, the main entrance may be located through the courtyard. Buildings with more than one retail space on the ground floor are encouraged to have separate entrances for each unit.
			<i>Staff Comments</i>	The entrance to the building faces both corners.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(B)2b	b. Multi-unit structures shall emphasize the individuality of units or provide visual interest by variations in roof lines or walls or other human scale elements. Breaking the facades and roofs of buildings softens the institutional image which may often accompany large buildings.
			<i>Staff Comments</i>	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(B)2c	c. Building designs shall maximize the human scale of buildings and enhance the small town "sense of place". This can be achieved by utilizing voids and masses, as well as details, textures, and colors on building facades. Human scale can also be achieved by incorporating structural elements such as

				colonnades and covered walkways, overhangs, canopies, entries, and landscaping. Particular attention should be paid to creating interest at the street level.
			<i>Staff Comments</i>	<i>Pedestrian access is from sidewalks and the perennial landscape area to the south of the building.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(B)2d	d. Buildings that exceed 30 feet in height, the entire roof surface shall not project to the highest point of the roof. The Commission shall review building height relative to the other dimensions of width and depth combined with detailing of parapets, cornices, roof, and other architectural elements.
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(B)2e	e. Livable outdoor spaces in multi-story buildings that create pleasing elements and reduce the mass of taller buildings are encouraged.
			<i>Staff Comments</i>	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(B)2f	f. Fire department staging areas shall be incorporated into the design elements of the building.
			<i>Staff Comments</i>	<i>The building is within 150' of the street.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(B)2g	g. New buildings adjacent to residential areas shall be designed to ensure that building massing and scale provide a transition to adjoining residential neighborhoods. Possible mitigation techniques include, but are not limited to the following: <ul style="list-style-type: none"> <li>i. Locating open space and preserving existing vegetation on the edge of the site to further separate the building from less intensive uses;</li> <li>ii. Stepping down the massing of the building along the site's edge; and</li> <li>iii. Limiting the length of or articulating building facades to reflect adjacent residential patterns</li> </ul>
			<i>Staff Comments</i>	
<b>Compliant</b>				
<b>Yes</b>	<b>No</b>	<b>N/A</b>		
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>		

**6A.6 Criteria.**

- A. The Commission or Hearing Examiner shall determine the following before approval is given:**
1. The project does not jeopardize the health, safety or welfare of the public.
  2. The project conforms to the applicable specifications outlined in the Design Review Guidelines, as set forth herein, applicable requirements of the Zoning Ordinance, and City Standards.
- B. Conditions. The Commission or Hearing Examiner may impose any condition deemed necessary. The Commission or Hearing Examiner may also condition approval of a project with subsequent review and/or approval by the Administrator or Planning Staff. Conditions which may be attached include, but are not limited to those which will:**
1. Ensure compliance with applicable standards and guidelines.
  2. Require conformity to approved plans and specifications.
  3. Require security for compliance with the terms of the approval.

4. **Minimize adverse impact on other development.**
5. **Control the sequence, timing and duration of development.**
6. **Assure that development and landscaping are maintained properly.**
7. **Require more restrictive standards than those generally found in the Zoning Ordinance.**

**C. Security. The applicant may, in lieu of actual construction of any required or approved improvement, provide to the City such security as may be acceptable to the City, in a form and in an amount equal to the cost of the engineering or design, materials and installation of the improvements not previously installed by the applicant, plus fifty percent (50%), which security shall fully secure and guarantee completion of the required improvements within a period of one (1) year from the date the security is provided.**

1. **If any extension of the one year period is granted by the City, each additional year, or portion of each additional year, shall require an additional twenty percent (20%) to be added to the amount of the original security initially provided.**
2. **In the event the improvements are not completely installed within one (1) year, or upon the expiration of any approved extension, the City may, but is not obligated, to apply the security to the completion of the improvements and complete construction of the improvements.**

The following conditions are suggested to be placed on any approval of this application:

- a) All applicable Fire Department and Building Department requirements shall be met.
- b) Any change in use or occupancy type from that approved at time of issuance of Building Permit may require additional improvements and/or approvals. Additional parking may also be required upon subsequent change in use, in conformance with Hailey's Zoning Ordinance at the time of the new use.
- c) All City infrastructure requirements shall be met. Detailed plans for all infrastructure to be installed or improved at or adjacent to the site shall be submitted for Department Head approval and shall meet City Standards where required. Infrastructure to be completed at the applicant's sole expense include, but will not be limited to, the following requirements and improvements: 1) applicant will either utilize both water mains (i.e. one for in the building and the other for irrigation) or abandon one of them at the water main, and 2) if water main lines within the alley are less than six (6) feet deep, the applicant shall install insulating material (blue board insulation or similar material) for each and every individual water service line and main line between and including the subject property and the nearest public street, as recommended by the City Engineer, and 3) conditions related to City street trees as outlined herein.
- d) The project shall be constructed in accordance with the application or as modified by these Findings of Fact, Conclusions of Law and Decision.
- e) All new and existing exterior lighting shall comply with the Outdoor Lighting Ordinance. A lighting plan and photometric plan should be submitted for review/approval by staff and one Planning and Zoning Commissioner.

- f) Except as otherwise provided, all the required improvements shall be constructed and completed, or sufficient security provided as approved by the City Attorney, before a Certificate of Occupancy can be issued.
- g) This Design Review approval is for the date the Findings of Fact are signed. The Planning & Zoning Administrator has the authority to approve minor modifications to this project prior to, and for the duration of a valid Building Permit.
- h) The specimen City street tree on the northeast corner (white oak) shall be retained. Existing grade shall be protected for a minimum distance of  $\frac{1}{2}$  to  $\frac{2}{3}$  of the drip line, both during construction and as a component of the final design. No disturbance within this area is permitted during construction, including no storage of construction materials. Irrigation to this tree shall be retained during construction.
- i) Irrigation to the replacement City street trees shall be to City standards. Power shall be provided to each tree, or as otherwise approved by the City.
- j) Street trees should be a minimum of 2-1/2 to 3" caliper. The new street trees should be placed in planter beds of minimum dimensions approved by the City, with adequate structural soil (planting medium) incorporated into the design.
- k) The curb cut on Main Street shall be eliminated.
- l) The 19 stall parking lot on Main Street is in excess of City regulations, but has been found to meet the standards herein, so long as this parking area is permitted to be used by transit riders, and for events at Wertheimer Park. The parking stall shall be modified as follows: 1) A pedestrian path shall be provided connection this parking area to the Main Street sidewalk/bus shelter area. 2) One of the two handicap stall shown in the portion of the parking lot closest to the building shall be relocated into the southern portion of the lot. 3) A trash receptacle shall be added in the vicinity of the bus shelter.
- m) The bus shelter as shown on the revised drawings is a requirement of the site plan, with the following modifications: 1) adequate lighting shall be shown (it is unclear from the current plans how it will be lit); 2) The bus pull-out area needs to have a no-parking, red curb zone of at least 80' along the Main St curb face, at the bus shelter location; and 3) the current roof design may cause snow to slide off onto waiting passengers: the pitch of the roof shall be modified an approved by staff.
- n) A letter from Clearcreek Disposal shall be provided stating that the location and design is adequate for dumpster pickup.
- o) The pad-mounted transformer shall be shown on the site plan.
- p) Rooftop equipment (if any) shall be shown on a roof plan.
- q) A City standard street light with a banner pole extension shall be added on the corner of Main and Maple, in the vicinity of the Hitchrack Motel, location to be determined by City Public Works Department.
- r) Details on the proposed landscape screening fence between the southern parking lot and the alley shall be submitted for review/approval by staff and one Planning and Zoning Commissioner.
- s) Two bike racks shall be added: one to serve the bank building, and one to serve the bus shelter.

- t) The dumpster location shall be relocated closer to the corner of Chestnut Street, with the final design to be approved by staff and the Chair of the P & Z, providing that the location will be workable for Clearcreek Disposal with regards to the access and the overhead power lines.
- u) The final fence design shall be approved by staff and the Chair of the P & Z.
- v) A construction staging plan shall be submitted for final approval by staff and the Chair of the P & Z.

A party aggrieved by a final decision of the Commission may appeal in writing any final decision by filing a Notice of Appeal with the Hailey City Clerk within fifteen (15) days from the date of the decision.

Signed this \_\_\_\_\_ day of \_\_\_\_\_, 2016.

\_\_\_\_\_  
Janet Fugate, Chair

Attest:

\_\_\_\_\_  
Robyn Davis, Community Development Assistant

**Return to Agenda**

## **FINDINGS OF FACT AND CONCLUSIONS OF LAW AND DECISION**

On August 22, 2016, the Hailey Planning & Zoning Commission considered a 6-month review of an application by United Parcel Service (UPS) for a Conditional Use Permit application for a temporary peak car to be used during the holiday season at the Hailey Distribution Center, 111 South River Street (Lot 1-7, Block 31, Hailey Townsite) in the Business (B) and Townsite Overlay (TO) Zoning Districts. According to §5.4 of the Zoning Ordinance, a Temporary Storage Container requires a Conditional Use Permit in the Business (B) District. The Commission, having been presented with all information and testimony in favor and in opposition to the proposal, hereby makes the following Findings of Fact, Conclusions of Law and Decision.

### **Notice**

Notice for the public hearing on August 22, 2016 was sent to the Mt. Express on July 29, 2016 and published in the Mountain Express on August 3, 2016. Notices were mailed to the adjoining property owners on August 22, 2106 and the property was posted on August 15, 2016.

### **Application and Procedural History**

The Planning and Zoning Commission approved a Conditional Use Permit application for a temporary peak car to be used during the holiday season at the Hailey Distribution Center on November 9, 2015. According to §5.4 of the Zoning Ordinance, a Temporary Storage Container requires a Conditional Use Permit in the Limited Business (LB) District. The Conditions of Approval required that the site and uses be reviewed for compliance within 6 months of the approval date. Staff has been working with the applicant to find an available PZ hearing date within the 6-month window, but the review has been delayed until August 22 due to the busy summer PZ meeting schedule. The Conditions of Approval are as follows:

- a) All Fire Department and Building Department requirements shall be met in regard all maintenance, administrative, and other functions of this facility.
- b) Trucks shall not enter or exit the site within 50' of the street intersection, and no trucks or vehicles should be parked at the northeast corner of the site, nor the City right-of-way in this vicinity to preserve site visibility.
- c) The applicant shall comply with the UPS Company idling rules and procedures at all times.
- d) The site and uses shall be reviewed for compliance with these findings six (6) months from the date of these findings of fact.

The temporary structure was used through the winter months, and removed in late March, 2016. Comments in the staff report relate only to the Conditions of Approval above. Upon receipt of these Findings of Fact, Conclusions of Law and Decision, the applicant may request a regulatory takings analysis pursuant to Idaho Code § 67-8003.

<b>General Requirements for all Conditional Use Permits</b>				
<b>Compliant</b>			<b>Standards and Commission Findings</b>	
Yes	No	N/A	City Code	City Standards and <i>Commission Findings</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.2.2	<p><b>Complete Application:</b></p> <p><b>11.2.2 The application shall include at least the following information:</b></p> <ul style="list-style-type: none"> <li>a. Name, address, and phone number of the applicant.</li> <li>b. Proof of interest in the subject property by the applicant, such as a deed, contract of sale, option to purchase, or lease agreement.</li> <li>c. Legal description of the subject property, including street address.</li> <li>d. Description of existing use.</li> <li>e. Zoning district of subject property.</li> <li>f. Description of proposed conditional use.</li> <li>g. A plan of the proposed site for the conditional use showing the location of all buildings, parking and loading areas, traffic access and traffic circulation, open spaces, easements, existing and proposed grade, energy efficiency considerations, landscaping, exterior lighting plan as required by Article VIII B of this Ordinance, refuse and service areas, utilities, signs, property lines, north arrow, and rendering of building exteriors, where applicable.</li> <li>h. A narrative statement evaluating the effects on adjoining property, the effect of such elements as noise, glare, odor, fumes, and vibration on adjoining property.</li> <li>i. A narrative statement identifying surrounding land uses and discussing the general compatibility of the proposed use with adjacent and other properties in the district.</li> <li>j. A narrative discussion of the relationship of the proposed use to the Comprehensive Plan.</li> <li>k. A list of the names and addresses of all property owners and residents within three hundred (300) feet of the external boundaries of the land being considered.</li> <li>l. Any other information as requested by the Administrator to determine if the proposed conditional use meets the intent and requirements of this Article.</li> <li>m. A fee established in a separate ordinance approved by the Council.</li> </ul> <p>Chronology of UPS on the subject property is as follows:</p> <p>1988- Letter from the Hailey Planning Department stating that permanent structures are required for the use to conform to the Hailey Zoning Code- use was considered conforming.</p> <p>1996- Letter from Carl Hjelm to UPS outlining Design Review Process- no mention of the use being nonconforming</p> <p>2001 City adopted SCI-I Zone with a permitted use of "Assembly and packaging firms" prior to Grotto letter to UPS. Not known when Assembly and packaging firms" was changed to the currently language in the Matrix, "Parcel Delivery and Shipping Services".</p> <p>2001- Letter from Kathy Grotto approving an addition to the building- no mention of the use being nonconforming</p> <p>District Use Matrix as adopted has a "blank" in the square "Parcel Delivery and Shipping Services"; history with City files would indicate that it was permitted; at the very least it is ambiguous. City Attorney advised the Commission that, in the case of an ambiguous use, the courts would rule in favor of the applicant. The Commission should take up the issue of the use, "Parcel Delivery and Shipping Services" at another time in a duly noticed meeting so as to remedy the "blank" in the District Use Matrix.</p> <p>Use approved in this application is an accessory use to the primary use: Temporary Structure:</p> <p style="text-align: center;">"Temporary Structure is any building, modular unit or structure that is intended</p>

				<p>for any use for a period of not more than one (1) year, excluding construction trailers or other structures erected sole in conjunction with a construction project.”</p> <p>Item H was not submitted in writing, and was discussed in the meeting after and overview by the applicant’s representative. It was described by the applicant that the Hailey Distribution Center has seen an increase in activity, and that 70% of the business occurs in peak times such as the winter holidays. These increases would be present with or without the Temporary Peak Car. The Temporary Peak Car is to increase the comfort level (warmth) of the employees during winter month. In warmer climates, the same activities are accomplished with exterior tables.</p> <p>July 2106: The Commission voted 3-1 to indicate that this use is “Not Permitted” in the District Use Matrix. The City Attorney will provide additional comments in the near future regarding the implications of this code change with regards the Temporary Structure.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>Department and Boards/Commissions Comments</b></p>	<p><b>Engineering:</b></p> <ul style="list-style-type: none"> <li>- The City Engineer previously recommended that trucks should not enter or exit the site within 50’ of the street intersection, and those trucks to do not park at the northeast corner of the site to preserve site visibility. Signs have been placed to accomplish this requirement. The Streets Department has no concerns regarding the signs as installed.</li> </ul> <p>Dust was cited by neighbors as an issue during the hearing. Dust abatement was performed this summer.</p> <p>Diesel trucks were discussed.</p>
				<p><b>Life/Safety:</b></p> <ul style="list-style-type: none"> <li>- <b>Police Department</b> <ul style="list-style-type: none"> <li>o No concerns</li> </ul> </li> <li>- <b>Fire Department</b> <ul style="list-style-type: none"> <li>No concerns.</li> </ul> </li> </ul>
				<p><b>Water and Sewer:</b></p> <ul style="list-style-type: none"> <li>-</li> </ul>
				<p><b>Building:</b></p> <ul style="list-style-type: none"> <li>- No concerns.</li> </ul>
				<p><b>Streets:</b></p> <ul style="list-style-type: none"> <li>- A seasonal drainage issues exists on the northwest property corner. A Catch- basin and drywell were discussed in the public right of way, which would need to be installed in dry weather.</li> </ul>
				<p><b>Boards and Committees:</b></p> <ul style="list-style-type: none"> <li>- No concerns.</li> </ul>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p><b>8.2 Signs</b></p>	<p><b>8.2 Signs: The applicant is hereby advised that a sign permit is required for any signage exceeding four square feet in sign area. Approval of signage areas or signage plan in Design Review does not constitute approval of a sign permit.</b></p>
			<p><i>Commission Findings</i></p>	<ul style="list-style-type: none"> <li>- <i>No signs are proposed.</i></li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>8B.4.1</b></p>	<p><b>8B.4.1 General Standards</b></p>

			Outdoor Lighting Standards	<p>a. All exterior lighting shall be designed, located and lamped in order to prevent:</p> <ol style="list-style-type: none"> <li>1. Overlighting;</li> <li>2. Energy waste;</li> <li>3. Glare;</li> <li>4. Light Trespass;</li> <li>5. Skyglow.</li> </ol> <p>b. All non-essential exterior commercial and residential lighting is encouraged to be turned off after business hours and/or when not in use. Lights on a timer are encouraged. Sensor activated lights are encouraged to replace existing lighting that is desired for security purposes.</p> <p>c. Canopy lights, such as service station lighting shall be fully recessed or fully shielded so as to ensure that no light source is visible from or causes glare on public rights of way or adjacent properties.</p> <p>d. Area lights. All area lights are encouraged to be eighty-five (85) degree full cut-off type luminaires.</p> <p>e. Idaho Power shall not install any luminaires after the effective date of this Article that lights the public right of way without first receiving approval for any such application by the Lighting Administrator.</p>
			Commission Findings	<ul style="list-style-type: none"> <li>• All new and existing lighting shall comply with Hailey's Outdoor Lighting Ordinance.</li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	9.4.8 A On-site Parking Req.	<p>See Section 9.4 for applicable code.</p> <ul style="list-style-type: none"> <li>- Require 1 space for 1,000 square feet, or, if the site is considered warehouse and storage, 1 space per every (full time) employee, whichever is greater.</li> </ul>
			Commission Findings	<p><i>The site is large enough to accommodate 1 space per employee (13 full time) and 1-2 customer spaces based on building size of less than 1,000 square feet.</i></p> <p><i>The parking has never been striped, as it is gravel. Staff and the City Engineer believe the best long-term solution to organize parking and pedestrian/bike activities are curb, gutter and sidewalk. That requirement is not triggered by a Temporary Storage container.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	9.4.8 (B)	<p><b>B. Where alleys exist, access to on-site parking for any non-residential use or for any multifamily dwelling of three or more units shall be from the alley. Parking areas adjacent to alleys may be designed to allow a vehicle to back from the parking area into the alley.</b></p>
			Commission Findings	<p><i>Many, but not all of the truck traffic use the alley, which is also not paved.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	9.4.8 (C)	<p><b>C. If the site is not serviced by an alley, access shall be from a single approach to the street to confine vehicular/pedestrian conflict to limited locations, allow more buffering of the parking area and preserve the street frontage for pedestrian traffic.</b></p>
			Commission Findings	<p><i>It is not possible to meet this standard without curb, gutter and sidewalk, which is not required of this permit.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	9.4.8 (D)	<p><b>D. Access for on-site parking areas or loading spaces shall be located in such a way that any vehicle entering or leaving such area shall be clearly visible by a pedestrian or motorist approaching the access or driveway from a public or private street.</b></p>
			Commission Findings	<p><i>The site is nonconforming with regards to this standard.</i></p>

				<i>The City Engineer recommends that no trucks be permitted to enter or exit the site from within 50' of the intersection, and they also be restricted from parking in this area. The site has been signed to meet this requirement.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	9.4.8 (E)	E. Access for subdivisions shall be provided in accordance with standards set forth in Section 4 of the Subdivision Ordinance.
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li>• <i>Not applicable</i></li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	9.4.8 (F)	F. Parking areas containing no more than two (2) parking spaces in any zoning district or parking areas within the LR, GR, TN, TI and LI Districts may be designed to allow a vehicle to back from the parking area into the public right-of-way.
			<i>Commission Findings</i>	<i>The site is nonconforming with regards to this standard. However, most customers do not back in and out of the parking lot into the street due to the high traffic volumes on River Street.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	9.4.8 (G)	G. Parking areas for residential uses only may be designed to allow required parking spaces for one vehicle to deny access to another vehicle, thus “stacking” the parking area. For non-residential uses, stacked parking may be allowed only for additional spaces that may be provided in excess of the required number of parking spaces.
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li>• <i>Not applicable</i></li> </ul>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Article 5.4: Bulk Requirements	<b>Limited Business (B) District:</b> <ul style="list-style-type: none"> <li>- <b>Minimum Lot Size: 0</b></li> <li>- <b>Maximum Lot Width: 0</b></li> <li>- <b>Maximum Building Height: 35</b></li> <li>- <b>Front Setback: 0 ft</b></li> <li>- <b>Side and Rear Yard Setback: 0 feet</b></li> </ul>
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li>- <i>Existing building meets all bulk requirements, as does the temporary storage container.</i></li> </ul>
Section 11.4 Criteria for Review of Conditional Use Permits				
Compliant			Standards and Commission Findings	
Yes	No	N/A	City Code	City Standards and <i>Commission Findings</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.1	<b>Compliance with the Comprehensive Plan</b> <b>11.1 Purpose.</b> The City of Hailey recognizes that certain uses possess unique and special characteristics with respect to their location, design, size, method of operation, circulation, and public facilities. In order to protect the public welfare and promote conformance with the Comprehensive Plan, conditional use permits are required for such uses upon review by the Commission.
			<i>Commission Findings</i>	<i>This application complies with the following goals and objectives of the Comprehensive Plan</i> <ul style="list-style-type: none"> <li>• <i>Goal 6.1: Economic Development“</i></li> </ul>

				<ul style="list-style-type: none"> <li>○ <i>Encourage a diversity of economic development opportunities within Hailey.”</i></li> <li>○ <i>This use provides 13 full-time jobs and up to 25 seasonal jobs</i></li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.4.1(a)	<p><b>11.4.1 The Commission or Hearing Examiner shall review the particular facts and circumstances of each proposed conditional use in terms of the following standards and, if approved, shall find adequate evidence showing that such use at the proposed location:</b></p> <p><b>a. Will, in fact, constitute a conditional use as established for the zoning district involved; and</b></p>
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li>● <i>According to §5.4 of the Zoning Ordinance, Temporary Storage Containers currently require a Conditional Use Permit in the Business (B) zone. The temporary storage container was removed in the spring of 2016.</i></li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.4.1(b)	<p><b>b. Will be designed, constructed, operated, and maintained to be harmonious and appropriate in appearance with the existing or intended character of the general vicinity, and that such use will not change the essential character of the same area;</b></p>
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li>● <i>The existing nonconforming use has been in place for 27 years. It is more industrial in nature than some other uses in the area, although business uses in this area of River Street are mixed. The proposed temporary storage container does not materially detract nor change the character of the area.</i></li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.4.1(c)	<p><b>c. Will not be hazardous or disturbing to existing or future neighboring uses;</b></p>
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li>● <i>Traffic to and from the building is seasonally high. However, many business uses could have greater vehicle trips if the site were built out for retail/commercial uses.</i></li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.4.1(d)	<p><b>d. Will be served adequately by essential public facilities and services such as highways, streets, police and fire protection, and drainage structure. Agencies responsible for the establishment of the proposed use shall be able to provide adequately any such service; and</b></p>
			<i>Commission Findings</i>	<p><i>A catch basin and drywell are needed in the public right of way at the northwest corner of the lot to mitigate drainage issues resulting from snow plowing on the UPS site. (City staff installed a French drain in this area, but it did not remedy the problem). The catch basin/drywell can be installed if needed.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.4.1(e)	<p><b>e. Will not create excessive additional requirements at public cost for public facilities and services; and</b></p>
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li>● <i>At this time, no additional cost will be incurred from any public agencies for this facility to function and operate.</i></li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.4.1(f)	<p><b>f. Will not involve uses, activities, processes, materials, equipment, or conditions of operation that will be detrimental to any persons, property, or the general welfare by</b></p>

				reason of excessive production of traffic, noise, smoke, fumes, glare, dust, odors, vibration, water or air pollution, or safety hazards; and
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li>No externalities are anticipated by this use.</li> <li>This application has been reviewed by all city departments and there have been no issues.</li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.4.1(g)	g. Will have vehicular approaches to the property which shall be designed so as not to create an interference with traffic on surrounding public thoroughfares;
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li></li> </ul>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	11.4.1(h)	b. Will be designed, constructed, operated, and maintained to be harmonious and appropriate in appearance with the existing or intended character of the general vicinity, and that such use will not change the essential character of the same area;
			<i>Commission Findings</i>	<ul style="list-style-type: none"> <li>Proposed property does not contain a natural, scenic, or historic feature or structure. The temporary structure is to the rear, and does not change the essential character of the area.</li> </ul>

**11.6 Conditions.**

The Commission or Hearing Examiner may impose any conditions which it deems necessary to secure the purpose of City regulations and give effect to the Comprehensive Plan. Conditions which may be attached include, but are not limited to those which will:

- 11.6.1 Require conformity to approved plans and specifications.
- 11.6.2 Require or restrict open spaces, buffer strips, walls, fences, signs, concealing hedges, landscaping and lighting.
- 11.6.3 Restrict volume of traffic generated, require off-street parking, and restrict vehicular movements within the site and points of vehicular ingress and egress or other conditions related to traffic.
- 11.6.4 Require performance characteristics related to the emission of noise, vibration and other potentially dangerous or objectionable elements.
- 11.6.5 Limit time of day for the conduct of specified activities.
- 11.6.6 Require guarantees such as performance bonds or other security for compliance with the terms of the approval.
- 11.6.7 Require dedications and public improvements on property frontages.
- 11.6.8 Require irrigation ditches, laterals, and canals to be covered or fenced.
- 11.6.9 Minimize adverse impact on other development.
- 11.6.10 Control the sequence, timing and duration of development.
- 11.6.11 Assure that development is maintained properly.
- 11.6.12 Designate the exact location and nature of development.
- 11.6.13 Require the provision for on-site or off-site public services.

- 11.6.14 Require more restrictive standards than those generally found in this Ordinance.**
- 11.6.15 Mitigate foreseeable social, economic, fiscal and environmental effects.**
- 11.6.16 Set a limit on the duration of the permit when deemed necessary.**
- 11.6.17 Allow for subsequent periodic review.**

**The Commission may impose any conditions that are deemed necessary to secure the purpose of City ordinances and give effect to the Comprehensive Plan. Conditions including but not limited to those set forth in Section 11.6 may be placed on any approval.**

### **Summary**

Section 11.1 of the Hailey Zoning Ordinance states that “the City of Hailey recognizes that certain uses possess unique and special characteristics with respect to their location, design, size, method of operation, circulation, and public facilities. In order to protect the public welfare and promote conformance with the Comprehensive Plan, conditional use permits are required for such uses upon review by the Commission.”

Conditional Use Permits are subject to review and revocation pursuant to Section 11.9 of the Hailey Zoning Ordinance. This statement will be included in the Findings of Fact, Conclusions of Law, and Decision for any CUP approved by the Commission.

By ordinance, the Commission is required to make a decision to approve, conditionally approve, or deny the application within forty-five (45) days after conclusion of the public hearing and issue its decision together with the reasons therefore. The Commission is required to review the application, all supporting documents and plans, and Section 11 of the Zoning Ordinance, in making their decision.

### **CONCLUSIONS OF LAW**

Based upon the above Findings of Fact, the Commission makes the following Conclusions of Law:

1. Adequate notice, pursuant to Zoning Ordinance No. 532, Section 11 was given.
2. The project is in general conformance with the Hailey Comprehensive Plan.
3. The project does not jeopardize the health, safety, or welfare of the public.
4. Upon compliance with the conditions set forth, the project conforms to the applicable standards of Section 11 , other Articles of the Zoning Ordinance and City Standards

The application for Conditional Use Permit submitted by UPS for a temporary peak car to be used during the holiday season at the Hailey Distribution Center located at 111 South River Street (Lot 1-7, Block 31, Hailey Townsite) is hereby approved subject to the following conditions:

- a) All Fire Department and Building Department requirements shall be met in regard all maintenance, administrative, and other functions of this facility.
- b) Trucks shall not enter or exit the site within 50' of the street intersection, and no trucks or vehicles should be parked at the northeast corner of the site, nor the City right-of-way in this vicinity to preserve site visibility.
- c) The applicant shall comply with the UPS Company idling rules and procedures at all times.

A party aggrieved by a final decision of the Commission may appeal in writing any final decision by filing a Notice of Appeal with the Hailey City Clerk within fifteen (15) days from the date of the decision.

Signed this \_\_\_\_\_ day of \_\_\_\_\_, 2016.

\_\_\_\_\_  
Janet Fugate, Chair

Attest:

\_\_\_\_\_  
Robyn Davis, Community Development Coordinator

**Return to Agenda**

# MEMORANDUM

TO: Hailey City Planning and Zoning Commission

FROM: Ned C. Williamson

DATE: September 12, 2016

RE: Request for Reconsideration – Wise Guy

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The Commission will be considering a Request for Reconsideration on September 12, 2016. In 2013, the Idaho legislature amended the Local Land Use Planning Act, *Idaho Code §§ 67-6501 et seq.*, to require a reconsideration procedure. *See Idaho Code § 67-6535(2)*. Following this amendment, Hailey adopted a reconsideration procedure which essentially restates the state law procedure in our zoning ordinance. *See Municipal Code § 17.03.050(D)*.

The applicant is requesting reconsideration of the condition imposed by the Commission that a Linden tree located in the right-of-way remain. As part of its request for reconsideration, the applicant is requesting that the Commission consider new evidence attached to the letter from its attorney, Lee Ritzau. Among other things, the new evidence consists of letters/reports from an arborist and two contractors.

I believe the Commission should first decide whether it believes it is appropriate to consider the new evidence. If you decide not to consider the new evidence, then you can proceed with a decision on the request for reconsideration without a public hearing. But if you decide to consider the new evidence, then you should continue the hearing to a future date to allow the public an opportunity to testify.

Our zoning ordinance is silent on the standards to be used when deciding whether to consider new evidence. At a minimum, the new evidence should be relevant and the applicant should show cause why it was not submitted at the last hearing. In my opinion, I believe the new evidence is clearly relevant. To show cause, the applicant states “given the removal and replacement language in the Hailey City Code and the Hailey comprehensive Plan, removal of the middle Street Tree would be permitted.”

In this case, I would suggest that you consider the new evidence and conduct a subsequent public hearing. At this stage of the proceedings, I would suggest that we decide this issue based on substantive, not procedural reasons. If you agree with this approach, you should discuss the request to consider new evidence during the September 12 hearing. If you decide to consider new evidence, then I would suggest making a motion to continue the request for reconsideration to a date certain. At the later hearing, the applicant can present the new evidence and make its case to delete or modify the contested condition and then the public can comment. At this later

Hailey City Planning and Zoning Commission  
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hearing, I would think the Hailey Tree Committee can offer its thoughts and recommendations on this request for reconsideration.

If you have any questions, please contact me. Thank you.

cc: Lisa Horowitz

LUBOVISKI, WYGLE, FALLOWFIELD & RITZAU, P.A.  
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September 6, 2016

Ned Williamson, City Attorney  
Lisa Horowitz, Community Development Director  
Hailey Planning and Zoning Commission  
City of Hailey  
115 Main Street South  
Hailey, Idaho 83333  
Via Email: [ned@williamsonlaw.net](mailto:ned@williamsonlaw.net) and [lisa.horowitz@haileycityhall.org](mailto:lisa.horowitz@haileycityhall.org)

Re: Hailey Planning and Zoning Request for Reconsideration Wise Guy Pizza, 411 N. Main Street (S ½ of Lot 3, Lots 4 & 5, Block 56, Hailey Townsite).

Dear Ned, Lisa, Chair Fugate, and Commissioners Scanlon, Smith, Pogue and Engelhardt:

I represent EH&FB, LLC/Wise Guy Pizza Pie which is the owner of 411 N. Main Street, Hailey, Idaho and the applicant for Design Review approval to build a 3,003 square foot pizzeria at this location.

My client is requesting reconsideration of the Hailey Planning and Zoning Commission's July 11, 2016 Design Review approval which permits construction of a pizzeria upon the condition that all three City Street Trees shall be retained. My client is only requesting reconsideration of the requirement the middle Street Tree be retained. The discussion regarding the retention of the Street Trees occurs at page 8 of 12 of the Findings of Fact Design Review (hereinafter "FOF") as well as page 12 of 12, subparagraph g) of the FOF.

The Street Tree at issue is the middle of three trees at this location. The spacing relating to the three Street Trees is as follows: the north tree; the middle tree at issue (which is approximately 25 feet south of the north tree); and the south tree (which is approximately 27 feet south of the middle tree).

This Request for Reconsideration is made pursuant to Idaho Code § 67-6535(2)(B) and Hailey City Code § 17.030.050(D). Hailey City Code § 17.06.080(A)4d and/or Hailey City Code § 17.06.080(A)1b are the code sections cited in the FOF by Hailey to require the retention of the middle Street Tree.

Hailey City Code § 17.06.080(A)4d states, "landscaped areas shall be planned as an integral part of the site with consideration of the urban environment. A combination of trees, shrubs, vines,

Ned Williamson, Esq  
Lisa Horowitz, Community Development Director  
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September 6, 2016  
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ground covers and ornamental grasses shall be used. New landscaped areas having more than ten (10) trees, a minimum of ten percent (10%) of the trees shall be at least four inch (4") caliper, twenty percent (20%) shall be at least three inch (3") caliper, and twenty percent (20%) shall be at least two and one-half inch (2½") caliper, and a maximum of twenty percent (20%) of any single tree species may be used in any landscape plan (excluding street trees). New planting areas shall be designed to accommodate typical trees at maturity. Buildings within the LI and SCI-I zoning district are excluded from this standard."

Hailey City Code § 17.06.080(A)1b states, "all existing plant material shall be inventoried and delineated, to scale, and noted whether it is to be preserved, relocated or removed. Removal of trees larger than six inch (6") caliper proposed to be removed require an arborist review. Any tree destroyed or mortally injured after previously being identified to be preserved, or removed without authorization, shall be replaced with a species of tree found in the tree guide and shall be a minimum of four inch (4") caliper." *Hailey City Code § 17.06.080(A)1b.*

The City of Hailey 2010 Comprehensive Plan, Part Three, Section 1, Community Forest, in part states, "an inventory of city trees was completed in 1995 and the publication of a Tree Selection & Planting Guide was completed the same year. An ordinance that recognizes the significance of our urban and native forest habitats, and the connections between the two habitats, to preserve our urban forest resource was adopted by the City Council in 2008; this ordinance was codified as Section 12.20 of the Municipal Code. The Hailey Tree Committee was also established in Section 2.36 and is tasked with creating a community forestry master plan and will oversee its implementation. The Design Review section of Municipal Code Title 17 requires street trees of appropriate size and species, in all new commercial and residential development, provides for developer installation, irrigation and maintenance of those trees for a defined time period and supports the replacement of trees when removed by development." *Hailey 201 Comprehensive Plan, Part Three, Section 1, Community Forest.*

Given the removal and replacement language contained in both Hailey City Code §17.06.080(A)1b and the Hailey Comprehensive Plan, my client is requesting reconsideration of the condition which requires retention of the middle Street Tree. This request is based upon the opinions of Derek Ruhter, Webb Landscape ISA Certified Arborist, Joe Marx of Idaho Mountain Builders, the general contractor, and Jonathan Lunceford of Lunceford Excavation, Inc., the excavation subcontractor, that the middle Street Tree will die as a result of the construction which was approved by Hailey in the FOF. Given this information, my client believes the prior decision requiring retention of this middle Street Tree by Hailey was deficient.

I am attaching as Exhibit A the two page Tree Assessment of Derek Ruhter. Mr. Ruhter recommends that either the tree be moved or that it be removed. Mr. Ruhter's recommendations are based upon his opinion which states "it is likely that 50% or more of the tree's roots would be removed." Mr. Ruhter further opines that "potential damage to branches" during construction

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September 6, 2016  
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and combined with the necessary pruning to “lift the canopy above the proposed mezzanine structure would require removal of a quarter to a third of the canopy” will further weaken the tree. It is a combination of the damage to the root ball and the damages from pruning which will occur during construction which combine to lead to Mr. Ruhter’s opinion that moving the tree or removing the tree are the only two viable options given the approved plans permitting construction of this pizzeria.

I am attaching as Exhibit B a west facing elevation prepared by Jay Cone Architecture which shows the removal of the middle Street Tree as well as a north view of the west end of the pizzeria which shows the middle Street Tree trimmed up due to the construction of the approved pizzeria and mezzanine. My client believes pruning the middle Street Tree by a quarter or a third so it is approximately equivalent to the north view of the middle Street Tree is not attractive or desirable for either its business or the City of Hailey.

I am attaching as Exhibit C the same west facing elevation, except that the middle Street Tree would remain as currently required by Hailey. This elevation does not appear realistic or feasible given the construction of the approved mezzanine and pizzeria.

I am also attaching as Exhibit D the seven page Tree Appraisal prepared by Derek Ruhter. Mr. Ruhter provides an Appraised Tree Value of between \$3,970 and \$2,630 for the middle Street Tree. The upper end of this valuation range is in Exhibit D while both values are in Exhibit B.

Finally, I am attaching as Exhibits E and F respectively the letters from Jonathan Lunceford (the excavation subcontractor) and Joe Marx (the general contractor) regarding the construction damage which in their opinion will likely lead to the death of the middle Street Tree.

My client apologizes for not providing the Planning and Zoning Commission with complete information (Exhibits A through F to the Request for Reconsideration) in either its design review application or at its design review hearing. It was under the impression that given the removal and replacement language in the Hailey City Code and the Hailey Comprehensive Plan, removal of the middle Street Tree would be permitted if it made this request. My client apologizes for not providing that attached information as it now realize it would have provided additional information to the P & Z Commission on this topic.

Neither the north or south Street Tree will be significantly damaged during the course of construction of the pizzeria. The south Street Tree will be insulated from significant excavation and construction damage given the outdoor seating to be built at the pizzeria. The north Street Tree will also be insulated from significant excavation and construction damage as Mr. Ruhter opines the excavation will only come within nine feet of this tree, and thus will not provide a significant negative impact on the root ball of the north Street Tree.

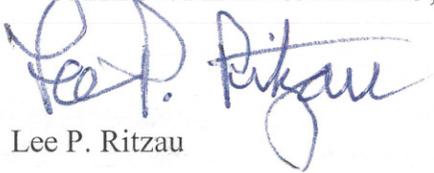
Ned Williamson, Esq  
Lisa Horowitz, Community Development Director  
Hailey Planning and Zoning Commission  
September 6, 2016  
Page 4

Given this information my client is requesting the Planning and Zoning Commission grant its request for reconsideration and either permit the middle Street Tree to be moved or removed. If you believe it would be beneficial to have either Mr. Ruhter, Mr. Marx, Mr. Lunceford, or Mr. Cone at the Request for Reconsideration meeting they are happy to attend. Please let me know so I can set up their attendance at the meeting. Please contact me should you desire any additional information.

My client thanks you for your time and consideration.

Sincerely,

LUBOVISKI, WYGLE,  
FALLOWFIELD & RITZAU, P.A.

A handwritten signature in blue ink, appearing to read "Lee P. Ritzau". The signature is stylized and cursive.

Lee P. Ritzau

cc: Erik Heiden and Fred Burke (via email)

Enclosures (Exhibits A through F)

# Exhibit A



Fred Burke  
411 N. Main Street  
Hailey, ID  
August 31, 2016

**Tree Assessment**  
Littleleaf Linden  
*Tilia cordata*

### **Introduction**

This assessment is for a Littleleaf Linden that is a street tree in the city of Hailey, ID. There are several potential conflicts with the Wise Guy Pizza Pie restaurant building proposed at 411 N. Main St. and the health of the tree.

### **Littleleaf Linden Condition**

The following is what I observed on August 28, 2016 when I inspected the trees. I used the plant appraisal format to rate the Linden tree's condition. The following is from the appraisal.

*The structure and health condition of the roots, trunk, scaffold branches, twigs, buds and foliage are evaluated and given a rating from 1 (poor) to 4 (good). The scores are totaled and a Condition Rating is given.*

- **Roots:** *There are no apparent deficiencies to the structure. The root flare is not visible, which may indicate a low planting depth when the tree was first installed or soil placed over the roots during construction or landscaping. Also the tree has been under drought conditions over the last 2 seasons. This could detrimentally affect the health of the roots.*
  - **Structure 3**
  - **Health 3**
- **Trunk:** *No apparent structure or health deficiencies.*
  - **Structure 4**
  - **Health 4**
- **Scaffold Branches:** *Most of the branches are strongly attached and appear to be healthy. There is one major co-dominant stem which reflects weakened structure.*
  - **Structure 3**
  - **Health 4**
- **Small Branches & Twigs:** *There are some dead small branches throughout the canopy which likely reflects the drought conditions the tree has been under the last few seasons.*
  - **Health 3**
- **Foliage & Buds:** *Some leaves present as yellow on the interior and are being shed. This is also a reflection of the drought conditions over the last few seasons. Aphids are also present.*
  - **Health 3**

**TOTAL SCORE – 27 (out of 32 possible) – Condition Rating of 84%**

The tree is in good health, but there are some issues/conflicts that can affect its long term health.



### Assessment

The first conflict is care. It does not appear that the Linden trees adjacent to the lot have not been adequately irrigated or received cared for the last two growing seasons after the lot was vacated by the previous business. The concern for the trees is root damage from these drought conditions. The root damage can be reflected in dead twigs and branches in the canopy; which is the case with this Linden tree. When initially contacted by the Fred Burke regarding these trees, the first recommendation I made was to provide for irrigation of the trees. Since then, Fred has provided for water to the trees. The trees seem to have tolerated the lack of care and appear to be in good condition and should recover if care & irrigation are continued.

The second conflict is potential root damage to the middle tree along the sidewalk. From the plans, it appears that the structures will be built to the property line, which is within three to four feet of the trunk. Ideally, the excavation would remain outside the canopy which extends 9 feet beyond the trunk, and 5 to 6 feet into the proposed construction. Looking at the history of the tree's care, it is most likely that the majority of the roots are in the lot that was previously irrigated. With the proposed construction, it is likely that 50% or more of the tree's roots would be removed.

This does not include the potential damage to branches over the proposed construction, which is the third conflict. It is likely that branches will be damaged. Pruning should be done and proper pruning prior to construction can prevent potential damage. However, to lift the canopy above the proposed mezzanine structure would require removal of a quarter to a third of the canopy. This in conjunction with the root damage would further weaken the tree.

### Recommendations

If the proposed construction were to move forward, I would give the following recommendations:

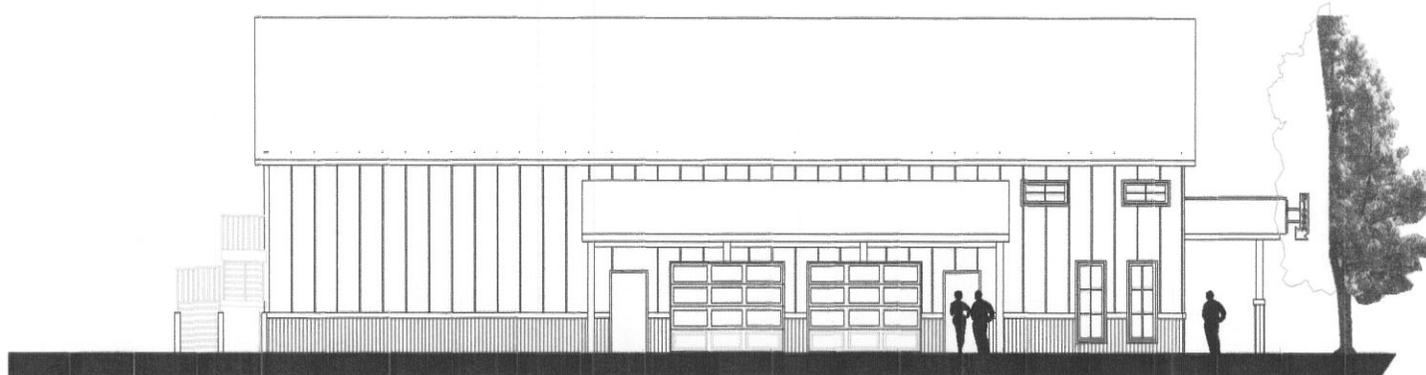
- Move the tree. The tree is in good health and has value. In speaking with Jeff Smith at Arbor Farms, he felt that this tree could be transplanted. This would take removing the concrete, spading the tree, moving the tree to another suitable location, and replacing the sidewalk. In addition, moving the tree may be complicated by any underground utilities in the area.  
**OR**
- Remove the tree. There is a high potential for damage to the roots and structure of the tree during construction. Also the building entrance and signage would be obstructed if the tree remained. The current value range of the tree is from \$2,630 to \$3,970. However, that value would decline substantially if the tree were damaged during construction.

Derek Ruhter  
Webb Landscape  
ISA Certified Arborist

# Exhibit B



WEST FACING VIEW - PROPOSED  
 \*PLANNING AND ZONING APPROVED BUILDING AND ENTRY ROOF 1/4" = 1'-0"



NORTH FACING VIEW - EXISTING TREE TRIMMED  
 \*PLANNING AND ZONING APPROVED BUILDING AND ENTRY ROOF 1/4" = 1'-0"



JAY CONE ARCHITECTURE, INC.  
 Custom Design & Planning  
 Commercial Residential  
 208.578.5225  
 601 El Dorado Lane  
 Hailey, Idaho 83422

jay@jcaarchitecture.com  
 www.jcaarchitecture.com

Date: 8/16/2016

Scale:

Wise Guy Pizza  
 411 North Main Street  
 Hailey Idaho

Issue:

THIS DOCUMENT IS PRELIMINARY AND NOT FOR CONSTRUCTION. IT IS SUBJECT TO CHANGE WITHOUT NOTICE. THE CLIENT ACCEPTS RESPONSIBILITY FOR THE ACCURACY OF THE INFORMATION PROVIDED AND THE RESULTS OF THE DESIGN. THE ARCHITECT ASSUMES NO LIABILITY FOR THE DESIGN OR CONSTRUCTION OF THE PROJECT.

# REV DATE

Scale: AS NOTED  
 Drawn: JC  
 Job: 2016-3

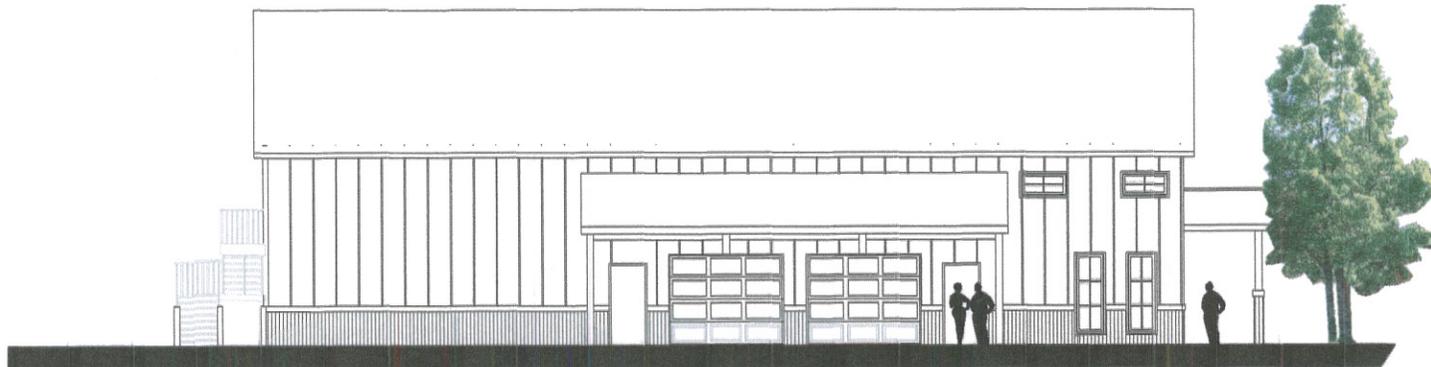
A-201

PRELIMINARY  
 NOT FOR CONSTRUCTION

# Exhibit C



WEST FACING VIEW  $1/4" = 1'-0"$   
 \*PLANNING AND ZONING APPROVED BUILDING AND ENTRY ROOF



NORTH FACING VIEW  $1/4" = 1'-0"$   
 \*PLANNING AND ZONING APPROVED BUILDING AND ENTRY ROOF



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 Hailey, Idaho 83422  
 jca@jcaarchitecture.com  
 www.jcaarchitecture.com

Date:  
 8/16/2016

Scale:

Wise Guy Pizza  
 411 North Main Street  
 Hailey Idaho

Issue:

THIS DOCUMENT IS PRELIMINARY AND NOT TO BE USED FOR CONSTRUCTION. IT IS SUBJECT TO CHANGE WITHOUT NOTICE. THE CLIENT ACCEPTS THE RISK OF CONSTRUCTION OF THIS PROJECT WITHOUT THE FINAL APPROVED SET OF PLANS. THE ARCHITECT ASSUMES NO LIABILITY FOR CONSTRUCTION OF THIS PROJECT.

# REV. DATE


Scale: AS NOTED  
 Drawn: JC  
 Date: 2016-8

PRELIMINARY  
 NOT FOR CONSTRUCTION

# Exhibit D



Fred Burke  
411 N. Main Street  
Hailey, ID  
August 31, 2016

**Tree Appraisal**  
Littleleaf Linden  
*Tilia cordata*

The tree appraised in this report is located one foot from the sidewalk along Main Street in Hailey. It is a Linden that is approximately 35 feet in height with a canopy that is 7 – 8 feet above the sidewalk and has a diameter of 18 feet. The tree's trunk has a Diameter at Breast Height (DBH) of 10 ½ inches. There are Linden trees to the north and to the south that appear to have been planted at the time the appraised tree was. They are also one foot from the sidewalk. The Linden to the south is 27 feet from the appraised tree; the other Linden is 25 feet to the north. These three trees form a row along the sidewalk.

This tree appraisal follows the guidelines set forth in the Guide for Plant Appraisal (9<sup>th</sup> Edition) by representatives of the Council of Tree & Landscape Appraisers. There are four primary factors in determining the value of a tree: Size, Species, Condition, and Location.

**Size**  
10.5" DBH

**Species**  
Each species of tree is given a percentage rating based on its value in the landscape. For this appraisal, the Species Ratings for Landscape Tree Appraisal in Utah was used. Of the available publications, this booklet most accurately reflects the appropriate value for trees in south central Idaho.

The species rating given is a range from 65% - 85%. As the Linden is an ornamental tree that is well adapted to the climate of the Wood River Valley, the species rating given for this appraisal is **85%**.

**Condition**  
The structure and health condition of the roots, trunk, scaffold branches, twigs, buds and foliage are evaluated and given a rating from 1 (poor) to 4 (good). The scores are totaled and a Condition Rating is given.

- **Roots:** There are no apparent deficiencies to the structure. The root flare is not visible, which may indicate a low planting depth when the tree was first installed or soil placed over the roots during construction or landscaping. Also the tree has been under drought conditions over the last 2 seasons. This could detrimentally affect the health of the roots.
  - **Structure 3**
  - **Health 3**
- **Trunk:** No apparent structure or health deficiencies.
  - **Structure 4**
  - **Health 4**



- **Scaffold Branches:** Most of the branches are strongly attached and appear to be healthy. There is one major co-dominant stem which reflects weakened structure.
  - **Structure 3**
  - **Health 4**
- **Small Branches & Twigs:** There are some dead small branches throughout the canopy which likely reflects the drought conditions the tree has been under the last few seasons.
  - **Health 3**
- **Foliage & Buds:** Some leaves present as yellow on the interior and are being shed. This is also a reflection of the drought conditions over the last few seasons. Aphids are also present.
  - **Health 3**
- **TOTAL SCORE – 27** (out of 32 possible) – **Condition Rating of 84%**

#### Location

- **Site:** The site is on Main Street in Hailey. It is an empty lot that has recently been cleared. (See attached photos.) Site rating of 80%.
- **Contribution:** The tree is the center tree in a row of trees on adjacent to a property that has been cleared. It is a street tree that provides a nice canopy for pedestrians. Contribution rating of 95%.
- **Placement:** The placement near the sidewalk provides a nice canopy for pedestrians. However the placement so close to the sidewalk increases the potential for heaving of the sidewalk and trip hazards for pedestrians. Placement 50%.

The location rating is an average of the site, contribution, and placement ratings. **Location Rating of 75%**

#### APPRAISED TREE VALUE

Since a 10" DBH Linden is not readily available, the Trunk Formula Method was used to determine the value of the tree. A worksheet is attached to this report outlining the steps used to determine the trees value. Once the value was determined, it was adjusted according to species, condition, and location.

**Appraised Tree Value = \$3,970**

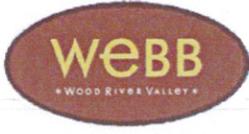
Derek Ruhter  
ISA Certified Arborist

A handwritten signature in black ink that reads "Derek Ruhter". The signature is written in a cursive style with a large initial "D".



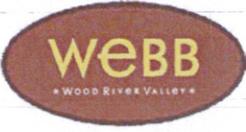
Appraised Linden as photographed from the North.





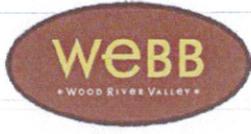
Appraised Linden as photographed from the West.





Distance of the tree's trunk to the sidewalk.





Lot as photographed from the sidewalk east of the appraised tree.



# Trunk Formula Method Work Sheet

Case # \_\_\_\_\_ Property 411 N. Main St. Date 8/31/16  
 Appraiser Derek Fuhrer

## Field Observations

1. Species Tilia Cordata
2. Condition 84 %
3. Trunk Circumference \_\_\_\_\_ in./cm Diameter 10.5 in./cm
4. Location % = [Site 80 % + Contribution 95 % + Placement 50 %]  
 $\div 3 = \underline{75}$  %

## Regional Plant Appraisal Committee and/or Appraiser-Developed or -Modified Information

5. Species rating 85 %
6. Replacement Tree Size (diameter) 4 in./cm  
 (Trunk Area) 13 in<sup>2</sup>/cm<sup>2</sup> TA<sub>R</sub>
7. Replacement Tree Cost \$ 620<sup>00</sup>  
 (see Regional Information to use Cost selected)
8. Installation Cost \$ 500<sup>00</sup>
9. Installed Tree Cost (#7 + #8) \$ 1,120<sup>00</sup>
10. Unit Tree Cost \$ 86.15 per in<sup>2</sup>/cm<sup>2</sup>  
 (see Regional Information to use Cost selected)

## Calculations by Appraiser using Field and Regional Information

11. Appraised Trunk Area:  
 (TA<sub>A</sub> or ATA<sub>A</sub>; use Tables 4.4-4.7)  
 or  $c^2$  (#3) \_\_\_\_\_  $\times 0.08$   
 or  $d^2$  (#3) 110  $\times 0.785$  = 86 in<sup>2</sup>/cm<sup>2</sup>
12. Appraised Tree Trunk Increase (TA<sub>INCR</sub>) =  
 TA<sub>A</sub> or ATA<sub>A</sub> 86 in<sup>2</sup>/cm<sup>2</sup> (#11) - TA<sub>R</sub> 13 in<sup>2</sup>/cm<sup>2</sup> (#6) = 73 in<sup>2</sup>/cm<sup>2</sup>
13. Basic Tree Cost = TA<sub>INCR</sub> (#12) 73 in<sup>2</sup>/cm<sup>2</sup>  $\times$  Unit Tree Cost (#10) \$ 86.15  
 per in<sup>2</sup>/cm<sup>2</sup> + Installed Tree Cost (#9) \$ 1,120 = \$ 7,409
14. Appraised Value = Basic Tree Cost (#13) \$ 7,409  $\times$  Species rating  
 (#5) 85 %  $\times$  Condition (#2) 84 %  $\times$  Location (#4) 75 % = \$ 3,968
15. If the Appraised Value is \$5,000 or more, round it to the nearest \$100; if it  
 is less, round to the nearest \$10.
16. Appraised Value = (#14) \$ 3,970

Items 5 through 10 are determined by the Regional Plant Appraisal Committee. The Wholesale Replacement Tree Cost, the Retail Replacement Tree Cost, or the Installed Tree Cost (#9) divided by the Replacement Tree Size (#6) can be used for the Unit Tree Cost (#10), or it can be set by the Regional Plant Appraisal Committee.

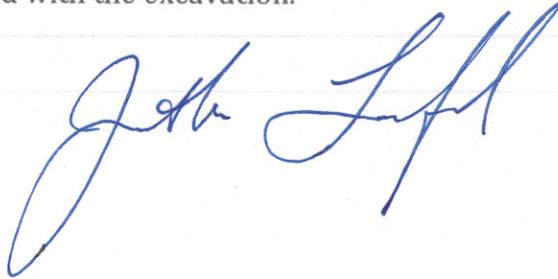
# Exhibit E

August 25, 2016

## Lunceford Excavation Inc.

In reference to 411 North Main st Hailey Idaho.  
We are excavating for a foundation very near the base of the tree that borders main street, when ever you excavate that close to the base of a tree there is always the chance of damaging the root system. When ever you damage the root system there is a very strong possibility that it will kill the tree. Please be advised of this as we move forward with the excavation.

Thank you  
Jonathan Lunceford  
Lunceford Excavation Inc.

A handwritten signature in blue ink, appearing to read "Jonathan Lunceford", is written over the typed name and company name.

# Exhibit F



**IDAHO MOUNTAIN BUILDERS**

August 25, 2016

---

**Joe Marx\_Owner**

251 First Ave. North\_P.O. Box 3761

Ketchum, ID 83340

*mobile: 208-720-7064*

*office: 208-726-1603*

[joe@idahomountainbuilders.com](mailto:joe@idahomountainbuilders.com)

To Whom It May Concern:

My company has been hired in the role of general contractor for the building of the Wise Guy Pizza Pie restaurant at 411 N Main in Hailey. I have been requested by the owners to express my concern for the viability of a particular tree on the property. The tree is located at the center of the lot on the street side of the property. It is my opinion that the construction process will very likely lead to the demise of the tree. The excavation will involve an over dig which could lead to damage of the tree's root system. If there is any issue with the existing condition of the soil the tree may have to be removed to import compaction materials. The mezzanine structure once constructed may actually physically touch the tree. The substrate preparation for the paver system at the front door will impact the root system of the tree.

Thank you,

Joe Marx

**Return to Agenda**

**DESIGN REVIEW  
 STAFF REPORT**

**TO:** Hailey Planning and Zoning Commission

**FROM:** Lisa Horowitz, Community Development Director

**RE :** Design Review application submitted by Benny Rod Shop, represented by Susan Scovell, for a new 3,685 square foot addition to be located at 1131 Airport Way (Lot 2A, Block 1, Friedman Park Subdivision) in the Light Industrial (LI) Zoning District.

**HEARING:** September 12, 2016

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**Applicant:** Keith and Beverly Thompson

**Request:** Design Review of a 3,685 square foot addition to an existing business, Benny Rod Shop

**Location:** 11311 Airport Way (Lot 2A, Block 1, Friedman Park Subdivision)

**Zoning:** Light Industrial (LI) Zoning District.

**Notice**

Notice for the public hearing was published in the Idaho Mountain Express on 8/24/16 and mailed to property owners within 300 feet on 8/24/16.

**Application**

The applicant is requesting a 3,685 square foot addition to an existing business, Benny Rod Shop. The addition consists of a new shop/storage space, an office and a restroom. The site is 18,600 square feet (.427 acres).

**Procedural History**

The existing 4,352 square foot building was built in 1987.

<b>General Requirements for all Design Review Applications</b>				
Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.050	<i>Complete Application</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Department	Engineering:

			Comments	<p>1. No comments received</p> <p>2.</p> <p><b>Life/Safety:</b> All Fire Department requirements shall be met.</p> <p><b>Water and Sewer:</b> No comments from water/sewer</p> <p><b>Building:</b> No comments</p> <p><b>Streets:</b> No comments</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.08A Signs	<p><b>17.08A Signs:</b> The applicant is hereby advised that a sign permit is required for any signage exceeding four square feet in sign area. Approval of signage areas or signage plan in Design Review does not constitute approval of a sign permit.</p> <p><i>Staff Comments</i> No new signs are proposed.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.09.040 On-site Parking Req.	<p>See Section 9.4 for applicable code.  <b>Require 1 space for 1,000 square feet, or, if the site is considered warehouse and storage, 1 space per every (full time) employee, whichever is greater.</b></p> <p><i>Staff Comments</i> Existing building: 4,352 sq. feet. Addition: 3,685 square feet. 8,037 total requires 8 parking spaces. Five parking spaces are shown in front of the building, and three angled spaces are shown on the east side for a total of eight. Required parking has been met.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.08C.040 Outdoor Lighting Standards	<p><b>8B.4.1 General Standards</b></p> <p>a. All exterior lighting shall be designed, located and lamped in order to prevent:</p> <ol style="list-style-type: none"> <li>1. Overlighting;</li> <li>2. Energy waste;</li> <li>3. Glare;</li> <li>4. Light Trespass;</li> <li>5. Skyglow.</li> </ol> <p>b. All non-essential exterior commercial and residential lighting is encouraged to be turned off after business hours and/or when not in use. Lights on a timer are encouraged. Sensor activated lights are encouraged to replace existing lighting that is desired for security purposes.</p> <p>c. Canopy lights, such as service station lighting shall be fully recessed or fully shielded so as to ensure that no light source is visible from or causes glare on public rights of way or adjacent properties.</p> <p>d. Area lights. All area lights are encouraged to be eighty-five (85) degree full cut-off type luminaires.</p> <p>Idaho Power shall not install any luminaires after the effective date of this Article that lights the public right of way without first receiving approval for any such application by the Lighting Administrator</p> <p><i>Staff Comments</i> Existing lighting meets City standards. New exterior lighting will be provided over the exit doors, and will be dark sky compliant.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Bulk Requirements	<p>LI Zone District:</p> <ul style="list-style-type: none"> <li>- Minimum Lot Size: 6,000 square feet</li> <li>- Maximum Lot Width: 60</li> <li>- Maximum Building Height: 35</li> <li>- Front Setback: 10 ft.</li> </ul> <p>Side and Rear Yard Setback: 10 feet</p> <p><i>Staff Comments</i> Front: 43' N. Side: 11' S. Side 16' Rear: 10'-6"</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.070(A)1 Street Improvements	<p>Sidewalks and drainage improvements are required in all zoning districts, except as otherwise provided herein.</p>

			<b>Required</b>	
			<b>Staff Comments</b>	<i>The existing property has a continuous asphalt vehicular access along Airport Way, similar to existing neighboring properties, that serves the existing auto repair use well. There are no sidewalks in the immediate vicinity. The applicant requests to provide an in-lieu fee instead of constructing a sidewalk that would not be connected to a system of sidewalks. Staff concurs with this request.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>17.06.070(B) Required Water System Improvements</b>	<b>In the Townsite Overlay District, any proposal for new construction or addition of a garage accessing from the alley, where water main lines within the alley are less than six (6) feet deep, the developer shall install insulating material (blue board insulation or similar material) for each and every individual water service line and main line between and including the subject property and the nearest public street, as recommended by the City Engineer.</b>
			<b>Staff Comments</b>	

**Design Review Requirements for Non-Residential, Multifamily, and/or Mixed Use Buildings within the City of Hailey**

**1. Site Planning: 17.06.080(A)1, items (a) thru (n)**

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1a	<p><b>a. The location, orientation and surface of buildings shall maximize, to the greatest extent possible sun exposure in exterior spaces to create spaces around buildings that are usable by the residents and allow for safe access to buildings</b></p> <p><i>Staff Comments</i> <i>The proposed project is an addition to an existing building located at approximately the center of the lot. The existing front yard is used by the current business for parking and storage of cars being repaired. The rear yard will allow safe access to the proposed office space at the rear of the proposed addition. Both the proposed office and associated restroom will be provided with natural light and ventilation with windows facing the site's yards. The building design (and the design of the entire site) is very functional in nature. The addition is planned to tie in with the existing building.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1b	<p><b>b. All existing plant material shall be inventoried and delineated, to scale, and noted whether it is to be preserved, relocated or removed. Removal of trees larger than 6-inch caliper proposed to be removed require an arborist review. Any tree destroyed or mortally injured after previously being identified to be preserved, or removed without authorization, shall be replaced with a species of tree found in the Tree Guide and shall be a minimum of 4-inch caliper.</b></p> <p><i>Staff Comments</i> <i>The existing property contains virtually no landscaping with the exception of a large deciduous tree located along the northern property line at the front of the lot. No change to the landscaping is proposed, except for the addition of seasonal hanging baskets.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1c	<p><b>c. Site circulation shall be designed so pedestrians have safe access to and through the site and to building.</b></p>

			<b>Staff Comments</b>	<i>The existing building is accessed via the parking area at the front through two man doors. The proposed addition will include another man door at the front of the building, as well as one along the south façade at the west end of the building and a pedestrian walkway to a door serving the office on the west façade. The existing property has a continuous asphalt vehicular access along Airport Way, similar to existing neighboring properties, that serves the existing auto repair use well. (</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1d	<b>d. Building services including loading areas, trash storage/pickup areas and utility boxes shall be located at the rear of a building; the side of the building adjacent to an internal lot line may be considered as an alternate location. These areas shall be designed in a manner to minimize conflict among uses and shall not interfere with other uses, such as snow storage. These areas shall be screened with landscaping, enclosures, fencing or by the principal building.</b>
			<b>Staff Comments</b>	<i>The dumpster is located along the access easement on the south side of the property will remain in place. It does not interfere with snow storage/removal. It is not visible from the street.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)1e	<b>e. Where alleys exist, or are planned, they shall be utilized for building services.</b>
			<b>Staff Comments</b>	<i>No alley exists, but the site is served by a twenty-five foot wide access/utility easement along the southern property line. The proposed rear addition will be accessed from that easement, and utilities to serve the addition will come from that easement.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)1f	<b>f. Vending machines located on the exterior of a building shall not be visible from any street.</b>
			<b>Staff Comments</b>	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1g	<b>g. On-site parking areas shall be located at the rear of the building and screened from the street. Parking and access shall not be obstructed by snow accumulation. (NOTE: If project is located in Airport West Subdivision, certain standards may apply that are not listed here. See code for details.)</b> <b>i. Parking areas located within the SCI zoning district may be located at the side or rear of the building.</b> <b>ii. Parking areas may be considered at the side of buildings within the B, LB, TI and LI zoning districts provided a useable prominent entrance is located on the front of the building and the parking area is buffered from the sidewalk adjacent to the street.</b>
			<b>Staff Comments</b>	<i>The existing building and related parking and loading arrangement have been in place since 1987, and pre-dates design review. A prominent entry does exist.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1h	<b>h. Access to on-site parking shall be from the alley or, if the site is not serviced by an alley, from a single approach to the street to confine vehicular/pedestrian conflict to limited locations, allow more buffering of the parking area and preserve the street frontage for pedestrian traffic.</b>
			<b>Staff Comments</b>	<i>The existing head-in parking is nonconforming, and is in place for much of Airport Way. Staff recommends that it be allowed to remain.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1i	<b>i. Snow storage areas shall be provided on-site where practical and sited in a manner that is accessible to all types of snow removal vehicles of a size that can accommodate moderate areas of snow.</b>
			<b>Staff Comments</b>	<i>Snow is currently hauled from the site to provide work area for the existing auto repair business. That will continue to be the case.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1j	<b>j. Snow storage areas shall not be less than 25% of the improved parking and vehicle and pedestrian circulation areas.</b>

			<i>Staff Comments</i>	<i>The snow is currently hauled off site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1k	<b>k. A designated snow storage area shall not have any dimension less than 10 feet.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)1l	<b>l. Hauling of snow from downtown areas is permissible where other options are not practical.</b>
			<i>Staff Comments</i>	<i>Snow is currently hauled from the site to provide work area for the existing auto repair business. That will continue to be the case.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)1m	<b>m. Snow storage areas shall not impede parking spaces, vehicular and pedestrian circulation or line of sight, loading areas, trash storage/pickup areas, service areas or utilities.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)1n	<b>n. Snow storage areas shall be landscaped with vegetation that is salt-tolerant and resilient to heavy snow.</b>
			<i>Staff Comments</i>	<i>N/A</i>

**2. Building Design: 17.06.080(A)2, items (a) thru (m)**

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)2a	<b>a. The proportion, size, shape and rooflines of new buildings shall be compatible with surrounding buildings.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)2b	<b>a. Standardized corporate building designs are prohibited.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2c	<b>b. At ground level, building design shall emphasize human scale, be pedestrian oriented and encourage human activity and interaction.</b>
			<i>Staff Comments</i>	<i>The existing building with the proposed addition is one story in height and has man doors, windows and relatively low eave heights that emphasize the human scale. The building is pedestrian in scale at one story.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2d	<b>c. The front façade of buildings shall face the street and may include design features such as windows, pedestrian entrances, building off-sets, projections, architectural detailing, courtyards and change in materials or similar features to create human scale and break up large building surfaces and volumes.</b>
			<i>Staff Comments</i>	<i>The existing building with the proposed addition faces Airport Way and contains design features such as windows, pedestrian entrances, building offsets, projections, architectural detailing and change in materials to create human scale and break up large building surfaces and volumes.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2e	<b>d. Any addition onto or renovation of an existing building shall be designed to create a cohesive whole.</b>
			<i>Staff Comments</i>	<i>The proposed changes are compatible with the design and scale of the existing building. Proposed horizontal wood siding will add visual interest and complement the existing building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2f	<b>e. All exterior walls of a building shall incorporate the use of varying materials, textures and colors.</b>

			<b>Staff Comments</b>	<i>The existing building utilizes blue vertical metal siding, white doors and trim and a white metal roof. The proposed addition will utilize the same materials, but adds horizontal wood trim at the pedestrian level to add visual interest while complementing the existing building. An actual material sample will be brought to the meeting.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2g	<b>f. Exterior buildings colors and materials shall be integrated appropriately into the architecture of the building and be harmonious within the project and with surrounding buildings.</b>
			<b>Staff Comments</b>	<i>The colors are planned to match the existing colors. The design and colors are similar to other buildings in the vicinity. The existing building utilizes blue vertical metal siding, white doors and trim and a white metal roof. The proposed addition will utilize the same materials, but adds horizontal wood trim at the pedestrian level to add visual interest while complementing the existing building.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)2h	<b>g. Flat-roofed buildings over two stories in height shall incorporate roof elements such as parapets, upper decks, balconies or other design elements.</b>
			<b>Staff Comments</b>	<i>N/A</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2i	<b>i. All buildings shall minimize energy consumption by utilizing alternative energy sources and/or passive solar techniques. At least three (3) of the following techniques, or an approved alternative, shall be used to improve energy cost savings and provide a more comfortable and healthy living space:</b> <b>i) Solar Orientation.</b> If there is a longer wall plane, it shall be placed on an east-west axis. A building's wall plane shall be oriented within 30 degrees of true south. <b>ii) South facing windows with eave coverage.</b> At least 40% of the building's total glazing surface shall be oriented to the south, with roof overhang or awning coverage at the south. <b>iii) Double glazed windows.</b> <b>iv) Windows with Low Emissivity glazing.</b> <b>v) Earth berming against exterior walls</b> <b>vi) Alternative energy.</b> Solar energy for electricity or water heating, wind energy or another approved alternative shall be installed on-site. <b>vii) Exterior light shelves.</b> All windows on the southernmost facing side of the building shall have external light shelves installed.
			<b>Staff Comments</b>	<i>No change to existing; addition will use high efficacy light fixtures and will be insulated as per code.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2j	<b>j. Gabled coverings, appropriate roof pitch, or snow clips and/or gutters and downspouts shall be provided over all walkways and entries to prevent snow from falling directly onto adjacent sidewalks.</b>
			<b>Staff Comments</b>	<i>The building does not drain towards walkways. Site drainage is shown on the drainage plan, and gutters are shown on the elevations. The applicant plans to relocate exterior material sample out of the drainage area.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)2k	<b>k. Downspouts and drains shall be located within landscape areas or other appropriate locations where freezing will not create pedestrian hazards.</b>
			<b>Staff Comments</b>	<i>The new roof section is shown to drain to a drywell.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)2l	<b>l. Vehicle canopies associated with gas stations, convenience stores or drive-through facilities shall have a minimum roof pitch of 3/12 and be consistent with the colors, material and architectural design used on the principal</b>

				<b>building(s).</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)2m	<b>m. A master plan for signage is required to ensure the design and location of signs is compatible with the building design and compliance with Chapter 17.08.</b>
			<i>Staff Comments</i>	<i>No change to the existing signage</i>
<b>3. Accessory Structures, Fences and Equipment/Utilities: 17.06.080(A)3, items (a) thru (i)</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)3a	<b>a. Accessory structures shall be designed to be compatible with the principal building(s).</b>
			<i>Staff Comments</i>	<i>N/A.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)3b	<b>b. Accessory structures shall be located at the rear of the property.</b>
			<i>Staff Comments</i>	<i>N/A.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)3c	<b>c. Walls and fences shall be constructed of materials compatible with other materials used on the site.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)3d	<b>d. Walls and fencing shall not dominate the buildings or the landscape. Planting should be integrated with fencing in order to soften the visual impact.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)3e	<b>e. All roof projections including, roof-mounted mechanical equipment, such as heating and air conditioning units, but excluding solar panels and Wind Energy Systems that have received a Conditional Use Permit, shall be shielded and screened from view from the ground level of on-site parking areas, adjacent public streets and adjacent properties.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)3f	<b>f. The hardware associated with alternative energy sources shall be incorporated into the building's design and not detract from the building and its surroundings.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)3g	<b>g. All ground-mounted mechanical equipment, including heating and air conditioning units, and trash receptacle areas shall be adequately screened from surrounding properties and streets by the use of a wall, fence, or landscaping, or shall be enclosed within a building.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3h	<b>h. All service lines into the subject property shall be installed underground.</b>
			<i>Staff Comments</i>	<i>No change- lines are underground.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)3i	<b>i. Additional appurtenances shall not be located on existing utility poles.</b>
			<i>Staff Comments</i>	<i>No change</i>

<b>4. Landscaping: 17.06.080(A)4, items (a) thru (n)</b>			
<b>Compliant</b>			<b>Standards and Staff Comments</b>
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code      City Standards and Staff Comments</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4a <b>a. Only drought tolerant plant species and/or xeriscape specific plant materials shall be used, as specified by the Hailey Landscaping Manual or an approved alternative.</b> <i>Staff Comments</i> All changes are to hardscape, with the exception of seasonal hanging baskets.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4b <b>b. All plant species shall be hardy to the Zone 4 environment.</b> <i>Staff Comments</i> See above.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4c <b>c. At a minimum, a temporary irrigation system that fully operates for at least two complete growing seasons is required in order to establish drought tolerant plant species and/or xeriscape specific plant materials. Features that minimize water use, such as moisture sensors, are encouraged.</b> <i>Staff Comments</i> N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4d <b>d. Landscaped areas shall be planned as an integral part of the site with consideration of the urban environment. A combination of trees shrubs, vines, ground covers and ornamental grasses shall be used. New landscaped areas having more than 10 trees, a minimum of 10% of the trees shall be at least 4-inch caliper, 20% shall be at least 3-inch caliper, and 20% shall be at least 2½ inch caliper and a maximum of 20% of any single tree species may be used in any landscape plan (excluding street trees). New planting areas shall be designed to accommodate typical trees at maturity. Buildings within the LI and SCI-I zoning district are excluded from this standard.</b> <i>Staff Comments</i> This zone is excluded from this requirement
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(A)4e <b>e. Seasonal plantings in planter boxes, pots, and/or hanging baskets shall be provided to add color and interest to the outside of buildings in the LI and SCI-I zoning districts.</b> <i>Staff Comments</i> Seasonal planter baskets on either side of the front doors are proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4f <b>f. Plantings for pedestrian areas within the B, LB, TN and SCI-O zoning districts shall be designed with attention to the details of color, texture and form. A variety of trees, shrubs, perennials, ground covers and seasonal plantings, with different shapes and distinctive foliage, bark and flowers shall be used in beds, planter boxes, pots, and/or hanging baskets.</b> <i>Staff Comments</i> No pedestrian areas exist.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4g <b>g. Storm water runoff should be retained on the site wherever possible and used to irrigate plant materials.</b> <i>Staff Comments</i> A drainage plan is in the submittal packet.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4h <b>h. A plan for maintenance of the landscaping areas is required to ensure that the project appears in a well maintained condition (i.e., all weeds and trash removed, dead plant materials removed and replaced).</b> <i>Staff Comments</i> N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4i <b>i. Retaining walls shall be designed to minimize their impact on the site and the appearance of the site.</b> <i>Staff Comments</i> No retaining walls are proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4j <b>j. Retaining walls shall be constructed of materials that are utilized elsewhere on the site, or of natural or decorative materials.</b> <i>Staff Comments</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4k <b>k. Retaining walls, where visible to the public and/or to residents or employees of the project, shall be no higher than four feet or terraced with a three-foot horizontal separation of walls.</b>

			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4l	<b>l. Landscaping should be provided within or in front of extensive retaining walls.</b>
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4m	<b>m. Retaining walls over 24" high may require railings or planting buffers for safety.</b>
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.06.080(A)4n	<b>n. Low retaining walls may be used for seating if capped with a surface of at least 12 to 16 inches wide.</b>
			<i>Staff Comments</i>	
			17.06.080(C)	<b>Nonresidential Building Located Within LI, SCI, TI or A Districts: In addition to the standards applicable to any nonresidential, multi-family or mixed use building located within the city of Hailey described in subsection A of this section, the following design standards also apply to any nonresidential building located within the LI, SCI, TI or A zoning districts:</b>
			<i>Staff Comments</i>	
			17.06.080(C)1	<b>1. Site Planning:</b>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(C)1a	<b>a. Adjoining parcels shall be considered when planning building configuration, vehicular circulation and access, parking and drainage.</b>
			<i>Staff Comments</i>	The current building configuration, vehicular circulation and access parking and drainage work well with the adjoining parcels. All roof run-off will be directed to drywells on site to avoid impact on neighboring properties.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(C)1b	<b>b. Reciprocal vehicular ingress and egress, circulation and parking arrangements are encouraged when the adjacent site(s) allows in order to facilitate the ease of vehicular movement between adjoining properties.</b>
			<i>Staff Comments</i>	The property is served along its southern property line by a utility/access easement granted by Lot 2B to the west.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.06.080(C)1c	<b>c. Vehicle circulation, parking and loading shall not block pedestrian access ways.</b>
			<i>Staff Comments</i>	The property contains no defined pedestrian access ways, but pedestrian access is fully adequate.

**A. The Commission or Hearing Examiner shall determine the following before approval is given:**

1. The project does not jeopardize the health, safety or welfare of the public.
2. The project conforms to the applicable specifications outlined in the Design Review Guidelines, as set forth herein, applicable requirements of the Zoning Ordinance, and City Standards.

**B. Conditions. The Commission or Hearing Examiner may impose any condition deemed necessary. The Commission or Hearing Examiner may also condition approval of a project with subsequent review and/or approval by the Administrator or Planning Staff. Conditions which may be attached include, but are not limited to those which will:**

1. Ensure compliance with applicable standards and guidelines.
2. Require conformity to approved plans and specifications.
3. Require security for compliance with the terms of the approval.
4. Minimize adverse impact on other development.

5. **Control the sequence, timing and duration of development.**
  6. **Assure that development and landscaping are maintained properly.**
  7. **Require more restrictive standards than those generally found in the Zoning Ordinance.**
- C. **Security. The applicant may, in lieu of actual construction of any required or approved improvement, provide to the City such security as may be acceptable to the City, in a form and in an amount equal to the cost of the engineering or design, materials and installation of the improvements not previously installed by the applicant, plus fifty percent (50%), which security shall fully secure and guarantee completion of the required improvements within a period of one (1) year from the date the security is provided.**
1. **If any extension of the one year period is granted by the City, each additional year, or portion of each additional year, shall require an additional twenty percent (20%) to be added to the amount of the original security initially provided.**
  2. **In the event the improvements are not completely installed within one (1) year, or upon the expiration of any approved extension, the City may, but is not obligated, to apply the security to the completion of the improvements and complete construction of the improvements.**

The following conditions are suggested to be placed on any approval of this application:

- a. All applicable Fire Department and Building Department requirements shall be met.
- b. Any change in use or occupancy type from that approved at time of issuance of Building Permit may require additional improvements and/or approvals. Additional parking may also be required upon subsequent change in use, in conformance with Hailey's Zoning Ordinance at the time of the new use.
- c. All City infrastructure requirements shall be met, and plans shall be modified to meet the comments herein. Detailed plans for all infrastructure to be installed or improved at or adjacent to the site shall be submitted for Department Head approval and shall meet City Standards where required. Infrastructure to be completed at the applicant's sole expense includes, but will not be limited to, the following requirements and improvements: a payment in lieu for the required sidewalk along the Airport Way street frontage shall be made prior to issuance of the building permit.
- d. The project shall be constructed in accordance with the application or as modified by these Findings of Fact, Conclusions of Law and Decision.
- e. All new and existing exterior lighting shall comply with the Outdoor Lighting Ordinance.
- f. Except as otherwise provided, all the required improvements shall be constructed and completed, or sufficient security provided as approved by the City Attorney, before a Certificate of Occupancy can be issued.
- g. This Design Review approval is for the date the Findings of Fact are signed. The Planning

& Zoning Administrator has the authority to approve minor modifications to this project prior to, and for the duration of a valid Building Permit.

**Motion Language**

**Approval:**

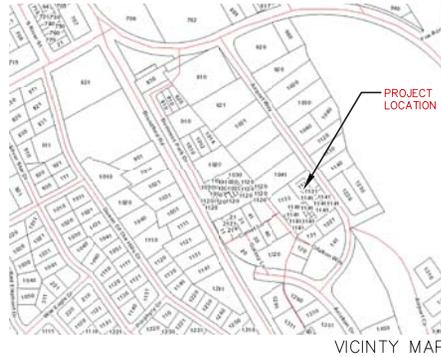
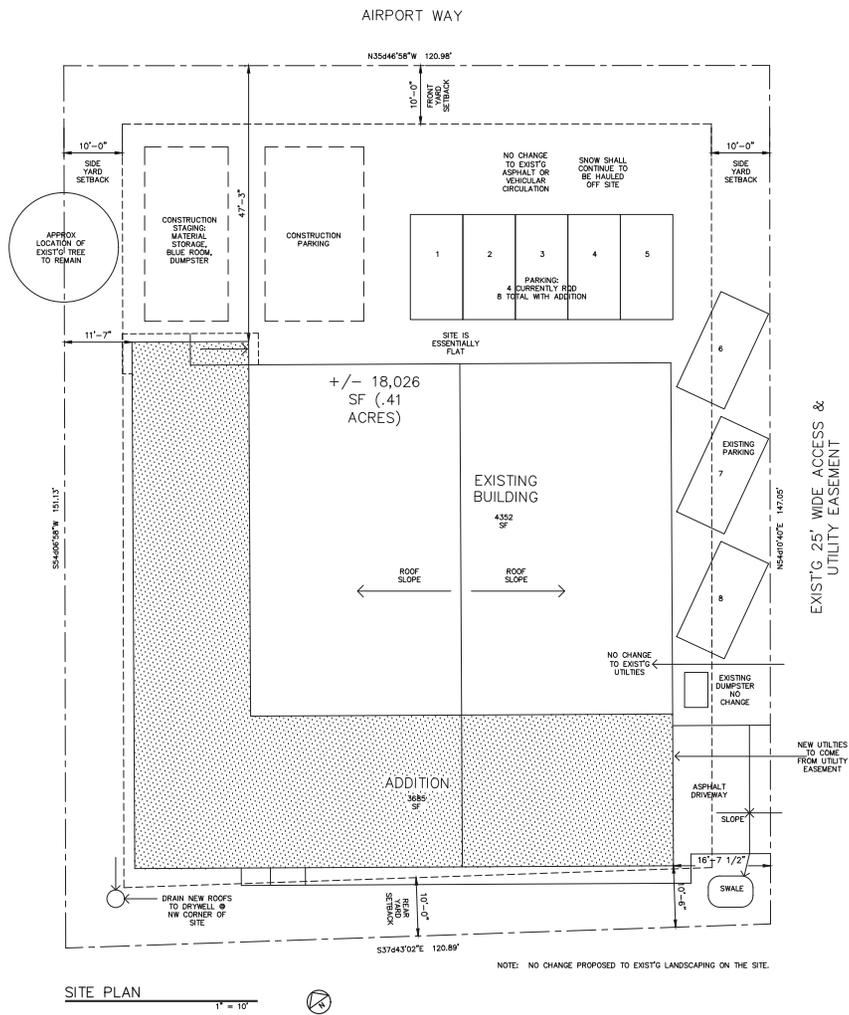
Motion to approve the Design Review application submitted by Benny Rod Shop, represented by Susan Scovell, for a new 3,685 square foot addition to be located at 1131 Airport Way (Lot 2A, Block 1, Friedman Park Subdivision) in the Light Industrial (LI) Zoning District, finding that the project does not jeopardize the health, safety or welfare of the public and the project conforms to the applicable specifications outlined in the Design Review Guidelines, applicable requirements of the Zoning Ordinance, Title 18, and City Standards, provided conditions (a) through (g) are met.

**Denial:**

Motion to deny the Design Review application submitted by Benny Rod Shop, represented by Susan Scovell, for a new 3,685 square foot addition to be located at 1131 Airport Way (Lot 2A, Block 1, Friedman Park Subdivision) in the Light Industrial (LI) Zoning District, finding that \_\_\_\_\_[the Commission should cite which standards are not met and provided the reason why each identified standard is not met].

**Continuation:**

Motion to continue the public hearing on the Design Review application by Benny Rod Shop, represented by Susan Scovell, for a new 3,685 square foot addition to be located at 1131 Airport Way (Lot 2A, Block 1, Friedman Park Subdivision) in the Light Industrial (LI) Zoning District, to \_\_\_\_[Commission should specify a date].



**SHEET INDEX:**

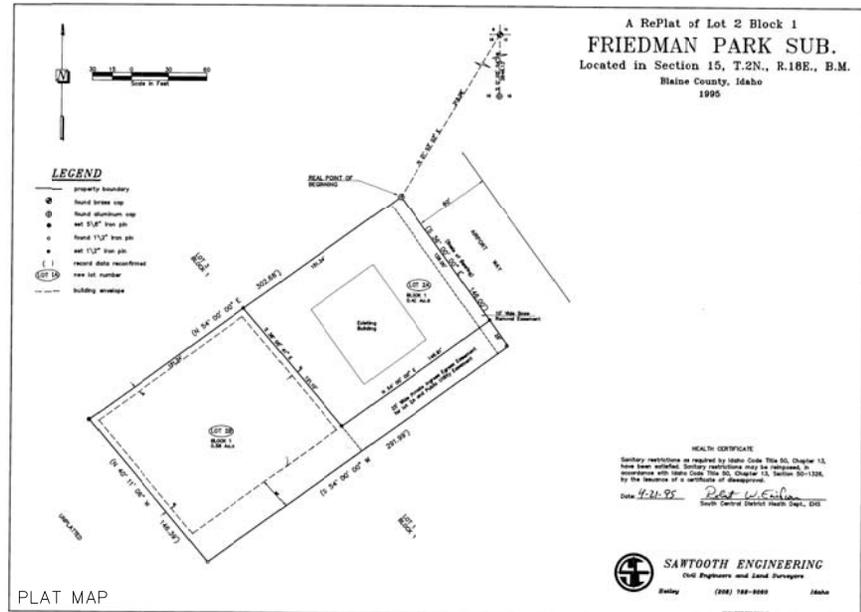
- 1 - COVER SHEET, SITE PLAN, VICINITY MAPS
- 2 - FLOOR PLAN, ELEVATIONS

**PROJECT INFORMATION**

OWNER: KEITH & BEVERLY THOMPSON  
 SITE ADDRESS: 1131 AIRPORT WAY  
 HAILEY, ID 83333  
 LEGAL DESCRIPTION:  
 FRIEDMAN PARK SUB, AM LOT 2A, BLOCK 1

**BUILDING INFORMATION NOTES**

SITE AREA: +/- 18,026 SF (.41 ACRES)  
 LAND USE: LIGHT INDUSTRIAL (LI)  
 BUILDING OCCUPANCY: F-1  
 BUILDING CONSTRUCTION TYPE: V  
 LOT COVERAGE:  
 FOOTPRINT OF EXIST'G BUILDING: 4,352 SF  
 FOOTPRINT OF PROPOSED ADDITION: 3,685 SF  
 TOTAL LOT COVERAGE: 8,037 SF (45%)

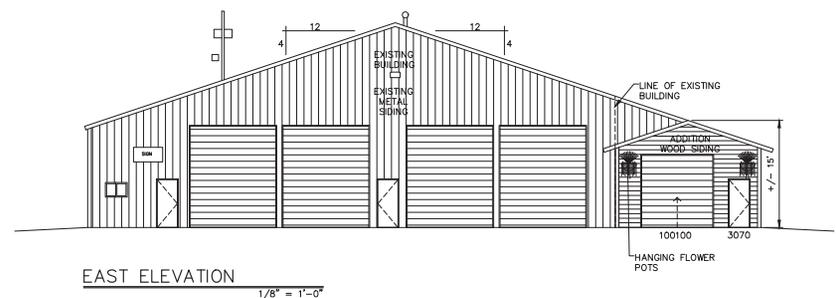
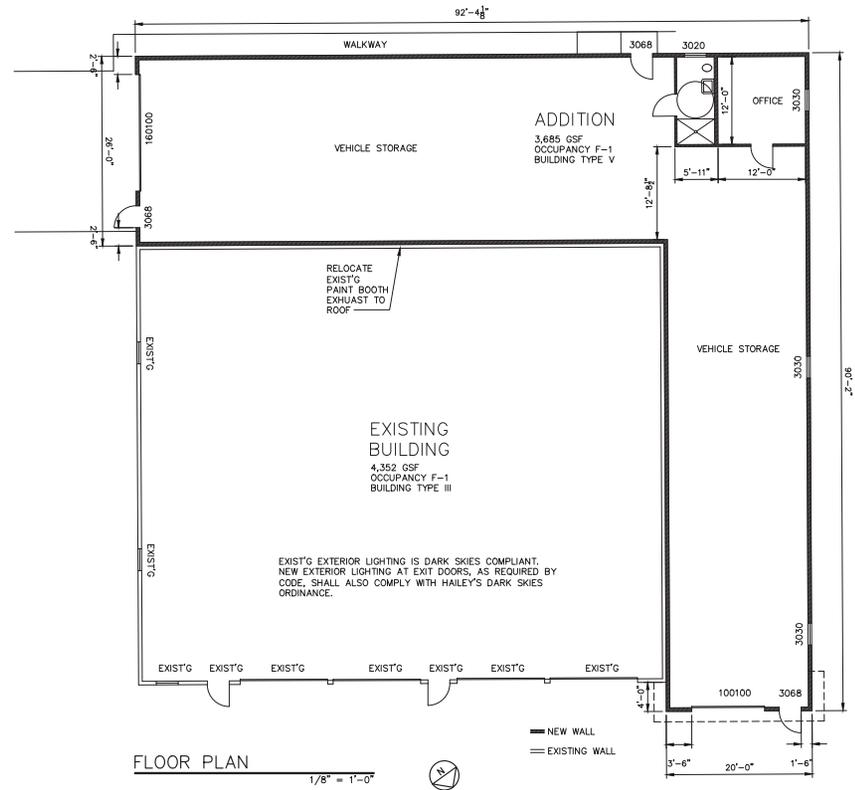
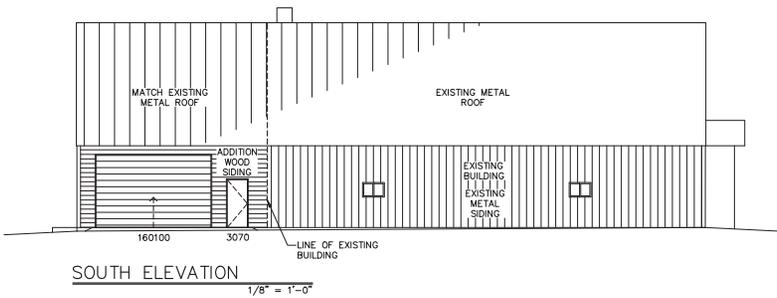
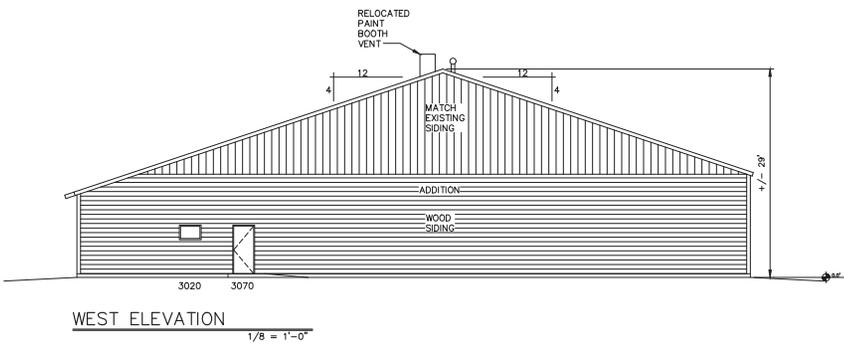
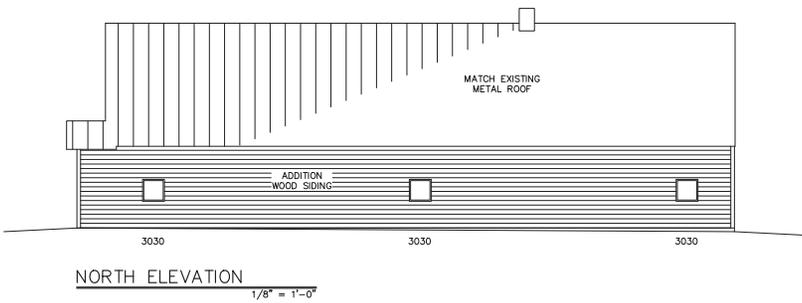


SUSAN SCOVELL, ARCHITECT  
 REG. ID. 83340  
 (208) 720-3009

REVISION STATUS  
 SUBMIT DATE:

BENNYZ ROD SHOP  
 1131 AIRPORT WAY  
 HAILEY, IDAHO  
 COMMERCIAL ADDITION

SCALE: VARIES  
 DATE: 8/17/2016  
 DRAWING NO. 1



SUSAN SCOVELL, ARCHITECT

KEY: 5863 ID: 83340  
(208) 720-3009

REVISION STATUS  
SUBMIT DATE:

BENNYZ ROD SHOP  
1131 AIRPORT WAY  
HAILEY, IDAHO  
COMMERCIAL ADDITION

SCALE: 1/8" = 1'-0"  
DATE: 8/17/2016  
DRAWING NO. 2

SHT. 2 OF 2



FRONT (EAST) ELEVATION



FRONT (EAST) ELEVATION



REAR (WEST) ELEVATION



NORTH & WEST ELEVATIONS



FRONT (EAST) ELEVATION



SOUTH ELEVATION



SOUTH ELEVATION



WEST ELEVATION



FRONT (EAST) ELEVATION



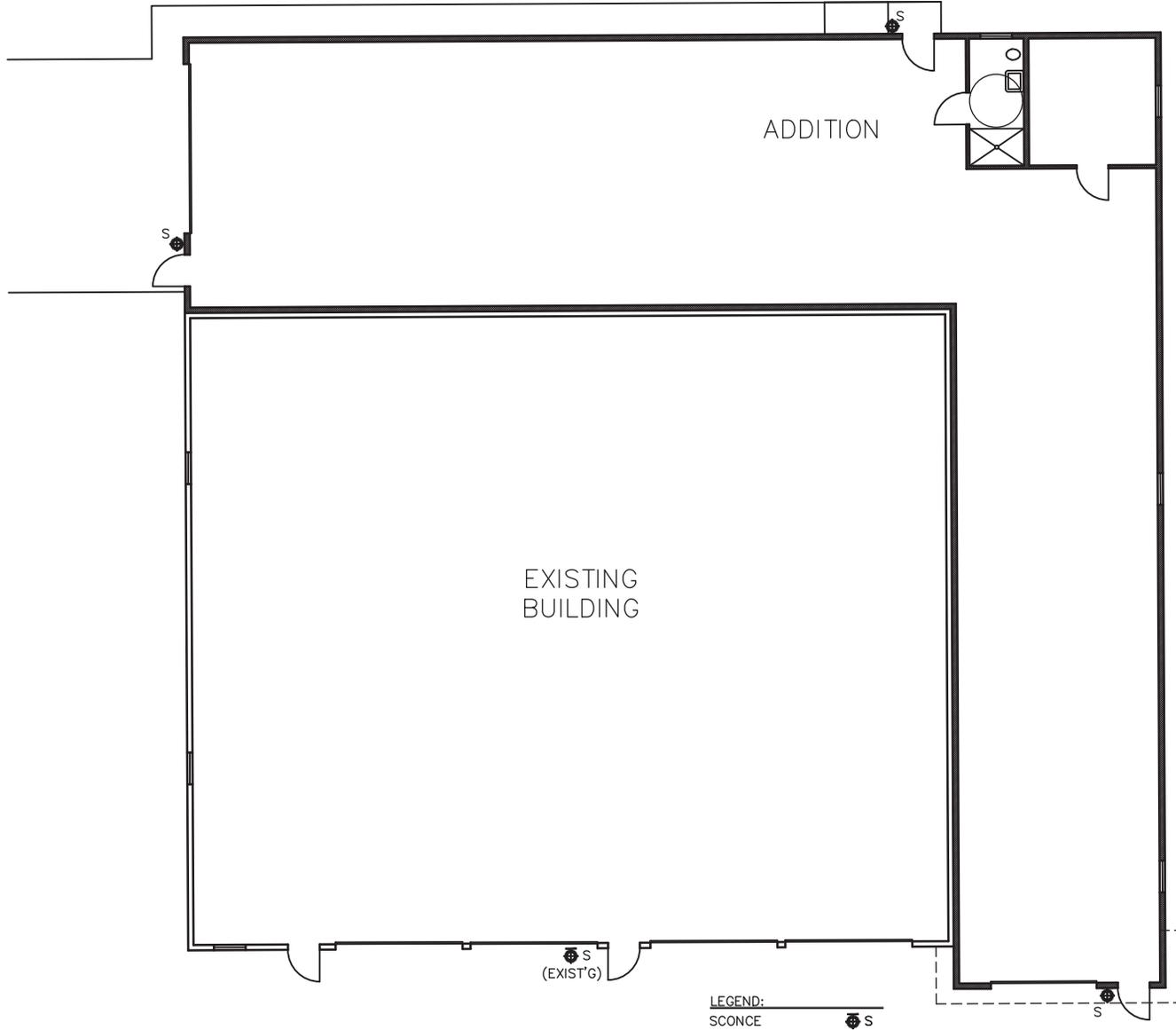
WEST & SOUTH ELEVATIONS



SOUTH ELEVATION



EXISTING EXTERIOR LIGHT FIXTURE



**EXTERIOR LIGHTING PLAN**

1/16" = 1'-0"



# Progress Lighting P5641-20/30K

Antique Bronze LED Cylinder Outdoor Wall Sconce - Down Light - 12" x 6"

Item # bci2704123 | Model #P5641-20/30K | [Cylinder Collection](#)

[How did you feel about this item? Be the first to review this product.](#)



[Click to Zoom](#)

**\$111.60**

Originally \$136.40, You Save 18%

**Antique Bronze**



**Free Shipping!**

Leaves the Warehouse Tomorrow, August 31st

**368 In Stock**

1

[Add To Cart](#)

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[Like](#)

### Questions about Progress Lighting P5641-LED?

Contact us for advice or more details. Our product specialists are here to help.

[\(800\) 375-3403](#) [Email an Expert](#) [Live Chat](#)



Lisa M

### We Also Recommend These Similar Products

Progress Lighting P5641  
Starting At \$106.02  
★★★★☆ 2

Progress Lighting P5642  
Starting At \$144.36  
★★★★★ 5

Progress Lighting P5636  
Starting At \$126.63  
★★★★★ 1

Progress Lighting P5643  
Starting At \$184.32  
★★★★☆ 4

Progress Lighting P5644  
Starting At \$222.84  
★★★★★ 5

- [Overview](#)
- [Specifications](#)
- [Reviews <sup>0</sup>](#)
- [Product Q&A <sup>0</sup>](#)
- [Matching Products](#)

Cylinder is a worldwide best-selling collection of outdoor light fixtures in both the residential and commercial markets. Fixtures are made of heavy-duty aluminum with powder coated finishes to resist rust and chipping in harsh outdoor environments. Cylinders are available in 5" or 6" diameter for wall or ceiling mounting, and come with medium base sockets or integrated LED engines.



Live Product Expert

**Features:**

- Durable and lightweight aluminum material
- High-output LED is similar to (2) 60 watt incandescent bulbs!
- Rated for installation in damp or dry locations
- For wet locations, purchase the accessory top cover lens (offered upon adding this item to your cart)

**Specifications:**

- Number of Light Engines: 1
- Watts per Bulb: 29
- Color Temperature: 3000K
- Color Rendering Index (CRI): 90
- Lumen Output: 2000
- Voltage: 120
- Height: 12"
- Width: 6"
- Extension: 8.875"

**LED Technology:**

- AC LED Light Engine - Progress Lighting uses AC (alternating current) LED light engines that eliminate the need for a driver or transformer. This results in a much more reliable light source which can last up to 60,000 hours or 30 years! Additionally, light engines are easily able to be replaced without changing the entire fixture.
- 90+ CRI - The new line of Progress LED fixtures have some of the best looking light and color representation in the industry. All fixtures conform to the California Title 24 lighting standards.
- Energy Star Qualified - All new Progress LED fixtures are energy star® qualified and come with a 5-year warranty. Rest assured that your new light fixture is environmentally-friendly and covered against defects.
- Full Dimming - Progress LED fixtures have flicker-free dimming to 10% brightness with most ELV (electronic low-voltage) dimmers from a variety of manufacturers.

**Additional Progress Lighting Links**

- [View the Manufacturer Warranty](#)
- [Browse All Progress Lighting Products](#)
- [Progress Lighting Cylinder Collection](#)

This Progress Lighting item can also be referenced by the following color/finish specific model #:

Progress Lighting P5641- 20/30K Antique Bronze	Progress Lighting P5641- 31/30K Black	Progress Lighting P5641- 82/30K Metallic Gray
Progress Lighting P5641- 30/30K White		

\*Discontinued model or finish

**More Than Just a Store - Your Reasons to Shop**

Discover the [best online deals](#) for the Progress Lighting P5641-LED and everything else for your home from the leading online home improvement store.

We're committed to providing you with the best assortment of Progress Lighting products with over 400 warehouses across the country.



Live Product Expert 

**Return to Agenda**

## STAFF REPORT

**TO:** Hailey Planning and Zoning Commission  
**FROM:** Lisa Horowitz, Community Development Director  
**RE:** Preliminary Plat – Sonitalena Cottages  
**HEARING:** September 12, 2016

---

**Applicant:** Old Cutters, Inc.  
**Project:** 521 River Street Condominiums  
**Request:** Preliminary Plat approval with recommendation to the City Council by Old Cutters, Inc. for a seven-unit cottage-style residential subdivision called Sonitalena Cottages  
**Location:** 621-681 CD Olena, (Lot 2, Block 4, Old Cutters)  
**Zoning:** General Residential (GR) Zoning District

### Notice

Notice for the public hearing was published in the Idaho Mountain Express on August 24, 2016; the notice was mailed to public agencies and to property owners within 300 feet on August 15, 2016.

### Application

Old Cutters, Inc. has submitted an application for Preliminary Plat approval for a seven lot townhouse subdivision. The design review for the 7 units was approved by the Commission in July. The units are under construction. Lot sizes range from 1,652 to 2,852 square feet, as shown on the preliminary plat. Each unit has a small amount of yard area, and a garage. All of the garages are located off of the alley. Common area of 7,899 is proposed for walkways and landscaping.

### Department Comments

**Life/safety issues:** No Comments

**Water and Sewer issues:** A sewer main extends under the garage building. This extension is not needed, and can be capped off under the common area. A 10-foot easement is needed for the remainder of the sewer main where it traverses through the site. This easement is permitted to be narrower in places where buildings impede the easement width. A plat note shall be added

noting that any landscape or paved area repairs necessary as a result of sewer main maintenance shall be the responsibility of Sonitalena Cottages HOA or Old Cutters, Inc.

An easement shall be added to over the water meter on Lot 4, Block 4 if that meter is to be used for the Sonitalena Cottages.

**Engineering issues:** No Comments

**Standards of Evaluation:**

**CHAPTER 16.04 DEVELOPMENT STANDARDS**

**16.04.010: GENERAL STANDARDS:**

**Applicability:** The configuration and development of proposed subdivisions shall be subject to and meet the provisions and standards found in this Ordinance, the Zoning Ordinance and any other applicable Ordinance or policy of the City of Hailey, and shall be in accordance with general provisions of the Comprehensive Plan.

**16.04.020: STREETS:**

Streets shall be provided in all subdivisions where necessary to provide access and shall meet all standards below.

- *Streets are existing and are provided.*

**L. Private Streets:**

**5. Private streets shall have adequate and unencumbered 10-foot wide snow storage easements on both sides of the street, or an accessible dedicated snow storage easement representing not less than twenty-five (25%) of the improved area of the private street. Private street snow storage easements shall not be combined with, or encumber, required on-site snow storage areas.**

- *1,300 square feet of Snow storage was required as part of the design review approval, and is shown on the preliminary plat, but in a modified location. Staff has requested that the two plans be reconciled.*

**M. Driveways**

- *Not applicable as streets and alley are existing and comply with City standards.*

**N. A parking access lane shall not be considered a street, but shall comply with all regulations set forth in the IFC and other applicable codes and ordinances. Development of lots will be in compliance.**

- *Not applicable. No new parking access lanes are proposed*

**O. Required fire lanes, whether in private streets, driveways or parking access lanes, shall comply with all regulations set forth in the IFC and other applicable codes and ordinances.**

- *Existing.*

**16.04.030: SIDEWALKS AND DRAINAGE IMPROVEMENTS:**

**A. Sidewalks and drainage improvements are required in all zoning districts, except as otherwise provided herein.**

- *Existing sidewalks, curb, and gutter are provided within the street right of way. Drainage was addressed and approved as part of the design review.*

**B. The length of sidewalks and drainage improvements constructed shall be equal to the length of the subject property line(s) adjacent to any Public Street or Private Street.**

- *Existing sidewalks, curb, and gutter are provided within the street right of way.*

**C. New Sidewalks shall be planned to provide pedestrian connections to any existing and future sidewalks adjacent to the site.**

- *N/A*

**D. Sites located adjacent to a Public Street or Private Street that are not currently thru-streets, regardless whether the street may provide a connection to future streets, shall provide sidewalks to facilitate future pedestrian connections.**

- *N/A*

**E. The requirement for sidewalk and drainage improvements are not required for any lot line adjustment. (Ord. 1191, 2015).**

- *N/A*

**16.04.040: ALLEYS AND EASEMENTS:**

**A. Alleys:**

**1. Alleys shall be provided in all Business District and Limited Business District developments where feasible.**

- *This standard is not applicable because the property is within the GR Zoning District.*

**2. The minimum width of an alley shall be twenty six feet (26').**

- *Minimum alley widths have been met.*

**3. All alleys shall be dedicated to the public or provide for public access.**

- *Alleys in Old Cutters are private alleys and are maintained by the HOA.*

**4. All infrastructures to be installed underground shall, where possible, be installed in the alleys platted.**

- *Infrastructure is underground in the alley.*

**5. Alleys in commercial areas shall be improved with drainage as appropriate and which the design meets the approval of the city engineer. The developer shall provide storm sewers and/or drainage areas of adequate size and number to contain any runoff within the streets in the subdivision upon the property in conformance with the latest applicable federal, state and local regulations. The developer shall provide copies of state permits for shallow injections wells (dry wells). Drainage plans shall be reviewed by city staff and shall meet the approval of the city engineer.**

- *N/A*

**6. Dead end alleys shall not be included.**

- *N/A*

**7. Where alleys are not provided, easements of not less than ten feet (10') in width may be required on each side of all rear and/or side lot lines (total width – 20 feet) where necessary for wires, conduits, storm or sanitary sewers, gas and water lines. Easements of greater width may be required along lines, across lots, or along boundaries, where necessary for surface drainage or for the extension of utilities.**

- *N/A*

**B. Easements:**

**Easements, defined as the use of land not having all the rights of ownership and limited to the purposes designated on the plat, shall be placed on the plat as appropriate. Plats shall show the entity to which the easement has been granted. Easements shall be provided for the following purposes:**

- *Some easements are shown on the plat. Two additional easements are needed for the sewer main, and for the water meter. Staff has recommended these as conditions of approval, to be added to the plat prior to Council review.*

**1. To provide access through or to any property for the purpose of providing utilities, emergency services, public access, private access, recreation, deliveries or such other purpose. Any subdivision that borders on the Big Wood River shall dedicate a 20-foot wide fisherman's access easement, measured from the Mean High Water Mark, which shall provide for non-motorized public access. Additionally, in appropriate areas, an easement providing non-motorized public access through the subdivision to the river shall be required as a sportsman's access.**

- *Not applicable.*

**2. To provide protection from or buffering for any natural resource, riparian area, hazardous area, or other limitation or amenity on, under, or over the land. Any subdivision that borders on the Big Wood River shall dedicate a one hundred (100) foot wide riparian setback easement, measured from the Mean High Water Mark, upon which no permanent structure shall be built, in order to protect the natural vegetation and wildlife along the river bank and to protect structures from damage or loss due to river bank erosion. A twenty-five (25) foot wide riparian setback easement shall be dedicated adjacent to tributaries of the Big Wood River. Removal and maintenance of live or dead vegetation within the riparian setback easement is controlled by the applicable bulk requirement of the Flood Hazard Overlay District. The riparian setback easement shall be fenced off during any construction on the property.**

- *Not applicable.*

**3. To provide for the storage of snow, drainage areas or the conduct of irrigation waters. Snow storage areas shall be not less than twenty-five percent (25%) of parking, sidewalk and other circulation areas. No dimension of any snow storage area may be less than 10 feet. All snow storage areas shall be accessible and shall not be located over any above ground utilities, such as transformers.**

- *Snow storage was required of the design review approval to meet the 25% requirement, and will be added to the plat.*

**16.04.050: BLOCKS: The length, width and shape of blocks shall be determined with due regard to adequate building sites suitable to the special needs of the type of use contemplated, the zoning requirements as to lot size and dimensions, the need for convenient access and safe circulations and the limitations and opportunities of topography. (Ord. 1191, 2015).**

- *All above requirements have been met in previous construction of existing facilities.*

**16.04.060: LOTS: All lots shown on the subdivision plat must conform to the minimum standards for lots in the district in which the subdivision is planned. The city will generally not approve single-family residential lots larger than one-half (1/2) acre (21,780 square feet). In the event a single-family residential lot greater than one-half (1/2) acre is platted, irrigation shall be restricted to not**

**more than one-half (1/2) acre, pursuant to Idaho Code section 42-111, and such restriction shall be included as a plat note. District regulations are found in the zoning ordinance.**

- *The existing seven-unit project meets current standards, and was recently approved through design review.*

**16.04.070: ORDERLY DEVELOPMENT:**

- *All above requirements have been met in previous construction of existing facilities.*

**16.04.080: PERIMETER WALLS, GATES AND BERMS: The city of Hailey shall not approve any residential subdivision application that includes any type of perimeter wall or gate that restricts access to the subdivision. This regulation does not prohibit fences on or around individual lots. They city shall also not allow any perimeter landscape berm more than three feet (3') higher than the previously existing (original) grade. (Ord. 1191, 2015).**

- *All above requirements have been met in previous construction of existing facilities.*

**16.04.090: CUTS, FILLS, GRADING AND DRAINAGE:**

- *All above requirements have been met in previous construction of existing facilities.*

**16.04.100: OVERLAY DISTRICTS:**

- *All above requirements have been met in previous construction of existing facilities.*

**16.04.110: PARKS, PATHWAYS AND OTHER GREEN SPACES:**

- *Not applicable. No new pathways or alternatives are proposed at this time.*

**Summary and Suggested Conditions**

The Commission shall review the proposed plat and continue the public hearing, approve, conditionally approve, or deny the preliminary plat. If approved, the plat application will be forwarded to the Council. If the Short Plat process is used, only the Final Plat is required for Council review.

The following conditions are suggested to be placed on any approval of this application:

- a) All Fire Department and Building Department requirements shall be met. Items to be completed at the applicant's sole expense include, but will not be limited to, the following requirements and improvements:
- b) All City infrastructure requirements shall be met as outlined in Section 5 of the Hailey Subdivision Ordinance. Detailed plans for all infrastructure to be installed or improved at or adjacent to the site shall be submitted for Department approval and shall meet

City Standards where required. Infrastructure to be completed at the applicant's sole expense include, but will not be limited to, the following requirements and improvements:

- c) The final plat shall include plat notes # through # as stated on the approved preliminary plat [with the following amendments and additions:
  - i. A ten-foot utility easement for the benefit of the City shall be added where the city sewer main bisects the property. The easement is permitted to be less than 10' only where necessary in areas where impeded by buildings. A plat note shall be added noting that Sonitalina Cottages HOA is responsible for repairs and maintenance of any property damage related to city maintenance of the sewer service lines in the easement area.
  - ii. An easement shall be placed over the water meter located on Lot 1, Block 4, for the benefit of Sonitalena Cottages.
- d) Issuance of permits for the construction of buildings within the proposed subdivision shall be subject to Section 2.02.080 of the Subdivision Ordinance.
- e) All improvements and other requirements shall be completed and accepted, or surety provided pursuant to Sections 16.04 and 16.05 of the Subdivision Ordinance, prior to recordation of the final plat.
- f) The final plat must be submitted within one (1) calendar year from the date of approval of the preliminary plat, unless otherwise allowed for within a phasing agreement.
- g) Any subdivision inspection fees due shall be paid prior to recording the final plat.
- h) The snow storage easements shall be modified to conform to the approved design review.

**Motion Language:**

Approval:

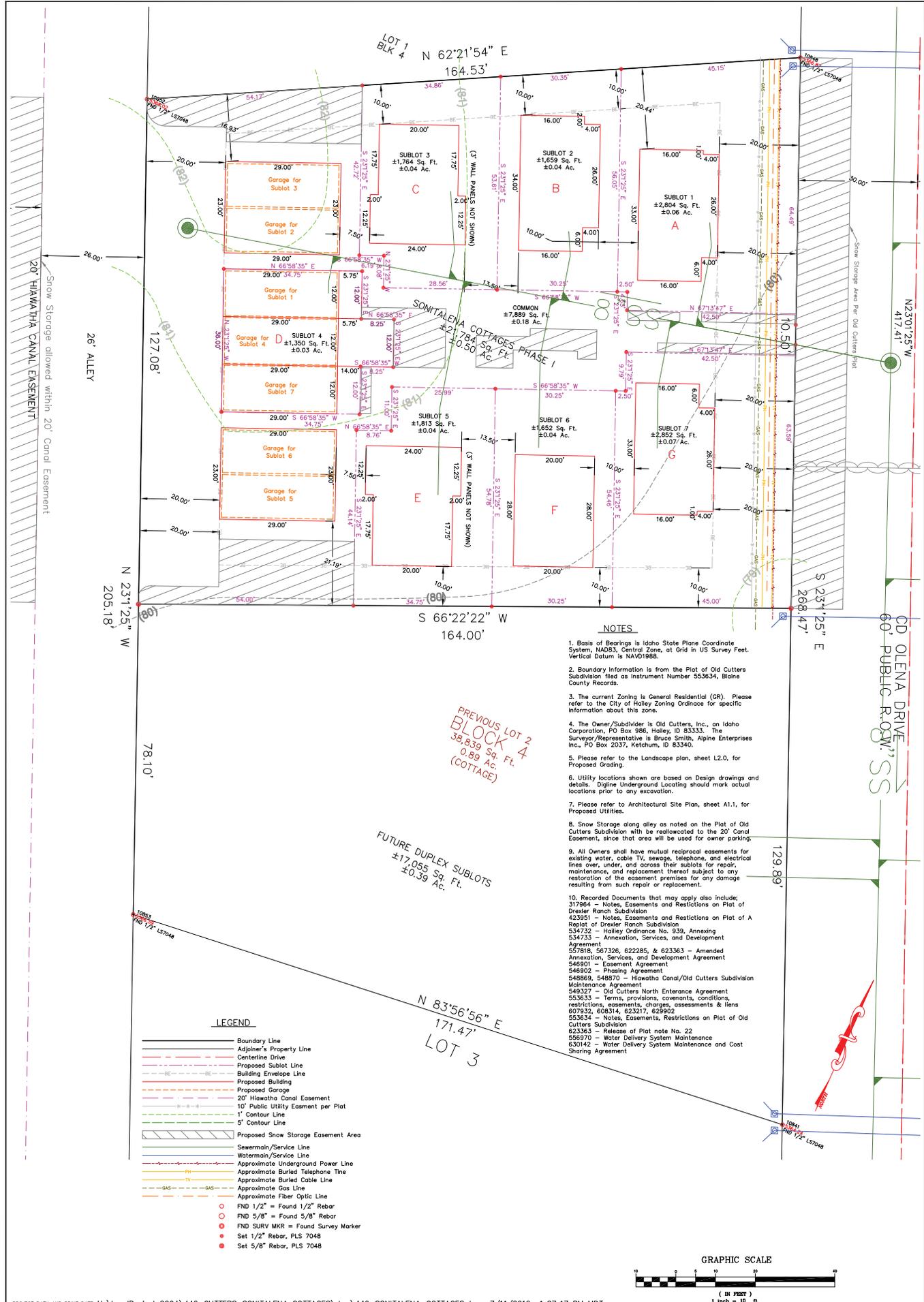
Motion to approve the Preliminary Plat for Sonitalena Cottages by Old Cutters, Inc. for a seven-unit cottage-style residential subdivision, finding that the application meets City Standards.

Denial:

Motion to deny Preliminary Plat for Sonitalena Cottages by Old Cutters, Inc. for a seven-unit cottage-style residential subdivision, finding that \_\_\_\_\_ [the Commission should cite which standards are not met and provided the reason why each identified standard is not met].

Continuation:

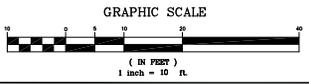
Motion to continue the public hearing upon the Preliminary Plat for Sonitalena Cottages by Old Cutters, Inc. for a seven-unit cottage-style residential subdivision to \_\_\_\_\_ [the Commission should specify a date].



- NOTES**
1. Basis of Bearings is Idaho State Plane Coordinate System, NAD83, Central Zone, at Grid in US Survey Feet. Vertical Datum is NAVD1988.
  2. Boundary information is from the Plat of Old Cutters Subdivision filed as Instrument Number 553634, Blaine County Records.
  3. The current Zoning is General Residential (GR). Please refer to the City of Hailey Zoning Ordinance for specific information about this zone.
  4. The Owner/Subdivider is Old Cutters, Inc., an Idaho Corporation, PO Box 986, Hailey, ID 83333. The Surveyor/Representative is Bruce Smith, Alpine Enterprises Inc., PO Box 2037, Ketchum, ID 83340.
  5. Please refer to the Landscape plan, sheet L2.0, for Proposed Grading.
  6. Utility locations shown are based on Design drawings and details. Digline Underground Locating should mark actual locations prior to any excavation.
  7. Please refer to Architectural Site Plan, sheet A1.1, for Proposed Utilities.
  8. Snow Storage along alley as noted on the Plat of Old Cutters Subdivision with be reallocated to the 20' Canal Easement, since that area will be used for owner parking.
  9. All Owners shall have mutual reciprocal easements for existing water, cable TV, sewage, telephone, and electrical lines over, under, and across their sublots for repair, maintenance, and replacement thereof subject to any restoration of the easement premises for any damage resulting from such repair or replacement.
  10. Recorded Documents that may apply also include:  
 317964 - Notes, Easements and Restrictions on Plat of Dreder Ranch Subdivision  
 423951 - Notes, Easements and Restrictions on Plat of A Ripist of Dreder Ranch Subdivision  
 534732 - Hailey Ordinance No. 939, Annexing Agreement  
 534733 - Annexation, Services, and Development Agreement  
 557818, 567326, 622285, & 623363 - Amended Annexation, Services, and Development Agreement  
 546901 - Easement Agreement  
 546902 - Pricing Agreement  
 546969, 546970 - Hailetha Canal/Old Cutters Subdivision Maintenance Agreement  
 546327 - Old Cutters North Entrance Agreement  
 553633 - Terms, provisions, covenants, conditions, restrictions, easements, charges, assessments & liens  
 607932, 608314, 623217, 623902  
 553634 - Notes, Easements, Restrictions on Plat of Old Cutters Subdivision  
 623363 - Release of Plat note No. 22  
 556970 - Water Delivery System Maintenance  
 630142 - Water Delivery System Maintenance and Cost Sharing Agreement

**LEGEND**

- Boundary Line
- Adjoiner's Property Line
- Centerline Drive
- Proposed Sublot Line
- Building Envelope Line
- Proposed Building
- Proposed Garage
- 20' Hailetha Canal Easement
- 10' Public Utility Easement per Plat
- 1' Contour Line
- 5' Contour Line
- Proposed Snow Storage Easement Area
- Sewermain/Service Line
- Watermain/Service Line
- Approximate Underground Power Line
- Approximate Buried Telephone Line
- Approximate Buried Cable Line
- Approximate Gas Line
- Approximate Fiber Optic Line
- FND 1/2" = Found 1/2" Rebar
- FND 5/8" = Found 5/8" Rebar
- FND SURV MKR = Found Survey Marker
- Set 1/2" Rebar, PLS 7048
- Set 5/8" Rebar, PLS 7048



PROJECT PATH AND PRINT DATE U:\LandProjects2004\446\_CUTTERS\_SONITALENA\_COTTAGES.dwg\446\_SONITALENA\_COTTAGES.dwg 7/11/2016 1:27:17 PM MDT

REVISIONS	NO	DATE	BY



Alpine Enterprises Inc.  
 Surveying, Mapping, and Natural Hazards Consulting  
 221 Northwood Way, Unit A-100  
 P.O. Box 2037, Ketchum, ID 83340 USA  
 (208) 727-1988 727-1987 fax  
 email: bsmith@alpineenterprisesinc.com

A PRELIMINARY PLAT SHOWING  
**SONITALEÑA COTTAGES**  
 A PORTION OF LOT 2, BLOCK 4, OLD CUTTERS SUBDIVISION  
 WITHIN S4, T.2N., R.18E., B.M., CITY OF HAILEY, BLAINE COUNTY, IDAHO  
 PREPARED FOR JOHN CAMPBELL

**Return to Agenda**

## STAFF REPORT

**TO:** Hailey City Council or Hailey Planning & Zoning Commission  
**FROM:** Lisa Horowitz, Community Development Director  
**RE:** Preliminary Plat – Quigley Farm Subdivision  
**HEARING:** September 12, 2016

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**Applicant:** Quigley Farm & Conservation Community, LLC

**Project:** Quigley Farms

**Request:** Preliminary Plat

**Location:** SESE Section 3 & NE & N1/2SE Section 10  
Map 2N R18E Section 3 & 10, Tax Lot 8368

**Property Address:** 1300 Fox Acres Road

**Size:** 205.22 Acres

**Zoning:** Residential/Agricultural (R-5)  
Residential 10 (UIB)  
Mid-Density Residential (R-.40)  
Rural Density (R-40)

### **Background**

In November, 2007, the City of Hailey received an application to annex this property, as well as adjacent property to the east (total application area 1,109 acres). In its initial review, the Commission evaluated a proposed 379 lot development with an 18-hole golf course and in September, 2008 recommended approval of the application subject to 54 conditions. Following the Commission's recommendation, the Council evaluated the application. During the Council's review, the Applicant agreed to eliminate any development above the Quigley pond and seek only one home site in Deadman's Gulch under the jurisdiction of Blaine County. The property proposed to be annexed was reduced to 912 acres ("Quigley Property"). The Council eventually evaluated the application and generally found the application conformed to the Hailey Comprehensive Plan. The Council instructed staff to negotiate with the Applicant and to forward a proposed annexation agreement to the Council so the Council could better evaluate the fiscal impacts of the application. During this time, the Applicant elected to revise the application by deleting the 18-hole golf course and by increasing the property density to 444 lots. In addition, the Applicant eliminated a separate pressurized irrigation system. The Council conducted several more hearings and remanded the application to the Commission to review the changes to the application and to make a recommendation to the Council regarding land use issues and compliance

with the Hailey Comprehensive Plan. Consistent with Council's instructions, the Commission evaluated the revised application and in November, 2011, the Commission submitted a recommendation to the Council. Following receipt of the Commission's recommendation of the 444 lot application, the Council considered testimony of the Blaine County Planning Director, Tom Bergin, who evaluated the potential development density if the Applicant's land was developed in Blaine County. Mr. Bergin explained that the property has a maximum density of 105 lots as a straight lot subdivision and 129<sup>1</sup> lots as a PUD subdivision. The annexation application was denied on June 28, 2012. The Findings of Fact for that denial are attached to this report.

A significantly modified Preliminary Plat and PUD were filed with Blaine County in April/May of 2016. On May 26, the County determined that the application materials were sufficient to proceed with the agency comment period.

This property is within Hailey Area of City Impact (ACI). The City and County have an adopted Area of City Impact Agreement, adopted by the City on November 14, 1994 as Ordinance 649. The ACI Agreement was enacted to:

**" ensure that development of land surrounding Hailey does not directly or indirectly negatively impact Hailey City services, infrastructure or quality of life; all in accordance with the Hailey Comprehensive Plan for the desirable future development of the City of Hailey."**

The Area of City Impact Agreement provides "[t]he City of Hailey's Subdivision rules and regulations shall prevail with the exceptions that the Blaine County Environmental regulations, Flood Plain and Hillside, contained in Blaine County Development Ordinances, shall prevail." Some of the provisions of this Ordinance have been rendered no longer valid by Idaho court decisions. Staffs, the Hailey City Attorney and the County Attorney have met, and determined that 1) Blaine County is the decision-maker for the current application, which was filed with Blaine County but 2) the Hailey Subdivision regulations (except for environmental, floodplain and hillside regulations) apply. Therefore, Hailey is a commenting agency to Blaine County, and the standards of review are, for the most part, the Hailey Subdivision standards. The City of Hailey considered an annexation for this property, as well as adjacent property to the east (total application area 1,109 acres) in 2011-2012. The proposal was for a 444-lot residential development. This application was denied on June 28, 2012. The Findings of Fact for that denial are attached to this report.

### **Notice**

Notice for the public hearing was published in the Idaho Mountain Express on July 13, 2016; the notice was mailed to property owners within 300 feet on July 13, 2016. The item was continued on the record to the September 12, 2016 Commission meeting.

### **Application**

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<sup>1</sup> Blaine County staff has indicated that further analysis of the property could potentially yield a density of 166 units

Listed below is a summary of the development team members for the Quigley Farm Planned Unit Development and Preliminary Plat proposal.

**Owner:** **Quigley Farm & Conservation Community, LLC**  
PO Box 2720  
Ketchum, ID 83340  
Contact: Dave Hennessy  
Telephone: (208) 725-2256  
Email: [dhennesy@hennesyco.com](mailto:dhennesy@hennesyco.com)

**Developer:** **Hennessy Company**  
PO Box 2720  
Ketchum, ID 83340  
Contact: Dave Hennessy  
Telephone: (208) 725-2256  
Email: [dhennesy@hennesyco.com](mailto:dhennesy@hennesyco.com)

**Planning:** **SERA Architects, Inc.**  
359 NW 5<sup>th</sup> Avenue  
Portland, OR 97209  
Telephone: (503) 445-7324  
Contact: Jeff Roberts  
Email: [jeffr@serapdx.com](mailto:jeffr@serapdx.com)

**WH Pacific, Inc.**  
9755 SW Barnes Road, Suite 300  
Portland, OR 97225  
Telephone: (503) 626-0455  
Contact: Tom Jones/Kevin Apperson  
Email: [tjones@whpacific.com/](mailto:tjones@whpacific.com)  
[kapperson@whpacific.com](mailto:kapperson@whpacific.com)

**Civil Engineering & Survey:** **Benchmark Associates, PA**  
100 Bell Drive  
Ketchum, Idaho 83340  
Telephone: (208) 726-9512  
Contact: Garth McClure  
Email: [garth@bma5b.com](mailto:garth@bma5b.com)

**Geotechnical Engineers:** **Butler Associates**  
PO Box 1034  
Ketchum, Idaho 83340  
Contact: Steve Butler  
Telephone: (208) 720-6432  
Email: [svgeotech@gmail.com](mailto:svgeotech@gmail.com)

**Wastewater & Environmental**

**Biohabitats**

412 NW Couch Street, Suite 202  
 Portland, OR 97209  
 Telephone: (800) 220-0919  
 Contact: Pete Munoz  
 Email: [pmunoz@biohabitats.com](mailto:pmunoz@biohabitats.com)

**Environmental:**

**Sawtooth Environmental Consulting, LLC**

PO Box 2707  
 Ketchum, Idaho 83340  
 Contact: Trent Stumph  
 Telephone: (208) 727-9748  
 Email: [trentstumph@cox-internet.com](mailto:trentstumph@cox-internet.com)

**Traffic**

**Lochner Engineering**

941 S Industry Way  
 Meridian, Idaho 93642  
 Telephone: (208) 336-2983  
 Contact: Vance Henry  
 Email: [pmunoz@biohabitats.com](mailto:pmunoz@biohabitats.com)

**2. Applicants Written Statement**

The Owner/Applicant is requesting a Planned Unit Development (PUD) and Preliminary Plat approval from Blaine County for the Quigley Farm PUD project. The proposed project will be located on a single tax lot which is referenced as T2N R18E Sections 3 & 10 Tax Lot 8368.

Based on the proposed plan, the maximum density calculations are identified below:

Use Type	Area Summary (Acres)	Total		
		Base Density (Dwelling Units)	Bonus Density (Dwelling Units)	Total Dwelling Units
Mid-Density (R-.40)	4.7	11.75	2.35	14.10
Residential/Agriculture (R-5)	105.7	21.16	5.29	27.51
Residential 10 (UIB)	19.4	1.94		1.94
Rural Density (R-40)	75.4	1.89		1.89
<i>Subtotal</i>	<i>205.3</i>	<i>36.74</i>		<i>45.44</i>
Community Housing Bonus @ 20%		<b>7.35</b>		<b>7.35</b>
<b>Total</b>	<b>205.3</b>	<b>44.09</b>		<b>51</b>

Based on the number of allowable dwelling units (rounded up to 51 units in total), the Owner/Applicant is proposing the following breakdown of unit types:

- 7 Large Lot Dwellings
- 9 Medium Lot Dwellings
- 10 Cottage Unit Dwellings *(4 of which are identified for community housing)*
- 6 Town House Sublot Dwellings *(2 of which are identified for community housing)*
- 9 Live/Work Mixed Use Dwellings
- 10 Multi-Family Dwellings *(2 of which are identified for community housing)*
- Mixed Live/work uses
- School Use
- Hospitality/Inn Site

In addition to the various land uses, the project will include network of roads, open space, paved and non-paved trails, an area for an active sports complex, winter and summer trailhead parking area, area for kids sledding and an area for winter Nordic. The project incorporates agriculture as a key project concept. Refer to Sheet C-2 for the conceptual layout of the proposed land uses. Also, refer to Sheets P-0 through P-4 for the Preliminary Plat layout.

From the previous packet: the table of contents of this application outlines all the application criteria, impact study, exhibit drawings and appendices submitted for review and approval. Please refer to these sections for more detailed information regarding the project. Please refer to the previous packet for this material.

Attached to this packet: additional information provided by the applicant dated August 25, 2016.

## Standards of Evaluation for a Subdivision

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.010 Development Standards	<p><b>Applicability:</b> The configuration and development of proposed subdivisions shall be subject to and meet the provisions and standards found in this Title, the Zoning Title and any other applicable Ordinance or policy of the City of Hailey, and shall be in accordance with general provisions of the Comprehensive Plan.</p>
			<i>Staff Comments</i>	<p><i>Hailey Comprehensive Plan:</i></p> <p><i>The 2012 Hailey Comprehensive Plan identifies Quigley Canyon as a Neighborhood Service Center. The Comprehensive Plan Land Use Map is attached to this report. A neighborhood Service Center is defined as follows:</i></p> <p style="padding-left: 40px;"><b>5.1.g Neighborhood Service Centers – Small commercial areas serving residents within walking distance (¼ to ½ mile) where commercial use is subordinate to residential uses and to Downtown or Community Activity Areas.</b></p> <p><i>More detail is needed to fully evaluate the following proposed uses: future inn-hospitality site and mixed-use commercial core. The following policies from the Comprehensive Plan are relevant to this project. Staff comments follow each policy.</i></p> <p style="padding-left: 40px;"><b>5.4 Protect open space within and surrounding Hailey, including visible ridgelines, undeveloped hillsides and agricultural areas which help define the unique character of Hailey.</b></p> <p><i>No development is proposed on ridgelines or hillsides. The Concept Plan incorporates agriculture as a long-term component of the site.</i></p> <p style="padding-left: 40px;"><b>5.6 Manage and accommodate population growth by infill development and, when appropriate, minimal expansion by annexation and/or density increases.</b></p> <p><i>The proposal is significantly smaller at 51 units than the 444-lot development reviewed by the City in 2012. Buildout in subsequent phases could allow for a total of 129 units, which is 29% of the previous proposal.</i></p> <p style="padding-left: 40px;"><b>5.7 Encourage development at the densities allowed in the Zoning Code.</b></p> <p><i>Staff has identified which city zoning districts may be appropriate if the project were developed under City codes. See Section 16.04.050 of this report.</i></p> <p style="padding-left: 40px;"><b>8.1 Encourage development that provides opportunities for home ownership and rental homes for individuals and families of all socio-economic levels.</b></p>

				<p><i>The project provides for home ownership, and contains eight (8) community housing units: Block 2, Lot 8 (2 units), Block 3, Lot 6 (duplex), and Block 4 Los 4,5,6 and 7.</i></p> <p><b>9.1 Plan for the long-term utilities, service and facility needs of the City while minimizing impacts to the greatest extent possible.</b></p> <p><i>This question is central to the City’s review of this plan. Comments in this staff report related to utilities, particular water, sewer and roads, will help the Commission determine if this policy has been met.</i></p> <p><b>10.1 Create and maintain a pedestrian and bicycle-friendly community that provides a safe, convenient and efficient multi-modal transportation system for all Hailey residents</b></p> <p><i>A variety of bicycle-pedestrian links are shown on the site plan. They facilities need to be linked back in to City bicycle-pedestrian infrastructure.</i></p> <p><b>12.1 Evaluate whether proposed regulatory or administrative actions may result in an unconstitutional taking of private property.</b></p> <p><i>No actions have been taken at this time.</i></p> <p><b>13.1 Encourage and facilitate the development of school facilities that are planned consistently with the city’s other land use policies.</b></p> <p><i>A future school site for the Sage School is shown on the site plan, but no information on building size, parking or site planning has been shown.</i></p>
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Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	16.04.020	Streets: Streets shall be provided in all subdivisions where necessary to provide access and shall meet all standards below.
			<b>Staff Comments</b>	<ol style="list-style-type: none"> <li>1. As set forth below, several of the standards in the following sections are not met, such as right of way width, typical section, intersection spacing, drainage components.</li> </ol>

				<p>2. <i>The City will not maintain the sidewalks during winter months, and no on street parking is allowed during snow removal periods.</i></p> <p>3. <i>Maintenance of any landscaping within the right of way shall be the responsibility of the development.</i></p> <p>4. <i>Other comments specific to the typical section include:</i></p> <p><i>70' ROW with CURB &amp; GUTTER: Drainage Swale will not work properly during winter months. No maintenance on Sidewalks during winter months.</i></p> <p><i>60' ROW with ON STREET PARKING: Sidewalks in these areas will be covered with snow during snow removal activities.</i></p> <p><i>50' ROW with CURB &amp; GUTTER: Trees along the curb line will impede snow removal and restrict snow storage.</i></p> <p><i>40' ROW: Asphalt travel lanes should be no less than 12' for current snow removal equipment.</i></p> <p>5. <i>20' ROW for "Private Drive/Public Alley": Alley's should be private and maintained by the development. While these 20 ROW's are called out as Alleys, they do not meet the City's definition of alley, and are function as Private Roads. These roadways should be redesigned to meet either the Alley standards or the Privet Road Standards. In addition to other alley deficiencies discussed herein, inverted crowns will build ice pack because of water during winter months.</i></p> <p><i>Snow storage easements, no less than 10 feet wide, shall be established adjacent to all right of ways and private alleys, on future plats.</i></p> <p>6. <i>Huckleberry Trail (road) serves the "potential trail head parking area", but is shown as a private. This area should be privately maintained with a public access easement.</i></p> <p>7. <i>Roadway cross sections on sheet C-6 show a "geothermal line" but no geothermal sources are identified.</i></p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	A.	<p><b>Development Standards: All streets in the subdivision must be platted and developed with a width, alignment, and improvements such that the street is adequate to safely accommodate existing and anticipated vehicular and pedestrian traffic and meets City standards. Streets shall be aligned in such a manner as to provide through, safe and efficient access from and to adjacent developments and properties and shall provide for the integration of the proposed streets with the existing pattern.</b></p>
			<b>Staff Comments</b>	<p><i>Street alignment is acceptable; width and typical section are not acceptable as discussed below. In addition, the developer will be responsible for providing the first "chip and fog seal" on the roadways after construction. Some off-site improvements are needed on Quigley Road and on Fox Acres to provide integration of proposed streets into the existing street pattern and to minimize traffic impacts on city streets. See Section 16.04.070 (C) of this report.</i></p>

?	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>B.</b></p> <p><b>Cul-De-Sacs; Dead-End Streets:</b> Cul-de-sacs or dead end streets shall be allowed only if connectivity is not possible due to surrounding topography or existing platted development. Where allowed, such cul-de-sacs or dead end streets shall comply with all regulations set forth in the IFC and other applicable codes and ordinances. Street rights-of-way extended into un-platted areas shall not be considered dead end streets.</p>
			<p><i>Staff Comments</i></p> <ul style="list-style-type: none"> <li>• Roundabouts need to be designed and constructed to accommodate loaders and other vehicles with wider turning radius for snow removal and other work.</li> <li>• No Cul-De-Sacs or Dead End Streets are proposed. Some streets are stubbed for future connection to another phase of development. See comments in this report about the need for additional connectivity to Quigley Road from one or more of these stub-end streets.</li> </ul>
			<p><b>C.</b></p> <p><b>Access:</b> More than one access may be required based on the potential for impairment of a single access by vehicle congestion, terrain, climatic conditions or other factors that could limit access.</p>
			<p><i>Staff Comments</i></p> <p>The primary project access is shown from Fox Acres Road. Staff finds this to be the most appropriate location for the primary project access. A secondary emergency access is proposed from Fox Acres Road to Quigley Road at the east end of the project. This emergency access will need to be compliant with an all-weather/all-season surface for emergency access and maintained year round. Quigley Road up to the new access will also need to meet the same standards. These improvements have not yet been shown on the plans, and should be all-weather surface. Staff recommends that the separated bike path coming from Fox Acres be continued through to the paved portion of Quigley Road to provide an alternate route to the High School and Community Campus.</p> <p>Mixed use commercial, an inn/hospitality development, and a future school are all proposed at the far easterly end of the development. These uses are proposed to be served off a single access (Quigley Farm Road) with no secondary access shown. Staff recommends Quigley Farm Road and the multi-use trail be extended for connection with Quigley Road to provide access on the eastern end of the project before this area can be developed. This access would offset some of the traffic impacts along Fox Acres Road and Buckhorn Drive, which both see substantial traffic during school days. Offsite Quigley Road will likely need substantial upgrades to be compliant with an all-weather/all-season surface for emergency access and will need to be maintained year round. This secondary access to Quigley Road in the vicinity of the mixed use commercial, inn/hospitality development, and future school site should be showing Phase 1, or these uses should be removed from Phase 1.</p>
<input type="checkbox"/> ?	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>D.</b></p> <p><b>Design:</b> Streets shall be laid out so as to intersect as nearly as possible at right angles and no street shall intersect any other street at less than eighty (80) degrees. Where possible, four way intersections shall be used. A recommended distance of 500 feet, with a maximum of 750 feet, measured from the center line, shall separate any intersection. Alternatively, traffic calming measures including but not limited to speed humps, speed tables, raised intersections, traffic circles or roundabouts, meanderings, chicanes, chokers, and/or neck-downs shall be a part of the street design. Alternate traffic calming measures may be approved with a recommendation by the City Engineer. Three way intersections shall only be permitted where most appropriate or where no other configuration is possible. A minimum distance of 150 feet, measured from the center line, shall separate any 2 three-way</p>

				<b>intersections.</b>
			<i>Staff Comments</i>	<i>Several lots access exclusively from a private alley and do not front on a public or private street as required by 16.04.060.E. If alleys are to function as streets and are recognized as such, then intersection separations, right of way widths, and road typical section requirements are not met.</i>
<input checked="" type="checkbox"/> ?	<input type="checkbox"/>	<input type="checkbox"/>	E.	<b>Centerlines:</b> Street centerlines which deflect more than five (5) degrees shall be connected by a curve. The radius of the curve for the center line shall not be more than 500 feet for an arterial street, 166 feet for a collector street and 89 feet for a residential street. <b>Alternatively,</b> traffic calming measures including but not limited to speed humps, speed tables, raised intersections, traffic circles or roundabouts, meanderings, chicanes, chokers, and/or neck-downs shall be a part of the street design. Alternate traffic calming measures may be approved with a recommendation by the City Engineer.
			<i>Staff Comments</i>	<i>Residential street curves exceed the maximum 89' centerline radius, although this design element can likely be remedied. Roundabouts are part of the street design.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	F.	<b>Width:</b> Street width is to be measured from property line to property line. The minimum street width, unless specifically approved otherwise by the Council, shall be as specified in City Standards for the type of street.
			<i>Staff Comments</i>	<i>Most streets do not conform to City Right of Way requirements from section 18.06.012.C.</i>  <i>As an example, Fox Acres Road is proposed as a 70' wide right of way whereas existing Fox Acres Road adjacent to the development is a 125' wide right of way (80' ROW is required for residential collectors).</i>  <i>Other roads within the development only propose a 50' wide right of way whereas 60' is required per City Ordinance for residential streets. The design as proposed will not connect cleanly to Fox Acres Road, and may not provide for adequate snow storage.</i>
<input checked="" type="checkbox"/> ?	<input type="checkbox"/>	<input type="checkbox"/>	G.	<b>Roadways:</b> Roadway, for the purpose of this section, shall be defined as the area of asphalt from curb face to curb face or edge to edge. Roadway includes areas for vehicle travel and may include parallel or angle in parking areas. The width of roadways shall be in accordance with the adopted City Standards for road construction.
			<i>Staff Comments</i>	<i>Roadway travel surface widths meet the City Standards. The alleys are shown as 20' width. However, if alleys are functioning as residential streets and emergency access then the widths are insufficient.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	H.	<b>Road Grades:</b> Road Grades shall be at least two percent (2%) and shall not generally exceed six percent (6%). Grade may exceed 6%, where necessary, by 1% (total 7%) for no more than 300 feet or 2% (total 8%) for no more than 150 feet. No excess grade shall be located within 200 feet of any other excess grade nor there any horizontal deflection in the roadway greater than 30 degrees within 300 feet of where the excess grade decreases to a 2% slope.

			<b>Staff Comments</b>	<i>Roadway grades meet the City Standards.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	I.	<b>Runoff: The developer shall provide storm sewers and/or drainage areas of adequate size and number to contain any runoff within the streets in the subdivision in conformance with the applicable Federal, State and local regulations. The developer shall provide copies of state permits for shallow injection wells (drywells). Drainage plans shall be reviewed by City Staff and shall meet the approval of the City Engineer. Developer shall provide a copy of EPA's "NPDES General Permit for Storm water Discharge from Construction Activity" for all construction activity affecting more than one acre.</b>
			<b>Staff Comments</b>	<i>A system of curbs, catch basins, drywells, and drainage swales are shown on the plans. The details for catch basins and drywells do not adhere to the City Standards and should be modified to conform. Run off swales drain into the catch basins which may be buried under snow storage on the roadsides, resulting in drainage structures that are inaccessible and poorly performing during key winter transitional months. In addition, the swales on the 70' ROW will only be approximately 1.0' lower than the curb flow line which results in the potential for drainage to encroach onto the roadway during winter months. Drainage structures need to be re-located so they collect runoff from the curb and gutter directly and so they are accessible during winter months.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	J.	<b>Signage: The developer shall provide and install all street and traffic control signs in accordance with City Standards.</b>
?			<b>Staff Comments</b>	<i>Street and traffic signage is not shown on the Roadway Plan will need to be provided at final design. Street name signs shall have a blue background and be per City Standard.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	K.	<b>Dedication; Names: All streets and alleys within any subdivision shall be dedicated for public use, except as provided herein. New street names (public and private) shall not be the same or similar to any other street names used in Blaine County.</b>
			<b>Staff Comments</b>	<ol style="list-style-type: none"> <li>1. Alley ways would be better suited as private and maintained by home Owners association. <ul style="list-style-type: none"> <li>• The inverted crowns shown for the alleys are hard to remove snow from with the city's current equipment</li> <li>• Utilities should run down alley ways for service connections when possible.</li> </ul> </li> <li>2. Streets are dedicated public right of way, alleys are private easements.</li> </ol>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	L.	<b>Private Streets:</b>
?			<b>Staff Comments</b>	<ol style="list-style-type: none"> <li>1. Private street signs shall be blue in background with white lettering per City Standard</li> <li>2. Huckleberry Trail is the only private street proposed, but it serves a public parking area.</li> <li>3. Private alley easements are proposed, and these alleys appear to function as a private street. The private alleys functioning in lieu of private streets do not conform to the requirements below.</li> </ol>

<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	L. 1.	Private streets may be allowed (a) to serve a maximum of five (5) residential dwelling units, (b) within Planned Unit Developments, or (c) within commercial developments in the Business, Limited Business, Neighborhood Business, Light Industrial, Technological Industry, and Service Commercial Industrial districts. Private streets are allowed at the sole discretion of the Council, except that no Arterial or Major Street, or Collector or Secondary Street may be private. Private streets shall have a minimum total width of 36 feet, shall be constructed to all other applicable City Standards including paving, and shall be maintained by an owner's association.
			<i>Staff Comments</i>	<i>According to the plat, the alleys are private easements. The alleys as designed are functioning as private streets and the sole access to several of the lots. The alleys are serving more than five lots, and do not meet minimum lot width, and are deficient in width for EMS. Alleys have no adjacent snow storage. The alleys should be redesigned to meet either the Alley standards or the Private Street standards.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	L. 2.	Private streets, wherever possible, shall provide interconnection with other public streets and private streets.
			<i>Staff Comments</i>	<i>The alleys connect with the proposed public streets.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	L. 3.	The area designated for private streets shall be platted as a separate parcel according to subsection 16.04.060C below. The plat shall clearly indicate that the parcel is unbuildable except for public vehicular and public pedestrian access and ingress/egress, utilities or as otherwise specified on the plat.
			<i>Staff Comments</i>	<i>If the alleys remain as designed, they should be platted as a separate parcel.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	L. 4.	Private street names shall not end with the word "Road", "Boulevard", "Avenue", "Drive" or "Street". Private streets serving five (5) or fewer dwelling units shall not be named.
			<i>Staff Comments</i>	<i>Street names are not shown on the alleys.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	L. 5.	Private streets shall have adequate and unencumbered 10-foot wide snow storage easements on both sides of the street, or an accessible dedicated snow storage easement representing not less than twenty five percent (25%) of the improved area of the private street. Private street snow storage easements shall not be combined with, or encumber, required on-site snow storage areas.
			<i>Staff Comments</i>	<i>Alleys are functioning as private streets and no snow storage is provided.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	L. 6.	Subdivisions with private streets shall provide two (2) additional parking spaces per dwelling unit for guest and/or overflow parking. These spaces may be located (a) within the residential lot (e.g., between the garage and the roadway), (b) as parallel spaces within the street parcel or easement adjacent to the travel lanes, (c) in a designated guest parking area, or (d) as a combination thereof. Guest/overflow parking spaces are in addition to the minimum number of parking spaces required pursuant to chapter 17.09 of this code. The dimension of guest/overflow parking spaces shall be no less than ten feet by twenty feet (10'x20') if angle parking, or ten feet by twenty four feet (10'x24') if parallel. Guest overflow parking spaces shall be improved with asphalt, gravel, pavers, grass block, or other all-weather dustless surface. No part of any required guest/overflow parking spaces shall be utilized for snow storage.

			<b>Staff Comments</b>	<i>Many of the streets as proposed are not wide enough for on-street parking.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>M.</b>	<b>Driveways:</b>
?			<b>Staff Comments</b>	<i>No driveways are proposed at this time. Public streets bordering the large lots should have controlled access. It is possible upon future development of the large lots that an internal street and alleyway system may tie into the currently proposed streets (similar to the Airport West development). Because of this, approach locations should be carefully planned and designated on the plat to limit the size, location, and number of approaches. These approaches should be located far enough away from proposed intersections to prevent conflict and driver distraction.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>M. 1.</b>	<b>Driveways may provide access to not more than two (2) residential dwelling units. Where a parcel to be subdivided will have one lot fronting on a street, not more than one additional single family lot accessed by a driveway may be created in the rear of the parcel. In such a subdivision, where feasible (e.g., no driveway already exists), both lots shall share access via a single driveway. Driveways shall not be named.</b>
			<b>Staff Comments</b>	<i>No driveways are proposed at this time.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>M. 2.</b>	<b>Driveways shall be constructed with an all-weather surface and shall have the following minimum roadway widths:</b> a. Accessing one residential unit: twelve feet (12') b. Accessing two residential units: sixteen feet (16') <b>No portion of the required fire lane width of any driveway may be utilized for parking, above ground utility structures, dumpsters or other service areas, snow storage or any other obstructions.</b>
			<b>Staff Comments</b>	<i>No driveways are proposed at this time.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>M. 3.</b>	<b>Driveways longer than 150 feet must have a turnaround area approved by the Fire Department. Fire lane signage must be provided as approved by the Fire Department.</b>
			<b>Staff Comments</b>	<i>No driveways are proposed at this time.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>M. 4.</b>	<b>Driveways accessing more than one residential dwelling unit shall be maintained by an owner's association, or in accordance with a plat note.</b>
			<b>Staff Comments</b>	<i>No driveways are proposed at this time.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>M. 5.</b>	<b>The area designated for a driveway serving more than one dwelling unit shall be platted as a separate unbuildable parcel, or as a</b>

				dedicated driveway easement. Easements and parcels shall clearly indicate the beneficiary of the easement or parcel and that the property is unbuildable except for ingress/egress, utilities or as otherwise specified on the plat. A building envelope may be required in order to provide for adequate building setback.
			<i>Staff Comments</i>	<i>No driveways are proposed at this time.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	M. 6.	<b>No driveway shall interfere with maintenance of existing infrastructure and shall be located to have the least adverse impact on residential dwelling units, existing or to be constructed, on the lot the easement encumbers and on adjacent lots.</b>
			<i>Staff Comments</i>	<ol style="list-style-type: none"> <li>1. <i>No paving within 10' of any property line in the city right of ways.</i></li> <li>2. <i>No driveways are shown, but it is likely these standards can be achieved.</i></li> </ol>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	N.	<b>Parking Access Lane: A parking access lane shall not be considered a street, but shall comply with all regulations set forth in the IFC and other applicable codes and ordinances.</b>
?			<i>Staff Comments</i>	<i>The only parking access lanes are located within the BCRD trailhead parking lot. The width of those access lanes is not shown. A parking lot for the sports complex should also be considered.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	O.	<b>Fire Lanes: Required fire lanes, whether in private streets, driveways or parking access lanes, shall comply with all regulations set forth in the IFC and other applicable codes and ordinances.</b>
			<i>Staff Comments</i>	<ol style="list-style-type: none"> <li>1. <i>Private alley easements - functioning as streets - are providing sole access to several lots - and are deficient in width for compliance with emergency vehicular access. Additional EMS conflicts could occur within these narrow alleys such as snow removal and parking.</i></li> <li>2. <i>Public Streets - With the exception of the 40' ROW section, all streets provide 13' travel lanes which is acceptable. The 40' ROW section must be reviewed by the Fire Department for comment.</i></li> <li>3. <i>All roads except the 60' ROW sections do not have parking, and "No Parking" signage must be provided on those streets. Most City streets allow for on-street parking, so this concept should be discussed.</i></li> </ol>
<b>16.04.060: Sidewalks and Drainage Improvements</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	A.	<b>Sidewalks and drainage improvements are required in all zoning districts and shall be located and constructed according to applicable city standards, except as otherwise provided herein.</b>
			<i>Staff Comments</i>	<i>60' ROW with on street parking</i>

				<ol style="list-style-type: none"> <li>1. Sidewalk will be covered with snow during snow removal events</li> <li>2. No winter maintenance during winter months.</li> </ol> <p><u>70' ROW CURB &amp; GUTTER</u> No winter maintenance on Sidewalks</p> <p><u>Other:</u> Sidewalks are shown adjacent to most streets but are missing on Huckleberry Trail (road). However, an asphalt pathway is provided generally along one side and a gravel pathway along the other.</p> <p>A system of Catch Basins, Drywells, and Drainage Swales are shown on the plans. The details for Catch Basins and Drywells do not adhere to the City Standards but could potentially be modified to conform. More detail and review will be necessary at final design.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>B.</b>	<b>The length of sidewalks and drainage improvements constructed shall be equal to the length of the subject property line(s) adjacent to any public street or private street.</b>
			<b>Staff Comments</b>	With the exception of Huckleberry Trail (road) all streets have adjacent sidewalks. Additional sidewalks and pathways occur throughout the development. Huckleberry Trail will require a sidewalk. The width of all sidewalks shall be defined by Title 18, Mobility Design and will be based on the classification of the road and other standards.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>C.</b>	<b>New sidewalks shall be planned to provide pedestrian connections to any existing and future sidewalks adjacent to the site.</b>
?			<b>Staff Comments</b>	<p>Pedestrian circulation and connectivity within the site is well planned. However, the following connections are needed:</p> <ol style="list-style-type: none"> <li>1) A pedestrian/bicycle connection should be completed out to Quigley Road.</li> <li>2) A striped bike lane is needed on Quigley Road to connect from the new Quigley neighborhood west to the Wood River Trail.</li> <li>3) A revised parking striping plan is needed on Fox Acres Road from the project entrance south to the Community Campus. An additional crossing of the floodway drainage swale is merited, and is shown on the plans. A sidewalk on the east side of Fox Acres Road should be discussed. (Note that if the Toe of the Hill trail is extended, this could be considered in lieu of a sidewalk on Fox Acres, which is the staff recommendation).</li> <li>4) See also comments under 16.04.110, Parks, Pathways and Other Green Spaces regarding the need for a nonmotorized connection from the Community Campus along the east border, the completion of the Toe of the Hill Trail.</li> </ol>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>D.</b>	<b>Sites located adjacent to a public street or private street that are not currently through streets, regardless whether the street may provide a connection to future streets, shall provide sidewalks to facilitate future pedestrian connections.</b>
?			<b>Staff Comments</b>	<p>Sidewalks are located adjacent to internal street stubs for future connectivity, except:</p> <ol style="list-style-type: none"> <li>1) The sidewalks north of the Fox Acres Roundabout should be extended to the end of the asphalt</li> </ol>

				<p>2) A pedestrian connection should be provided from Antler Drive in the area shown as Open Space/Potential Recreation Use.</p> <p>3) Pedestrian connections from Quigley Road should be discussed by the Commission.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	E.	<b>The requirement for sidewalk and drainage improvements are not required for any lot line adjustment.</b>
			<i>Staff Comments</i>	<i>Not Applicable</i>
<b>16.04.040: Alleys and Easements</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	A.	<b>Alleys:</b>
			<i>Staff Comments</i>	<i>According to the plat alleys are private easements. Alleys are functioning as private streets and the sole access to several of the lots. Alleys are serving more than five lots. Alleys are deficient in width for EMS. Alleys have no adjacent snow storage.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A. 1.	<b>Alleys shall be provided in all Business District and Limited Business District developments where feasible.</b>
?			<i>Staff Comments</i>	<ol style="list-style-type: none"> <li>1. Inverted crowns are hard to remove snow from with the city's current equipment.</li> <li>2. Utilities should run down alley ways for service connections when possible.</li> <li>3. The Mixed Use portions of the proposal would likely be zoned either Neighborhood Business or Limited Business if City zoning were applied. If zoned Neighborhood Business, the area must be limited to a maximum of three (3) acres. These areas are currently proposed as large lots and are expected to provide greater detail at time of design review or re-subdivision.</li> </ol>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	A. 2.	<b>The minimum width of an alley shall be twenty six (26') feet.</b>
			<i>Staff Comments</i>	<i>Private alley easements are proposed as 20'.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	A. 3.	<b>All alleys shall be dedicated to the public or provide for public access.</b>
			<i>Staff Comments</i>	<i>Alleys are proposed as private drive easements and public utility easements according to the plat, but they are annotated as "public alley" on the cross sections shown on sheet C-6. Alleys should remain private and should be maintained by the Home Owners Association. An easement shall cover the area of the alley and shall be dedicated to utilities and public access.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A. 4.	<b>All infrastructures to be installed underground shall, where possible, be installed in the alleys platted.</b>

?				<p><i>Staff Comments</i></p> <ul style="list-style-type: none"> <li>• Water and sewer lines and service connections should be located in alley ways where possible.</li> <li>• Additional infrastructure (cable, utilities) could be placed in the alleys instead of the streets.</li> </ul>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A. 5.	<p>Alleys in commercial areas shall be improved with drainage as appropriate and which the design meets the approval of the City Engineer. The Developer shall provide storm sewers and/or drainage areas of adequate size and number to contain any runoff within the streets in the subdivision upon the property in conformance with the latest applicable Federal, State and local regulations. The developer shall provide copies of state permits for shallow injection wells (drywells). Drainage plans shall be reviewed by City Staff and shall meet the approval of the City Engineer.</p> <p><i>Staff Comments</i></p> <p>No alleys are shown at this time in what may be neighborhood commercial areas.</p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	A. 6.	<p>Dead-end alleys shall not be allowed.</p> <p><i>Staff Comments</i></p> <p>No dead end alleys are shown (other than stubs for future phases).</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A. 7.	<p>Where alleys are not provided, easements of not less than ten (10) feet in width may be required on each side of all rear and/or side lot lines (total width = 20 feet) where necessary for wires, conduits, storm or sanitary sewers, gas and water lines. Easements of greater width may be required along lines, across lots, or along boundaries, where necessary for surface drainage or for the extension of utilities.</p> <p><i>Staff Comments</i></p> <p>Alleys are not platted in all locations, and side/rear yard easements are not generally shown. However, it does not appear these easements are necessary as the utility layout can be accommodated in the adjacent streets.</p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	B.	<p>Easements. Easements, defined as the use of land not having all the rights of ownership and limited to the purposes designated on the plat, shall be placed on the plat as appropriate. Plats shall show the entity to which the easement has been granted. Easements shall be provided for the following purposes:</p> <p><i>Staff Comments</i></p> <p>Snow storage easements are not provided. If the alleys serve as exclusive access to several lots snow storage will be necessary. Frequently a 10' snow storage easement is reserved along public ROW's, and should be considered adjacent the alleys as well, regardless of whether they remain public or are made private.</p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	B. 1.	<p>To provide access through or to any property for the purpose of providing utilities, emergency services, public access, private access, recreation, deliveries or such other purpose. Any subdivision that borders on the Big Wood River shall dedicate a 20-foot wide fisherman's access easement, measured from the Mean High Water Mark, which shall provide for non-motorized public access. Additionally, in appropriate areas, an easement providing non-motorized public access through the subdivision to the river shall be required as a sportsman's access.</p> <p><i>Staff Comments</i></p> <p>See numerous previous comments regarding alley deficiencies.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	B. 2.	<p>To provide protection from or buffering for any natural resource, riparian area, hazardous area, or other limitation or amenity on, under, or over the land. Any subdivision that borders on the Big Wood River shall dedicate a one hundred (100) foot wide riparian setback easement, measured from the Mean High Water Mark, upon which no permanent structure shall be built, in order to protect</p>

				<p>the natural vegetation and wildlife along the river bank and to protect structures from damage or loss due to river bank erosion. A twenty-five (25) foot wide riparian setback easement shall be dedicated adjacent to tributaries of the Big Wood River. Removal and maintenance of live or dead vegetation within the riparian setback easement is controlled by the applicable bulk requirement of the Flood Hazard Overlay District. The riparian setback easement shall be fenced off during any construction on the property.</p>
			<i>Staff Comments</i>	<p>Section 5.6.2 of the Area of City Impact Agreement states that “the City of Hailey’s Subdivision rules and regulations shall prevail with exceptions that the Blaine County Environmental regulations, Floodplain, and Hillside, contained in Blaine County Development Ordinances shall prevail”. Therefore, riparian areas and the Quigley Creek Floodplain will be reviewed by Blaine County.</p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	B. 3.	<p>To provide for the storage of snow, drainage areas or the conduct of irrigation waters. Snow storage areas shall be not less than twenty-five percent (25%) of parking, sidewalk and other circulation areas. No dimension of any snow storage area may be less than 10 feet. All snow storage areas shall be accessible and shall not be located over any above ground utilities, such as transformers.</p>
			<i>Staff Comments</i>	<ol style="list-style-type: none"> <li>1. Snow storage easements are provided, but area calculations are not. Additional information will be necessary to determine compliance.</li> <li>2. Snow storage areas should not have encroachments impeding snow storage.</li> </ol>
<b>16.04.050: Blocks</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.050	<p><b>Blocks: The length, width and shape of blocks shall be determined with due regard to adequate building sites suitable to the special needs of the type of use contemplated, the zoning requirements as to lot size and dimensions, the need for convenient access and safe circulation and the limitations and opportunities of topography.</b></p>
?			<i>Staff Comments</i>	<p>Staff recommends the following zoning designations:</p> <ol style="list-style-type: none"> <li>1. <u>Future School Site</u> (6.17 acres): General Residential (GR): GR is a common school zone district in Hailey. The proposed school site is relatively flat, and will require a connection to Quigley Road. Internal circulation and site planning considerations have not been provided and cannot be evaluated at this time. The school may or may not need a Conditional Use Permit, depending on City or County zoning rules.</li> <li>2. <u>Mixed Use Commercial Core</u> (2 parcels, .92 acres and 1.33 acres, total of 9 live work units): Neighborhood Business (NB) or Limited Business (LB): LB allows for Dwelling Units in Mixed Use buildings, multifamily and a variety of ground floor live-work businesses. Permitted density: 20 units/acre. Alleys and internal circulation will be needed for these lots. Design cannot be evaluated at this time. Neighborhood Business is to provide areas for neighborhood service centers, increase convenience to neighborhood residents and promote other forms of multi-modal transportation and circulation. The uses in the NB district shall be limited commercial uses that will be clearly subordinate to, and support the residential nature of the area. Maximum area for NB centers is 3 acres.</li> </ol>

				<p>3. <u>Future Inn/Hospitality Site</u> (1 parcel. 1.3 acres): Limited Business provides for Bed and breakfast/Inns, and Hotels. These uses are not permitted in Neighborhood Business. This site is bisected by the Mountain Overlay District. It is not clear how the shape of the parcel with regards to the hillside will accommodate the proposed use. Staff recommends that the road be relocated to avoid the road cut described elsewhere in this report.</p> <p>4. <u>Multifamily Housing</u> (.83 acres): General Residential (GR): Allows for multifamily as a permitted use at 10 units/acre. Need to address minimum lot size of 6,000 square feet (proposal has an average lot size of 4,032 square feet).</p> <p>5. <u>Cottage Units</u> (10 lots ranging from 6,500-8,000 lot size): General Residential (GR). Minimum lot size of 6,000 square feet works well.</p> <p>6. <u>Townhouses</u> (3 duplex lots of 10,99_? -12,582 square feet, total of 6 units: General Residential (GR). Minimum lot size of 6,000 square feet works well; multifamily/duplex is permitted.</p> <p>7. <u>Small Lot Single Family</u> (9 lots, lot sizes 9,541-12,734): Limited Residential 1 (LR-1) or General Residential (GR): minimum lot size on either zone district fits (LR-1- 8,000 square feet, GR- 6,000 square feet). GR allows for an ADU.</p> <p>8. <u>Large Lot Single Family</u> (7 lots, 30,928-40,510 square feet): Limited Residential 2 (LR-2): minimum lot size of 12,000 square feet works well. Note that residential irrigation would be limited to areas ½ acre or less.</p>
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**16.04.060: Lots**

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.060	<p>Lots: All lots shown on the subdivision plat must conform to the minimum standards for lots in the District in which the subdivision is planned. The City will generally not approve single-family residential lots larger than one-half (1/2) acre (21,780 square feet). In the event a single-family residential lot greater than one-half (1/2) acre is platted, irrigation shall be restricted to not more than one-half (1/2) acre, pursuant to Idaho Code §42-111, and such restriction shall be included as a plat note. District regulations are found in the Zoning Chapter.</p> <p><i>Staff Comments</i> See above; irrigation should be restricted on the Large Lot SF lots.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A.	<p>If lots are more than double the minimum size required for the zoning district, the Developer may be required to arrange lots in anticipation of future re-subdivision and provide for future streets where necessary to serve potential lots, unless the plat restricts further subdivision.</p> <p><i>Staff Comments</i> Lots identified for Mixed Use, Future Inn/Hospitality and Multifamily Housing may be re-subdivided in the future.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	B.	<p>Double frontage lots shall be prohibited except where unusual topography, a more integrated street plan, or other conditions make it undesirable to meet this requirement. Double frontage lots are those created by either public or private streets, but not by driveways or alleys. Subdivisions providing a platted parcel of 25 feet or more between any street right-of-way and any single row of lots shall not be considered to have platted double frontage lots. The 25-foot wide parcel provided must be landscaped to provide a buffer between the street and the lot(s).</p> <p><i>Staff Comments</i> No double frontage lots are proposed, unless alleys are converted into streets.</p>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	C.	<b>No unbuildable lots shall be platted. Platted areas that are not buildable shall be noted as such and designated as “parcels” on the plat. Green Space shall be clearly designated as such on the plat.</b>
			<i>Staff Comments</i>	<i>Green space and open space parcels are delineated. It is not clear how many of these open space parcels will be used.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	D.	<b>A single flag lot may be permitted at the sole discretion of the Hearing Examiner or Commission and Council, in which the “flagpole” projection is serving as a driveway as provided herein, providing connection to and frontage on a public or a private street. Once established, a flag lot may not be further subdivided, but a lot line adjustment of a flag lot is not considered a further subdivision. The “flagpole” portion of the lot shall be included in lot area, but shall not be considered in determining minimum lot width. The “flagpole” shall be of adequate width to accommodate a driveway as required by this ordinance, fire and other applicable codes. Flag lots within the Townsite Overlay District are not allowed, except where parcels do not have street access, such as parcels adjacent to the ITD right-of-way.</b>
			<i>Staff Comments</i>	<i>No flag lots are shown.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	E.	<b>All lots shall have frontage on a public or private street. No frontage width shall be less than the required width of a driveway as provided under Sections 4.1.11.1 and 4.5.4 of this Ordinance. Townhouse Sub-Lots are excluded from this requirement; provided, however, that Townhouse Developments shall have frontage on a street.</b>
			<i>Staff Comments</i>	<i>Lots in Block 4 should be modified to all have access on a public street. Access just to the alley does not meet this standard.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	F.	<b>In the Townsite Overlay District, original Townsite lots shall be subdivided such that the new platted lots are oriented the same as the original lots, i.e. lots shall be subdivided in such a way as to maintain frontage on both the street and alley. Exceptions may be made for corner properties with historic structures.</b>
			<i>Staff Comments</i>	<i>N/A</i>
<b>16.04.070: Orderly Development</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A.	<b>Phasing Required: Development of subdivisions shall be phased to avoid the extension of City services, roads and utilities through undeveloped land.</b>
			<i>Staff Comments</i>	<i>Future phases are clearly anticipated. A concept plan has been submitted, and is attached to this report. For purposes of analysis at this time, buildout is analyzed at 129 lots, which was the maximum permitted under the County PUD Ordinance in 2012, when the City last reviewed a proposal in this suite. Further analysis on the part of County staff has indicated that the buildout density of the entire site could be 166 units. A total buildout density has not been spelled out in the application materials. The applicant has clarified the square footages of the various non-residential uses, as outlined herein, and in the attachment from the applicant.</i>  <i>Items shown on the Concept Plan but not analyzed at this time due to very limited information provided include Outdoor Amphitheater, Agriculture and Large Lot development.</i>  <i>The applicant has provided a map showing buildout of the phase just north of the current phase, on the portion of the site connecting to</i>

				<p>Quigley Road, attached to this report.</p> <p>Tax Lot 8370 should also be discussed. This lot is southeast of the proposed PUD, behind the High School. It is a key lot for recreational development of trails and access to BLM. It is not clear if it is intended to be retained as a developable parcel or not. While it is not under the same ownership, it appears more tied to this Phase of development than Tax Lot 8369, which is further east out Quigley Canyon.</p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	B.	<p><b>Agreement:</b> Developers requesting phased subdivisions shall enter into a phasing agreement with the City. Any phasing agreement shall be approved and executed by the Council and the Developer on or before the preliminary plat approval by the Council.</p>
			Staff Comments	<p>No phasing plans have been provided. This would be a requirement for the area under one ownership (area just north of the current phase).</p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	C.	<p><b>Mitigation of Negative Effects:</b> No subdivision shall be approved which affects the ability of political subdivisions of the state, including school districts, to deliver services without compromising quality of service delivery to current residents or imposing substantial additional public costs upon current residents, unless the Developer provides for the mitigation of the effects of subdivision. Such mitigation may include, but is not limited to the following:</p> <ol style="list-style-type: none"> <li>1. Provision of on-site or off-site street or intersection improvements.</li> <li>2. Provision of other off-site improvements.</li> <li>3. Dedications and/or public improvements on property frontages.</li> <li>4. Dedication or provision of parks or green space.</li> <li>5. Provision of public service facilities.</li> <li>6. Construction of flood control canals or devices.</li> <li>7. Provisions for ongoing maintenance.</li> </ol>
			Staff Comments	<ol style="list-style-type: none"> <li>1. Provision of on-site or off-site street or intersection improvements</li> <li>2. A Traffic Impact Study (TIS) has been provided that proposes offsite improvements to mitigate development traffic. These improvements include:                     <ul style="list-style-type: none"> <li>• An additional west bound right turn lane at the Fox Acres/SH-75 intersection</li> <li>• A North bound to West bound "acceptance" lane at the Fox Acres/Creekside intersection</li> </ul> </li> </ol> <p>The TIS is reasonably complete, although the list of needed off-site improvements should be expanded. The TIS does not contemplate traffic traveling down Quigley Road should that connection be established, and may need to expand to analyze that area if the connection is provided. In particular, a striped bike lane is needed on Quigley Road, connecting to the Wood River Trail.</p> <p>The TIS shows very minor unmitigated delay at other Fox Acres intersections for the full horizon year.</p> <p>Offsite Considerations on Fox Acres: During sporting or special events numerous vehicles attempt to park on existing Fox Acres Road west of the High School in either the currently striped portion (which is not striped for parking), or in the un-striped portion (which becomes disorganized). There is no pedestrian walkway along those portions of the road that are heavily utilized during these events and there is</p>

			<p>significant potential for conflict between pedestrians and vehicles at peak times. A sidewalk adjacent to existing Fox Acres Road from the community campus entrance to this development may be desirable. In addition, this 44' wide section of roadway contains a two way left turn bay that is an in-efficient use of roadway area. This area could be restriped to either:</p> <p>Alternate A: one 12' lane in each direction plus head in 60° parallel parking(20') on the east side</p> <p>Alternate B: one 12' lane in each direction plus a 10' parallel parking lane on each side (preferred if bike lanes are added in the future)</p> <p>Alternate C: TBD</p> <p>A 5' concrete sidewalk adjacent to the east side is desired although landscape, berms, and right of way may limit the feasibility of this component. (This component could be added interior to the existing curb line by elimination of parallel parking on one side in Alternative B.) Alternately, the Commission should discuss whether completion of the Toe of the Hill Trail is a higher priority than this sidewalk.</p> <p><b>3. Provision of other off-site improvements.</b></p> <p><i>Water:</i> If the project is connected to municipal water, well site(s) could be located on the subject property. Note that the City does not concur with fire protection connected to an irrigation source, for public safety reasons, as described elsewhere in this report.</p> <p><i>Sewer:</i> With regards to wastewater provision, off-site improvements to upgrade one or more sewer lines may be needed. Analysis should be undertaken with regards to the collection system such as a potential bottleneck through a 6 inch "high school" mainline and an 8" mainline in Fox Acres south to the Countryside mainline. A viable option to the potential bottleneck in the 8" mainline is the construction of a new bypass parallel to the Wood River Trail at Fox Acres. Analysis on how this project affects municipal sewer service has not been provided at this time by the applicant.</p> <p><b>4. Dedications and/or public improvements on property frontages.</b></p> <p>As noted above, nonmotorized improvements at Antler Drive and nonmotorized and vehicular improvements on Quigley Road to the second entrance are appropriate.</p> <p><b>5. Dedication or provision of parks or green space.</b></p> <p>The proposed Open Space/Potential Outdoor Recreation Use parcel at the eastern project boundary has been discussed as possibly being dedicated to the Blaine County School District, although this is not delineated on the plans. This dedication should be clearly spelled out as part of any project approvals. Note that development of this land into a sports complex, including developed sports fields, parking and restrooms is a great need, further impacted by full buildout of this project, and should be discussed. Dedications to public entities should not be encumbered by major easements or encumbrances.</p> <p>Various other recreational commitments are made via this PUD, most of which are spelled out in the letter from Blaine County Recreation District and the related attachments.</p>
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			<p><b>6. Provision of public service facilities.</b></p> <p><i>See comments in this report regarding water, sewer and roads. Additionally, all municipal services are anticipated to be impacted if this project remains in the County. City Police and Fire will likely be first responders to incidents due to the proximity of the property. All city streets and neighborhoods will be impacted by vehicular travel, as the only routes in to and out of the project are from City streets. If the project remains in the County the City will not have the benefit of any financial resources typically associated with building permit fees, development impact fees and property tax values to offset impacts to public services.</i></p> <p><b>7. Construction of flood control canals or devices.</b></p> <p><i>This will be analyzed by the County review of the project per the City Area of City Impact Agreement.</i></p> <p><b>8. Provisions for ongoing maintenance.</b></p> <p><i>Despite the fact that the applicant is not currently proposing annexation, the Area of City Impact anticipates, over the long-term, that lands will annex to the City. Per the ACI Agreement, subdivisions in the ACI area must comply with City subdivision standards.</i></p> <p><i>The biggest issue relative to this standard is the ability of the Homeowners Association or other entity to adequately maintain the private sewer system as currently proposed. Implications of errors are extremely high for City municipal water supply. While staff recognizes the proposed treatment system is a viable, attractive, and an environmentally sensitive <u>alternative</u> to conventional treatment, the focus of discussion should be:</i></p> <p style="padding-left: 40px;"><i>Is this alternative system "clearly superior" to the City system? If so, then:</i></p> <p style="padding-left: 80px;"><i>Are these alternative systems what the City wants to operate in addition to the current treatment plant?</i></p> <p style="padding-left: 80px;"><i>If so, how many of these systems around the perimeter of the City does the City want to operate?</i></p> <p style="padding-left: 80px;"><i>Is this system appropriate for areas immediately adjacent to the City, or is it more appropriate for remote areas?</i></p> <p><i>Staff is concerned that City operation of this alternative system, (and therefore setting the precedent for possibly many other future systems), will become a drain on City resources (training, equipment, monitoring, maintenance, additional staff, etc.) if, in the future the City takes over operations.</i></p> <p><i>Staff is concerned that this system is not common in the local area and could be susceptible to unique climate or operational conditions not realized in other communities which could impact its effectiveness. The applicant has provided other cold area examples, but this would be the first operational system in Idaho according to conversations with DEQ.</i></p> <p><i>Staff is concerned about operation of this system by a future home owner organization, and their ability to fund prompt repairs when needed.</i></p>
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			<p><i>If the effluent needs to be delivered to the City system, the pre-treated effluent would cause problems: no pre-treated effluent can be delivered into the City collection system (pre-treated effluent may complicate the City's ongoing biological treatment process)</i></p> <p><i>The City does not support this wastewater design in this location, and it has not meet the standard of review established in 16.05.010, showing that "the alternative is clearly superior in design and effectiveness and will promote the public health, safety and general welfare".</i></p> <p><i>It has not been finalized how all of the public trails are to be maintained by other entities, such as the BCRD, or how the Open Space Parcel on the east side is to be developed and maintained.</i></p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<p><b>D.</b></p> <p><b>When the developer of contiguous parcels proposes to subdivide any portion of the contiguous parcels, an area development plan shall be submitted and approved. The Commission and Council shall evaluate the following basic site criteria and make appropriate findings of fact:</b></p> <ol style="list-style-type: none"> <li><b>1. Streets, whether public or private, shall provide an interconnected system and shall be adequate to accommodate anticipated vehicular and pedestrian traffic.</b></li> <li><b>2. Non-vehicular circulation routes shall provide safe pedestrian and bicycle ways and provide an interconnected system to streets, parks and green space, public lands, or other destinations.</b></li> <li><b>3. Water main lines and sewer main lines shall be designed in the most effective layout feasible.</b></li> <li><b>4. Other utilities including power, telephone, cable, and gas shall be designed in the most effective layout feasible.</b></li> <li><b>5. Park land shall be most appropriately located on the Contiguous Parcels.</b></li> <li><b>6. Grading and drainage shall be appropriate to the Contiguous Parcels.</b></li> <li><b>7. Development shall avoid easements and hazardous or sensitive natural resource areas.</b></li> </ol> <p><b>The commission and council may require that any or all contiguous parcels be included in the subdivision.</b></p>
			<p><b>Staff Comments</b></p> <p><i>Future phases are clearly anticipated. A concept plan has been submitted, and is attached to this report. For purposes of analysis at this time, buildout is analyzed at 129 lots, which is the maximum permitted under the County PUD Ordinance. However, this has not been spelled out in the application materials. It is also unclear at this time the total square footage associated with mixed use, hospitality and school uses. Staff understands that the current applicant does not own all of the contiguous parcels to the east of the current phase. It may be possible to leave details of these phases to future developers, so long as buildout maximums (total number of units, and square footage by use) are established at this time for the purposes of analysis.</i></p> <p><i>Items shown on the Concept Plan but not analyzed at this time due to very limited information provided include Outdoor Amphitheater, Agriculture and Large Lot development.</i></p> <p><i>The applicant has shown buildout of the phase just north of the current phase, on the portion of the site connecting to Quigley Road. This parcel is owned by the Applicant.</i></p> <p><i>Tax Lot 8370 should also be discussed. This lot is southeast of the proposed PUD, behind the High School. It is a key lot for recreational development of trails and access to BLM. It is not clear if it is intended to retain a developable parcel on this lot or not. While it is not under the same ownership, it appears more tied to this Phase of development than Tax Lot 8369, which is further east out Quigley Canyon.</i></p>

<b>16.04.080: Perimeter Walls, Gates and Berms</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.080	<b>The City of Hailey shall not approve any residential subdivision application that includes any type of perimeter wall or gate that restricts access to the subdivision. This regulation does not prohibit fences on or around individual lots. The City shall also not allow any perimeter landscape berm more than 3' higher than the previously existing (original) grade.</b>
?			<i>Staff Comments</i>	<i>No perimeter gate is proposed. The perimeter berm on the southeast corner of the project separating the existing soccer fields from the proposed Future Open Space should be removed, as it unnecessarily separates like uses. The perimeter berm on the south side adjacent to the football field seems appropriate to remain in place.</i>
<b>16.04.090: Cuts, Fills, Grading and Drainage</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A.	<b>Plans Required: Proposed subdivisions shall be carefully planned to be compatible with natural topography, soil conditions, geology and hydrology of the site, as well as to minimize cuts; fills, alterations of topography, streams, drainage channels; and disruption of soils or vegetation. Fill within the floodplain shall comply with the requirements of the Flood Hazard Overlay District of the Zoning Ordinance.</b>
?			<i>Staff Comments</i>	<i>Grading generally appears to be relatively minor relative to existing conditions.</i>  <i>When Quigley Creek is crossed culverts are proposed and minor grading will be necessary.</i>  <i>Grading near the existing stream may fall within a currently designated floodplain (current FEMA flood plain is not shown on the plans). Although the application contains a site specific flood plain study, that study does not represent a regulatory flood plain until adopted by FEMA. This will be further reviewed per the County standards as called for in the Area of City Impact Agreement.</i>  <i>There is substantial cut into the hillside adjacent to Huckleberry Trail (road) on the easterly end of this phase. The applicant has stated that this cut is to accommodate drainage. Staff recommends that this road be located outside of the hillside area. Additionally, the grading plan will need to be completed at the far east end where the contours are truncated.</i>  <i>There is an existing berm at the south end of the playfields on Parcel A. Consideration should be given for the removal of this berm to facilitate greater connectivity to the existing playfields immediately to the south. This berm does not comply with 16.04.080.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A. 1.	<b>A preliminary soil report prepared by a qualified engineer may be required by the Hearing Examiner or Commission and/or Council as part of the preliminary plat application.</b>
			<i>Staff Comments</i>	<i>A geotechnical report has been submitted. Soils report may be necessary at final design but is not requested at this time.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A. 2.	<b>A preliminary grading plan prepared by a civil engineer may be required by the Hearing Examiner or Commission and/or the Council</b>

			<p>as part of the preliminary plat application, to contain the following information:</p> <ul style="list-style-type: none"> <li>a. Proposed contours at a maximum of two (2) foot contour intervals;</li> <li>b. Cut and fill banks in pad elevations;</li> <li>c. Drainage patterns;</li> <li>d. Areas where trees and/or natural vegetation will be preserved;</li> <li>e. Location of all street and utility improvements including driveways to building envelopes; and</li> <li>f. Any other information which may reasonably be required by the Administrator, Hearing Examiner, Commission and/or Council.</li> </ul>
			<p><i>Staff Comments</i></p> <p><i>Preliminary plat content appears sufficient but additional details will be necessary at final design.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>B.</b></p> <p><b>Design Standards: The proposed subdivision shall conform to the following design standards:</b></p>
			<p><i>Staff Comments</i></p>
			<p><b>B. 1.</b></p> <p><b>Grading shall be designed to blend with natural land forms and to minimize the necessity of padding or terracing of building sites, excavation for foundations, and minimize the necessity of cuts and fills for streets and driveways.</b></p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<p><b>B. 2.</b></p> <p><b>Areas within a subdivision which are not well suited for development because of existing soil conditions, steepness of slope, geology or hydrology shall be allocated for Green Space for the benefit of future property owners within the subdivision.</b></p>
			<p><i>Staff Comments</i></p> <p><i>There is substantial cut into the hillside adjacent to Huckleberry Trail (road).</i></p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<p><b>B. 3.</b></p> <p><b>Where existing soils and vegetation are disrupted by subdivision development, provision shall be made by the Developer for Revegetation of disturbed areas with perennial vegetation sufficient to stabilize the soil upon completion of the construction, including temporary irrigation for a sufficient period to establish perennial vegetation. Until such time as the vegetation has been installed and established, the Developer shall maintain and protect all disturbed surfaces from erosion.</b></p>
			<p><i>Staff Comments</i></p> <p><i>No post construction re-vegetation details were identified. Because large portions of this property are either open space, play fields, or right of way, a post construction re-vegetation plan should be considered.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>B. 4.</b></p> <p><b>Where cuts, fills or other excavation are necessary, the following development standards shall apply:</b></p> <ul style="list-style-type: none"> <li>a. Fill areas for structures or roads shall be prepared by removing all organic material detrimental to proper compaction for soil stability.</li> <li>b. Fill for structures or roads shall be compacted to at least 95 percent of maximum density as determined by American Association State Highway Transportation Officials (AASHTO) and American Society of Testing &amp; Materials (ASTM).</li> <li>c. Cut slopes shall be no steeper than two horizontal to one vertical. Subsurface drainage shall be provided as necessary for stability.</li> <li>d. Fill slopes shall be no steeper than three horizontal to one vertical. Neither cut nor fill slopes shall be located on natural slopes of three to one or steeper, or where fill slope toes out within twelve (12) feet horizontally of the top of existing or planned cut slope.</li> <li>e. Tops and toes of cut and fill slopes shall be set back from structures and property lines as necessary to accommodate</li> </ul>

				<b>drainage features and drainage structures.</b>
			<b>Staff Comments</b>	<i>There are steep slopes at the east end of Huckleberry Trail (road), but the actual gradient is difficult to calculate due to the scale of the plans.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>B. 5.</b>	<b>The developer shall provide storm sewers and/or drainage areas of adequate size and number to contain the runoff upon the property in conformance with the applicable Federal, State and local regulations. The developer shall provide copies of state permits for shallow injection wells (drywells). Drainage plans shall be reviewed by planning staff and shall meet the approval of the city engineer. Developer shall provide a copy of EPA’s “NPDES General Permit for Storm-water Discharge from Construction Activity” for all construction activity affecting more than one acre.</b>
<b>?</b>			<b>Staff Comments</b>	<i>Drainage structures are shown on the plans. In general, they seem likely to contain the site runoff but future calculations and final details will need to be developed for final design.</i>  <i>A Storm Water Pollution Prevention Plan (SWPPP) will be necessary for any disturbances greater than 1 acre, and should be provided at final design.</i>

**16.04.100: Overlay Districts**

<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>A.</b>	<b>Flood Hazard Overlay District:</b>
<b>?</b>			<b>Staff Comments</b>	<i>General notes:</i>  <i>Per "Flood Hazard Analysis" page 7 by Brockway Engineering: "Flood Plain delineation, and BFE's. . . have not been adopted by FEMA. . . A Letter of Map Revision (LOMR) must be submitted. . ."</i>  <i>The current 2010 FEMA floodplain must be used for regulatory purposes until a LOMR is approved or approval shall be a condition of approval. The existing 2010 FEMA floodplain is much wider and more extensive than the proposed LOMR floodplain.</i>  <i>The City may want to consider participating in the LOMR to update Quigley creek to reflect the channel and culverts constructed along Fox Acres Road by the High School, adjacent to the project.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>A. 1.</b>	<b>Subdivisions or portions of subdivision located within the Flood Hazard Overlay District shall comply with all provisions of Section 4.10 of the Zoning Ordinance.</b>
			<b>Staff Comments</b>	<i>Section 5.6.2 of the Area of City Impact Agreement states that "the City if Hailey's Subdivision rules and regulations shall prevail with exceptions that the Blaine County Environmental regulations, Floodplain, and Hillside, contained in Blaine County Development Ordinances shall prevail". Therefore, riparian areas and the Quigley Creek Floodplain will be reviewed by Blaine County.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>A. 2.</b>	<b>Subdivisions located partially in the Flood Hazard Overlay District shall have designated building envelopes outside the Flood Hazard Overlay District to the extent possible.</b>
			<b>Staff Comments</b>	<i>Section 5.6.2 of the Area of City Impact Agreement states that "the City if Hailey's Subdivision rules and regulations shall prevail with</i>

				exceptions that the Blaine County Environmental regulations, Floodplain, and Hillside, contained in Blaine County Development Ordinances shall prevail". Therefore, riparian areas and the Quigley Creek Floodplain will be reviewed by County staff under the County environmental regulations.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>A. 3.</b>	<b>Any platted lots adjacent to the Big Wood River or its tributaries shall have designated building envelopes.</b>
			<i>Staff Comments</i>	Section 5.6.2 of the Area of City Impact Agreement states that "the City if Hailey's Subdivision rules and regulations shall prevail with exceptions that the Blaine County Environmental regulations, Floodplain, and Hillside, contained in Blaine County Development Ordinances shall prevail". Therefore, riparian areas and the Quigley Creek Floodplain will be reviewed by County staff under the County environmental regulations.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>B.</b>	<b>Hillside Overlay District:</b>
			<i>Staff Comments</i>	Section 5.6.2 of the Area of City Impact Agreement states that "the City if Hailey's Subdivision rules and regulations shall prevail with exceptions that the Blaine County Environmental regulations, Floodplain, and Hillside, contained in Blaine County Development Ordinances shall prevail". Therefore, hillside will be reviewed by Blaine County. See notes below regarding road cuts.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>B. 1.</b>	<b>Subdivisions or portions of subdivisions located within the Hillside Overlay District shall comply with all provisions of Section 4.14, of the Zoning Ordinance.</b>
			<i>Staff Comments</i>	See above comments in Section 16.04.100(B).  There are no details regarding proposed recreation trails or activity in areas like Parcel G, which will fall on steep hillsides. It is assumed all trails will be designed using best practices.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>B. 2.</b>	<b>Subdivisions located partially in the Hillside Overlay District shall have designated building envelopes outside the Hillside Overlay District.</b>
			<i>Staff Comments</i>	See above comments in Section 16.04.100(B).
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>B. 3.</b>	<b>All approved subdivisions shall contain a condition that a Site Alteration Permit is required before any development occurs.</b>
			<i>Staff Comments</i>	This standard can be addressed at a later time.
<b>16.04.110: Parks, Pathways and Other Green Spaces</b>				
<b>Compliant</b>			<b>Standards and Staff Comments</b>	
<b>Yes</b>	<b>No</b>	<b>N/A</b>	<b>City Code</b>	<b>City Standards and Staff Comments</b>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>A.</b>	<b>Parks and Pathways: Unless otherwise provided, every subdivision shall set aside a Park and/or Pathway(s) in accordance with standards set forth herein.</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>Staff Comments</i>	Numerous open space parcels and pathways are shown on the plans, creating the feel of a pedestrian-oriented development. A letter from Blaine County Recreation District (BCRD) to Blaine County regarding the recreational opportunities presented by this project are attached to this report. Two significant recreational facilities are contemplated:  1) The 6.93 -acre parcel on the southeast side of the property titled "Open Space Potential Outdoor Recreation Use" has verbally been discussed as a parcel to be dedicated to the Blaine County School District for use as an expanded sports complex. This facility is needed, with demand increased by buildout of this project. Currently, city and school district sports fields are heavily used and seasonally in high demand. BCRD has noted over 800 individual enrollments per year in youth soccer and baseball

				<p>combined. Full development of this parcel would include sports field construction, landscaping, irrigation, restrooms, additional parking, a nonmotorized connection to Antler Lane and removal of the berm at the south which separates this area from the current developed fields. Agreements and plat restrictions would need to be developed regarding the drain field located under this open space if this type of wastewater system is ultimately approved.</p> <p>2) The area labeled, "Potential BCRD trailhead Parking Lot" could serve as a regional summer hiking biking trailhead, and a winter Nordic trailhead. The BCRD has noted close to 10,000 south valley Nordic skier days in their correspondence to the County. The connectivity to the High School and the Community Campus creates a strong synergistic link with other youth programs. This trailhead could provide access to a whole new network of single track trails, tied in with the completion of the BLM Travel Management Plan (currently underway). A written statement from the applicant to the BCRD Board is attached to the BCRD letter in this packet. The BCRD has requested that the parking area, multi-use trail easement and Huckleberry Road be moved 20-30 feet northwest to accommodate recreational programming on Parcel G. Parcel G is needed to complete this recreational amenity (approximately 9 acres). The BCRD also believes that the Tax Lot directly south of the current application (owned by Quigley Green, not by Quigley Farms), is needed for long term summer and winter access easements.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A. 1.	<b>Parks:</b>
?			<b>Staff Comments</b>	<p>The 6.93-acre parcel on the southeast side of the property titled "Open Space Potential Outdoor Recreation Use" has verbally been discussed as a parcel to be dedicated to the Blaine County School District for use as an expanded sports complex. This facility is needed, with demand increased by buildout of this project. Currently, city sports fields are heavily used and seasonally in high demand. (See statistics on program enrollment in the letter from BCRD included in this report). Full development of this parcel would include sports field construction, landscaping, irrigation, restrooms, additional parking, a nonmotorized connection to Antler Lane and removal of the berm at the south which separates this area from the current developed fields. Agreements and plat restrictions would need to be developed regarding the drain field located under this open space if this type of wastewater system is ultimately approved, including impacts on the use of the fields if drain field problems were to occur.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A. 1. a.	<p><b>The developer of any subdivision, or any part thereof, consisting of three (3) or more residential lots, including residential townhouse sub-lots and residential condominium units, without regard to the number of phases within the subdivision, shall set aside or acquire land area within, adjacent to or in the general vicinity of the subdivision for Parks. Parks shall be developed within the City of Hailey and set aside in accordance with the following formula:</b></p> <p><b>P = x multiplied by .0277</b></p> <p><b>"P" is the Parks contribution in acres</b></p> <p><b>"x" is the number of single family lots, residential townhouse sub-lots or residential condominium units contained within the plat. Where multi-family lots are being platted with no fixed number of units, "x" is maximum number of residential lots, sub-lots, and units possible within the subdivision based on current zoning regulations</b></p>

			<b>Staff Comments</b>	<p><i>Mathematical calculations of this formula result in the following requirements:</i></p> <p><i>Current Phase: .0277 x 51 = 1.4127 acres</i>  <i>Project Buildout: .0277 x 129 = 3.5733 acres</i></p> <p><i>Clearly the project exceeds this standard between the combination of the sports complex and the summer/winter trailhead. Note that City standards herein would direct that dedicated parks be fully developed by the developer.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>A.1.b</b>	<p><b>In the event the subdivision is located in the Business (B), Limited Business (LB), Neighborhood Business (NB), or Transitional (TN) zoning districts, the area required for a Park shall be reduced by 75%, but in no event shall the area required for a Park/Cultural Space exceed 17.5% of the area of the lot(s) being developed.</b></p>
			<b>Staff Comments</b>	<p><i>Although these zone districts are not contemplated at this time, LB zoning is a possibility for some of the uses proposed. The project likely exceeds this standard.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>A. 2.</b>	<p><b>Pathways: The developer of any subdivision, or any part thereof, shall provide pathways for all trails and paths identified in the master plan that are located on the property to be subdivided or on city property adjacent to the property to be subdivided, and sidewalks required by this ordinance.</b></p>
			<b>Staff Comments</b>	<p>1. <i>There is an asphalt pathway currently existing along the west side of Fox Acres Road (south of the development), but it is separated from the roadway by a drainage channel and is difficult to access from the roadway. Persons attempting to access the existing playfields to the west of the roadway, (or persons traveling along the pathway attempting to access the road), must cross steep slopes with unstable rock and undesirable vegetation. The project proposes to connect to this existing pathway and will provide a connection from that pathway to Fox Acres Road near the southern boundary of the site.</i></p> <p><i>The new "pathway" along Fox Acres Road is proposed as a 6' concrete sidewalk, whereas the existing pathway west of the drainage easement on Fox Acres Road is an approximately 9-10' wide asphalt path. A 9-10' asphalt path is likely more appropriate. The primary multi-use path planned by the applicant in collaboration from BCRD swings through the site to the east. The path along Fox acres is designed as a lesser path. Additionally, the proposed secondary path terminates at Quigley Road but continuing the pathway to Quigley Road is recommended. Correspondence between the BCRD and the applicant confirm that the primary multiuse path will be designed and constructed to BCRD standards, and dedicated to BCRD via a permanent easement for management and maintenance.</i></p> <p>2. <i>The BCRD has indicated a desire to complete the Toe of the Hill Trail connecting the community campus to the `summer/winter trailhead. This could also serve the High School.</i></p>

				<p>3. <i>Offsite Considerations on Fox Acres: During sporting or special events numerous vehicles attempt to park on existing Fox Acres Road west of the High School in either the currently striped portion (which is not striped for parking), or in the un-striped portion (which becomes disorganized). There is no pedestrian walkway along those portions of the road that are heavily utilized during these events and there is significant potential for conflict between pedestrians and vehicles at peak times. A sidewalk adjacent to existing Fox Acres Road from the community campus entrance to this development may be desirable. In addition, this 44' wide section of roadway contains a two way left turn bay that is an in-efficient use of roadway area. This area could be restriped to either:</i></p> <p><i>Alternate A: one 12' lane in each direction plus head in 60° parallel parking (20') on the east side</i>  <i>Alternate B: one 12' lane in each direction plus a 10' parallel parking lane on each side (preferred if bike lanes are contemplated in the future)</i>  <i>Alternate C: TBD</i></p> <p><i>A 5' concrete sidewalk adjacent to the east side is desired although landscape, berms, and right of way may limit the feasibility of this component. (This component could be added interior to the existing curb line by elimination of parallel parking on one side in Alternative B.) Note that the Commission should discuss if the completion of the Toe of the Hill Trail, connecting the Community Campus to the summer/winter trailhead is of a higher priority.</i></p> <p>4. <i>A striped bike lane is needed on Quigley Road, connecting from the current Phase to the Wood River Trail. Further analysis is needed to determine if enough paved area exists to accommodate the bike lanes within the current paved road.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	B.	<p><b>Multiple Ownership: Where a parcel of land is owned or otherwise controlled, in any manner, directly or indirectly:</b></p> <ol style="list-style-type: none"> <li>1. <b>By the same individual(s) or entity(ies), including but not limited to corporation(s), partnership(s), limited liability company(ies) or trust(s), or</b></li> <li>2. <b>By different individuals or entities, including but not limited to corporations, partnerships, limited liability companies or trusts where a) such individual(s) or entity(ies) have a controlling ownership or contractual right with the other individual(s) or entity(ies), or b) the same individual(s) or entity(ies) act in any manner as an employee, owner, partner, agent, stockholder, director, member, officer or trustee of the entity(ies),</b></li> <li>3. <b>Multiple subdivisions of the parcel that cumulatively result in three (3) or more residential lots, townhouse sub-lots or condominium units, are subject to the provisions of this ordinance, and shall provide the required improvements subject to the required standards at or before the platting or development of the lots, sub-lots or units.</b></li> <li>4. <b>Parks and Lands Board: The parks and lands board shall review and make a recommendation to the hearing examiner or commission and council regarding each application subject to the provisions of Section 4.10 of this ordinance. Such recommendation will be based on compliance with the master plan and provisions of this ordinance.</b></li> </ol>

			<b>Staff Comments</b>	<p><i>Future phases are clearly anticipated. A concept plan has been submitted, and is attached to this report. For purposes of analysis at this time. Buildout is analyzed at 129 lots, which is the maximum permitted under the County PUD Ordinance. However, this has not been spelled out in the application materials. At this time, it is also unclear what are the total square footage associated with mixed use, hospitality and school uses. Staff understands that the current applicant does not own all of the contiguous parcels to the east of the current phase. It may be possible to leave details of these phases to future developers, so long as buildout maximums (total number of units, and square footage by use) are established at this time for the purposes of analysis.</i></p> <p><i>Recreational amenities shown on the Concept Plan but not analyzed at this time due to very limited information provided include Outdoor Amphitheater and Nordic trails.</i></p> <p><i>Staff recommends that the Commission consider requesting the applicant to show buildout of the phase just north of the current Phase, on the portion of the site connecting to Quigley Road. This parcel is owned by the Applicant. The site plan is relatively incomplete without a sense of how this Phase lays out. A full subdivision design is not needed at this time, but a preliminary layout and maximum number of units and square footage by use would allow for a more complete analysis of the project.</i></p> <p><i>Parcel G is needed to complete the summer winter trailhead recreational amenity (approximately 9 acres). The BCRD also believes that the Tax Lot directly south of the current application (owned by Quigley Green, not by Quigley Farms), is needed for long term summer and winter access easements.</i></p> <p><i>City of Hailey Parks and Lands Board review is planned for early September.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>C.</b>	<b>Parks and Lands Board: The parks and lands board shall review and make a recommendation to the hearing examiner or commission and council regarding each application subject to the provisions of Section 4.10 of this ordinance. Such recommendation will be based on compliance with the master plan and provisions of this ordinance.</b>
			<b>Staff Comments</b>	<i>Parks and Lands Board review is planned for early September.</i>
			<b>D.</b>	<b>Minimum Requirements:</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>D. 1.</b>	<b>Private Green Space: Use and maintenance of any privately owned green space shall be controlled by recorded covenants or restrictions which run with the land in favor of the future owners of the property within the tract and which cannot be modified without the consent of the council.</b>
			<b>Staff Comments</b>	<i>A number of Open Space parcels are shown on the plat, but not shown how they would be programmed or used. It is assumed that these parcels (with the exception of the 6.93 acre Open Space parcel at the south, and the BCRD trailhead parking area) would be managed by the Homeowners Association.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>D. 2.</b>	<b>Neighborhood Park: A neighborhood park shall include finished grading and ground cover, large grassy areas, trees and shrubs, sheltered picnic table(s), trash container(s), dog station(s), bike racks, park bench(es), parking as required by ordinance, and two or more of the following: play structure, restrooms, an athletic field, trails, hard surface multiple use court (tennis or basketball courts), or gardens that demonstrate conservation principles. Neighborhood Parks shall provide an average of 15 trees per acre, of which at</b>

				<p>least 15% shall be of 4" caliper or greater. A maximum of 20% of any single tree species may be used. Landscaping and irrigation shall integrate water conservation. A neighborhood park shall be deeded to the City upon completion, unless otherwise agreed upon by the developer and City.</p>
			<i>Staff Comments</i>	<p>The 6.93-acre parcel on the southeast side of the property titled "Open Space Potential Outdoor Recreation Use" has verbally been discussed as a parcel to be dedicated to the Blaine County School District for use as an expanded sports complex. This facility is needed, with demand increased by buildout of this project. Currently, city sports fields are heavily used and seasonally in high demand. (See statistics outlined in the letter from BCRD to Blaine County). Full development of this parcel would include sports field construction, landscaping, irrigation, restrooms, additional parking, a nonmotorized connection to Antler Lane and removal of the berm at the south which separates this area from the current developed fields. Agreements and plat restrictions would need to be developed regarding the drain field located under this open space if this type of wastewater system is ultimately approved, including impacts on the use of the fields if drain field problems were to occur.</p> <p>The parcel, if fully developed, would meet the criteria for a "Neighborhood Park" as contemplated in the Subdivision Ordinance.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	D. 3.	<p><b>Mini Park:</b> A mini park shall include finished grading and ground cover, trees and shrubs, picnic table(s), trash container(s), dog station(s), bike racks and park bench(es). All mini parks shall provide an average of 15 trees per acre, of which at least 15% shall be of 4" caliper or greater. A maximum of 20% of any single tree species may be used. Landscaping and irrigation shall integrate water conservation.</p>
			<i>Staff Comments</i>	<p>A Neighborhood Park is more appropriate for this scale of development proposal than a mini park.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	D. 4.	<p><b>Park/Cultural Space:</b> A park/cultural space shall include benches, planters, trees, public art, water features and other elements that would create a gathering place. Connective elements, such as parkways or enhanced sidewalks may also qualify where such elements connect two or more parks or park/cultural spaces.</p>
			<i>Staff Comments</i>	<p>The Outdoor Amphitheater identified on the Concept Plan would meet this criteria. That facility has not been analyzed at this time due to limited details provided.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	D. 5.	<p><b>Pathway:</b> Pathways shall have a minimum twenty foot (20') right-of-way width and shall be paved or improved as recommended by the Parks and Lands Board. Construction of Pathways shall be undertaken at the same time as other public improvements are installed within the development, unless the Council otherwise allows when deemed beneficial for the project. The Developer shall be entitled to receive a Park dedication credit only if the Developer completes and constructs a Pathway identified in the Master Plan, or completes and constructs a Pathway not identified in the Master Plan where the Pathway connects to existing or proposed trails identified in the Master Plan. The City may permit easements to be granted by Developers for Pathways identified in the Master Plan, thereby allowing the Developer to include the land area in the determination of setbacks and building density on the site, but in such cases, a Park dedication credit will not be given. A Developer is entitled to receive a credit against any area required for a Park for every square foot of qualified dedicated Pathway right-of-way.</p>

			<b>Staff Comments</b>	<p>The primary pathway planned to be built to BCRD multiuse trail standards, and dedicated via easement to BCRD for maintenance and programming, is shown within a 20' easement. BCRD has asked that this easement location be shifted slightly, as noted in their correspondence attached to this Report. City of Hailey Parks and Lands Board review is contemplated in September. Other pathway comments include:</p> <ol style="list-style-type: none"> <li>1. There is an asphalt pathway currently existing along the west side of Fox Acres Road (south of the development), but it is separated from the roadway by a drainage channel and is difficult to access from the roadway. Persons attempting to access the existing playfields to the west of the roadway, (or persons traveling along the pathway attempting to access the road), must cross steep slopes with unstable rock and undesirable vegetation. The project proposes to connect to this existing pathway and will provide a connection from that pathway to Fox Acres Road near the southern boundary of the site.</li> <li>2. The new "pathway" along Fox Acres Road is proposed as a 6' concrete sidewalk, whereas the existing pathway west of the drainage easement on Fox Acres Road is an approximately 9-10' wide asphalt path. This is a secondary path, stemming from the primary multi-use trail to be dedicated to the BCRD. Additionally, the proposed path terminates at Quigley Farm Road but continuing the pathway to Quigley Road is recommended. Maintenance responsibility for the entire pathway has not been spelled out in detail, but is likely recommended to be assigned to the BCRD.</li> <li>3. Offsite Considerations on Fox Acres: During sporting or special events numerous vehicles attempt to park on existing Fox Acres Road west of the High School in either the currently striped portion (which is not striped for parking), or in the un-striped portion (which becomes disorganized). There is no pedestrian walkway along those portions of the road that are heavily utilized during these events and there is significant potential for conflict between pedestrians and vehicles at peak times. A sidewalk adjacent to existing Fox Acres Road from the community campus entrance to this development is desired. In addition, this 44' wide section of roadway contains a two way left turn bay that is an in-efficient use of roadway area. This area could be restriped to either:       <ul style="list-style-type: none"> <li>• Alternate A: one 12' lane in each direction plus head in 60° parallel parking (20') on the east side</li> <li>• Alternate B: one 12' lane in each direction plus a 10' parallel parking lane on each side</li> <li>• Alternate C: TBD</li> </ul> </li> <li>4. A 5' concrete sidewalk adjacent to the east side is desired although landscape, berms, and right of way may limit the feasibility of this component. (This component could be added interior to the existing curb line by elimination of parallel parking on one side in Alternative B.) Alternately, the Commission could recommend the nonmotorized connection that would complete the Toe of the Hill trail, in lieu of this sidewalk.</li> <li>5. A 4'-wide striped bike lane is needed on Quigley Road, connecting to the Wood River Trail (in accordance with MUTCD</li> </ol>
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				Standards). 6. The 10' Multi use path should have a constant cross slope. Crowned path ways will complicate snow removal. 7. A winter maintenance plan for pathways has not been determined at this time, but will ultimately be determined by the BCRD.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	E.	<b>Specific Park Standards: All Parks shall meet the following criteria for development, location and size (unless unusual conditions exist that prohibit meeting one or more of the criteria):</b>
			<i>Staff Comments</i>	<i>This standard is not analyzed at this time.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	E. 1.	<b>Shall meet the minimum applicable requirements required by 4.10.04 of this section.</b>
			<i>Staff Comments</i>	<i>This standard is not analyzed at this time.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	E. 2.	<b>Shall provide safe and convenient access, including ADA standards.</b>
			<i>Staff Comments</i>	<i>This standard is not analyzed at this time.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	E. 3.	<b>Shall not be gated so as to restrict access and shall not be configured in such a manner that will create a perception of intruding on private space. If a Park is privately owned and maintained, the use of the park shall not be exclusive to the homeowners, residents or employees of the development.</b>
			<i>Staff Comments</i>	<i>None of the open space is proposed to be gated.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	E. 4.	<b>Shall be configured in size, shape, topography and improvements to be functional for the intended users. To be eligible for Park dedication, the land must, at a minimum, be located on slopes less than 25 degrees, and outside of drainways, floodways and wetland areas. Mini Parks shall not be occupied by non-recreational buildings and shall be available for the use of all the residents or employees of the proposed subdivision.</b>
			<i>Staff Comments</i>	<i>The size, shape and location of the 6.93-acre parcel that could be used as a school district sports complex is in a good location relative to the adjacent sports fields. Discussion of impacts of the wastewater drain fields (if approved) should be undertaken.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	E. 5.	<b>Shall not create undue negative impact on adjacent properties and shall be buffered from conflicting land uses.</b>
			<i>Staff Comments</i>	<i>The 6.93-acre parcel under consideration as a school sports complex is adjacent to the existing Deerfield neighborhood. Staff recommends a pedestrian/nonmotorized connection from Antler Drive. A vehicular connection could result in this access point being used for sports events. Fox Acres is the more appropriate access point, and connects these fields to the other sports fields owned and managed by the School District. The District has verbally indicated that the fields would not be lit.</i>

				<p><i>If the sport fields are not connected to the city water services, the City will not have any means to limit or restrict watering times and days, nor will it have a mechanism to incentivize reduced watering through a tiered rate structure or any payment for water consumption. The city's groundwater resources may be impacted by usage from a shared aquifer, if reuse/reclaimed grey water is not applied to the irrigated areas.</i></p> <p><i>Impacts of the proposed Outdoor Amphitheater are not known at this time.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	E. 6.	<p><b>Shall require low maintenance, or provide for maintenance or maintenance endowment.</b></p> <p><i>Staff Comments</i>  <i>Verbal discussions have taken place with BCSD regarding the sports complex and BCRD regarding the Nordic trails, paved trails and parking lot. Various commitments from the applicant to the BCRD are outlined in the letter from the applicant to the BCRD Board. Final details are yet to be determined.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	F.	<p><b>Specific Pathway Standards: All Pathways shall meet the following criteria for development, location and size (unless unusual conditions exist that prohibit meeting one or more of the criteria):</b></p> <p><i>Staff Comments</i>  <i>See earlier comments in this report regarding pathways.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	F. 1.	<p><b>Shall meet the minimum applicable requirements required by section 4.10.04 of this section.</b></p> <p><i>Staff Comments</i>  <i>See earlier comments in this report regarding pathways.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	F. 2.	<p><b>Shall be connected in a useful manner to other Parks, Pathways, Green Space and recreation and community assets.</b></p> <p><i>Staff Comments</i>  <i>See earlier comments in this report regarding pathways, and various needed connections to Fox Acres Road, Quigley Road and Antler Drive.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	G.	<p><b>Specific Green Space Standards: If green space is required or offered as part of a subdivision, townhouse or condominium development, all green space shall meet the following criteria for development, location and size (unless unusual conditions exist that prohibit meeting one or more of the criteria):</b></p> <p><i>Staff Comments</i>  <i>See earlier comments in this report regarding open space.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	G. 1.	<p><b>Shall meet the minimum applicable requirements required by section 4.10.04 of this section.</b></p> <p><i>Staff Comments</i>  <i>See earlier comments in this report regarding open space.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	G. 2.	<p><b>Public and private green spaces on the same property or adjacent properties shall be complementary to one another. Green space within proposed developments shall be designed to be contiguous and interconnecting with any adjacent Green Space (both existing and potential future space).</b></p> <p><i>Staff Comments</i>  <i>Staff recommends that the Commission consider requesting the applicant to show buildout of the phase just north of the current Phase,</i></p>

				<p><i>on the portion of the site connecting to Quigley Road. This parcel is owned by the Applicant. The site plan is relatively incomplete without a sense of how this Phase lays out. It is not clear if future green spaces are anticipated in this Phase, or not.</i></p> <p><i>Parcel G is needed to complete the summer winter trailhead recreational amenity (approximately 9 acres). The BCRD also believes that the Tax Lot directly south of the current application (owned by Quigley Green, not by Quigley Farms), is needed for long term summer and winter access easements.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>G. 3.</b>	<b>The use of the private green space shall be restricted to Parks, Pathways, trails or other recreational purposes, unless otherwise allowed by the City.</b>
			<i>Staff Comments</i>	<i>It is not clear at this time how the proposed Amphitheater might relate to this standard.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>G. 4.</b>	<b>The private ownership and maintenance of green space shall be adequately provided for by written agreement.</b>
			<i>Staff Comments</i>	<i>This will be addressed in future review.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>H.</b>	<b>In-Lieu Contributions:</b>
			<i>Staff Comments</i>	
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>H. 1.</b>	<b>After receiving a recommendation by the Parks and Lands Board, the Council may at their discretion approve and accept voluntary cash contributions in lieu of Park land dedication and Park improvements.</b>
			<i>Staff Comments</i>	<i>Cash contributions are not anticipated for this project.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>H. 2.</b>	<b>The voluntary cash contributions in lieu of Park land shall be equivalent to the area of land (e.g., square footage) required to be dedicated under this ordinance multiplied by the fair market value of the land (e.g., \$/square foot) in the development at the time of preliminary plat approval by the Council. The City shall identify the location of the property to be appraised, using the standards in Sections 4.10.5.4 and 4.10.5.5 of this ordinance. The appraisal shall be submitted by a mutually agreed upon appraiser and paid for by the applicant.</b>
			<i>Staff Comments</i>	<i>See above.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>H. 3.</b>	<b>Except as otherwise provided, the voluntary cash contribution in lieu of Park land shall also include the cost for Park improvements, including all costs of acquisition, construction and all related costs. The cost for such improvements shall be based upon the estimated costs provided by a qualified contractor and/or vendor. In the Business (B), Limited Business (LB), Neighborhood Business (NB) and Transitional (TN) zoning districts, in-lieu contributions will not include the cost for Park improvements.</b>
			<i>Staff Comments</i>	<i>See above.</i>

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	H. 4.	In-lieu contributions must be segregated by the City and not used for any other purpose other than the acquisition of Park land and/or Park improvements, which may include upgrades and replacement of Park improvements. Such funds should be used, whenever feasible or practicable, on improvements within walking distance of the residents of the subdivision.
			<i>Staff Comments</i>	<i>See above.</i>

**16.05: Improvements Required:**

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	16.05.010	<p><b>Minimum Improvements Required:</b> It shall be a requirement of the Developer to construct the minimum infrastructure improvements set forth herein and any required infrastructure improvements for the subdivision, all to City Standards and procedures, set forth in Title 18 of the Hailey Municipal Code and adopted by ordinance in accordance with the notice and hearing procedures provided in Idaho Code §67-6509. Alternatives to the minimum improvement standards may be recommended for approval by the City Engineer and approved by the City Council at its sole discretion only upon showing that the alternative is clearly superior in design and effectiveness and will promote the public health, safety and general welfare.</p>
			<i>Staff Comments</i>	<p><i>Despite the fact that the applicant is not currently proposing annexation, the Area of City Impact anticipates, over the long-term, that lands will annex to the City. Per the ACI Agreement, subdivisions in the ACI area must comply with City subdivision standards.</i></p> <p><i>This project does not propose to meet minimum infrastructure improvements, in that it does not propose to connect to municipal water or sewer. Analysis of wastewater issues and whether the proposal meets this standard of review are found under Sewer Connections, Standard 16.05.030 of this report. Analysis of water issues and whether the proposal meets this standard of review are found under Water Connections, Standard 16.05.040 of this report.</i></p> <p><i>Section 16.11.010 also allows for exceptions to strict application of City requirements:</i></p> <p style="text-align: center;"><b>Whenever the tract to be subdivided is, in the opinion of the Commission and the Council, of such unusual shape or size, or is surrounded by such development or unusual conditions that the strict application of the requirements contained herein would result in real difficulties and substantial hardships or injustices, the Council may vary or modify such requirements by making findings for their decision so that the Developer is allowed to develop his property in a reasonable manner, while ensuring that the public welfare and interests of the City and surrounding area are protected and the general intent and spirit of this Ordinance are preserved. As used in this Section the phrase "real difficulties and substantial hardships or injustices" shall apply only to situations where strict application of the requirements of this Ordinance will deny to the Developer the reasonable and beneficial use of the property in question, and not in situations where the Developer establishes only</b></p>

				that exceptions will allow a more financially feasible or profitable subdivision.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	A.	Plans Filed, Maintained: Six (6) copies of all improvement plans shall be filed with the City Engineer and made available to each department head. Upon final approval two (2) sets of revised plans shall be returned to the Developer at the pre-construction conference with the City Engineer's written approval thereon. One set of final plans shall be on-site at all times for inspection purposes and to note all field changes upon.
			<i>Staff Comments</i>	<i>N/A at this time: this will be required if this becomes a City project.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	B.	Preconstruction Meeting: Prior to the start of any construction, it shall be required that a pre-construction meeting be conducted with the Developer or his authorized representative/engineer, the contractor, the City Engineer and appropriate City departments. An approved set of plans shall be provided to the Developer and contractor at or shortly after this meeting.
			<i>Staff Comments</i>	<i>N/A at this time: this will be required if this becomes a City project.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	C.	Term Of Guarantee Of Improvements: The developer shall guarantee all improvements pursuant to this Section for no less than one year from the date of approval of all improvements as complete and satisfactory by the city engineer, except that parks shall be guaranteed and maintained by the developer for a period of two years.
			<i>Staff Comments</i>	<i>N/A at this time: this will be required if this becomes a City project.</i>
<b>16.05.020: Streets, Sidewalks, Lighting, Landscaping</b>				
<input checked="" type="checkbox"/> ?	<input type="checkbox"/>	<input type="checkbox"/>	16.05.020	Streets, Sidewalks, Lighting, Landscaping: The developer shall construct all streets, alleys, curb and gutter, lighting, sidewalks, street trees and landscaping, and irrigation systems to meet City Standards, the requirements of this ordinance, the approval of the Council, and to the finished grades which have been officially approved by the city engineer as shown upon approved plans and profiles. The developer shall pave all streets and alleys with an asphalt plant-mix, and shall chip-seal streets and alleys within one year of construction.
			<i>Staff Comments</i>	<ol style="list-style-type: none"> <li>1. Careful consideration should be given in tree placement next to road ways so the trees do not impede snow storage and removal.</li> <li>2. Trees shall be selected from the list provided in the 2013 Wood River Valley Tree Guide and shall be appropriate for areas adjacent to ROWs, when installed along pathways, sidewalks, etc.</li> <li>3. Tree placement should be considered in the vision triangle of any intersection.</li> <li>4. Tree placement shall not block sight lines of any regulatory or guidance.</li> </ol>

				5. No street lighting, street trees, irrigation system details are shown. Other details such as typical street sections and components will need to be revised to conform to City Standards.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	A.	Street Cuts: Street cuts made for the installation of services under any existing improved public street shall be repaired in a manner which shall satisfy the Street Superintendent, shall have been approved by the Hailey City Engineer or his authorized representative, and shall meet City Standards. Repair may include patching, skim coats of asphalt or, if the total area of asphalt removed exceeds 25% of the street area, the complete removal and replacement of all paving adjacent to the development. Street cut repairs shall also be guaranteed for no less than one year. (Ord. 1191, 2015)
			<i>Staff Comments</i>	No street cuts are proposed.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	B.	Signage: Street name signs and traffic control signs shall be erected by the Developer in accordance with City Standard, and the street name signs and traffic control signs shall thereafter be maintained by the City.
?			<i>Staff Comments</i>	<ol style="list-style-type: none"> <li>1. Plans indicating Road Markings and signage will need to be supplied for review.</li> <li>2. No signage is shown.</li> </ol>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	C.	Streetlights: Street lights in the Recreational Green Belt, Limited Residential, General Residential, and Transitional zoning districts are not required improvements. Where proposed, street lighting in all zoning districts shall meet all requirements of Chapter VIII B of the Hailey Zoning Ordinance.
			<i>Staff Comments</i>	No street lights are shown.
<b>16.05.030: Sewer Connections</b>				
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	16.05.030	Sewer Connections: The developer shall construct a municipal sanitary sewer connection for each and every developable lot within the development. The developer shall provide sewer mains of adequate size and configuration in accordance with City standards, and all federal, state, and local regulations. Such mains shall provide wastewater flow throughout the development. All sewer plans shall be submitted to the city engineer for review and approval. At the city engineer's discretion, plans may be required to be submitted to the Idaho Department of Environmental Quality (DEQ) for review and comments.
			<i>Staff Comments</i>	<p>A private "decentralized" onsite community sewer system is proposed rather than a connection to the City of Hailey municipal system.</p> <p><u>Summary of Proposed Treatment System:</u> The onsite system will generally consist of individual structures or houses connected to a septic tank/effluent pump chamber. Effluent is then transmitted via a small diameter collection system to an onsite treatment facility consisting of a pre-filter followed by a constructed wetland and then subsurface infiltration. This system will service properties within development Phases 1 &amp; 2. Future phase 3 will be served by one of three cluster wastewater treatment systems. The infiltration system for Phases 1 &amp; 2 is located on Parcel A Block 1. The location of the other three systems is not determined.</p> <p><u>Issues identified with the proposed Treatment System:</u></p> <ul style="list-style-type: none"> <li>• Is located <b>WITHIN the Wellhead Protection Zone</b> (upstream of a City well)</li> </ul>

			<ul style="list-style-type: none"> <li>• May not have a staff of full time operators constantly monitoring the treatment process</li> <li>• May not be able to generate the funding necessary for immediate repairs or responses to catastrophic failures</li> <li>• Uses equipment and technology inconsistent with current City infrastructure, resulting in additional training, acquisitions, and effort if the City is to become involved in the operation of the facility (which is not recommended)</li> <li>• If adopted, the concept of decentralized treatment could result in many dissimilar systems within or adjacent to the City</li> <li>• Relies on a constructed wetland process not typically used in the Wood River Valley</li> <li>• Is located immediately adjacent to the City boundary and disposes of effluent with no opportunity for additional groundwater dilution before migration into City borders</li> </ul> <p>The system is located immediately adjacent to the City boundary. While a DEQ Level 1 Nutrient Pathogen Study has received preliminary review by DEQ, additional study is necessary. The Level 1 spreadsheet does not directly identify groundwater nitrate plumes, and due to the location of the proposed system it is likely this plume will migrate into the City Limits. A DEQ Level 2 Nutrient Pathogen study must be performed to demonstrate the plume will not encroach into City boundaries.</p> <p>The existing municipal sewer system appears to have sufficient treatment capacity. The current "FIRM" capacity is 0.7 mgd, max capacity is 1.2 mgd, operating around 0.6 mgd currently. Nonetheless, the city should evaluate future infill within the existing City limits in addition to the proposed development. There may be issues with the collection system such as a potential bottleneck through a 6 inch "high school" mainline and an 8" mainline in Fox Acres south to the Countryside mainline. A viable option to the potential bottleneck in the 8" mainline is the construction of a new bypass parallel to the Wood River Trail at Fox Acres.</p> <p>The biggest issue relative to standard 16.05.010 is the ability of the Homeowners Association or other entity to adequately maintain the private sewer system as currently proposed. While staff recognizes the proposed treatment system is a viable, attractive, and an environmentally sensitive <u>alternative</u> to conventional treatment, the focus of discussion should be:</p> <p style="padding-left: 40px;">Is this alternative system "clearly superior" to the City system? If so, then:      Are these alternative systems what the City wants to operate in addition to the current treatment plant?      If so, how many of these systems around the perimeter of the City does the City want to operate?      Is this system appropriate for areas immediately adjacent to the City, or is it more appropriate for remote areas?</p> <p>Staff is concerned that City operation of this alternative system, (and therefore setting the precedent for possibly many other future systems), will become a drain on City resources (training, equipment, monitoring, maintenance, additional staff, etc.) if the City takes over operations in the future.</p> <p>Staff is concerned that this system is not common in the local area and could be susceptible to unique climate or operational conditions not realized in other communities which could impact its effectiveness. The applicant has provided other cold area examples, but this would be the first operational system in Idaho according to conversations with DEQ.</p>
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				<p><i>Staff is concerned about operation of this system by a future home owner organization, and their ability to fund prompt repairs when needed.</i></p> <p><i>If the effluent needs to be delivered to the City system, the pre-treated effluent would cause problems: no pre-treated effluent can be delivered into the City collection system (pre-treated effluent may complicate the City's ongoing biological treatment process)</i></p> <p><i>The City does not support this wastewater design in this location, and it has not meet the standard of review established in 16.05.010, showing that "the alternative is clearly superior in design and effectiveness and will promote the public health, safety and general welfare".</i></p> <p><i>The City standard above requires the project to connect to municipal waste water, but no connection is proposed. Staff recommends connection to the city system. The city's collection system and treatment capacity should be further reviewed and an adequate solution should be determined. Each of the above options would need to be considered for full build-out within the current City limits and all phases of the proposed project area.</i></p> <p><u>Additional Comments:</u></p> <ol style="list-style-type: none"> <li>1. Sewer service locations, system materials, locations, etc., would need to be in conformance with City standards.</li> <li>2. Woodside pump house:             <ol style="list-style-type: none"> <li>a. The proximity of proposed drain fields to Woodside pump house is of concern for the City's well head protection plan.</li> <li>b. Woodside and 3<sup>rd</sup> are routinely measuring at highest Nitrate levels every year. It is not known if this is a direct correlation with the farm fields out Quigley, and if homes and drain fields are introduced, what the impacts will be on nitrate levels for these two wells.</li> </ol> </li> <li>3. If served by the City, all sewer lines should be gravity fed into the City's collection system without the use of lift stations. Sewer mains shall be 8" gravity mains in conformance with City standards.</li> <li>4. If not served by the City, test wells should be installed downstream of the drain fields to monitor water quality before occupancy to establish a baseline, then continuously monitored after occupancy to ensure no contamination.</li> <li>5. All construction shall have DEQ approval.</li> </ol>
<b>16.05.040: Water Connections</b>				
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	A.	<p><b>Requirements: The developer shall construct a municipal potable water connection, water meter and water meter vault in accordance with City Standards, or other equipment as may be approved by the city engineer, for each and every developable lot within the development. The developer shall provide water mains and services of adequate size and configuration in accordance with City Standards, and all federal, state, and local regulations. Such water connection shall provide all necessary appurtenances for fire protection, including fire hydrants, which shall be located in accordance with the IFC and under the approval of the Hailey Fire</b></p>

			<p><b>Chief. All water plans shall be submitted to the city engineer for review and approval. At the City Engineer's discretion, plans may be required to be submitted to the Idaho Department of Environmental Quality (DEQ) for review and comments.</b></p> <p><b>Staff Comments</b></p> <p><i>A private onsite community potable water system is proposed rather than a connection to the City of Hailey municipal system. The proposed potable water system consists of two wells and the associated distribution system. The development also proposes a separate irrigation/fire protection system supplied by a third well. A reuse system is also proposed that will distribute treated effluent for use as irrigation water via a separate small diameter pressure pipe.</i></p> <p><i>Per the report prepared by Brockway Engineering:</i></p> <p><i>"water rights on the project are adequate for irrigation and mitigation for a new water right for potable supply. . . The development has existing water rights to irrigate 276.5 acres of land. . . The project will develop 43 acres of the 276.5 acres, and approximately 12 acres will be retired. . .The available water supply for the project is adequate and reliable to meet all demands for irrigation, potable, and fire suppression requirements." pages 16 &amp; 17.</i></p> <ol style="list-style-type: none"> <li>1. <i>Well Impacts per the report prepared by Brockway Engineering:</i> <p><i>Drawdown associated with operational maximum day demand has an insignificant (&lt; 0.5 feet) effect beyond 500 feet from the well site. Hydraulic conductivities used for the Brockway analysis are from 11 to 150 ft/day. However, staff notes the hydraulic conductivity used for the Nutrient Pathogen Study submitted by the developer is 3000 ft/day. This discrepancy should be explained by the applicant.</i></p> </li> <li>2. <i>The required fire flow is 1500 gpm for two hours, based upon review by the Wood River Fire District. Fire flows will be provided by the non-potable irrigation water system.</i></li> <li>3. <i>All construction and operations shall have DEQ and City approval.</i></li> <li>4. <i>The development should install City compliant water vaults at time of development, but meters are not necessary unless connected to the City system.</i></li> <li>5. <i>Road typical sections show a "geothermal" line but no resources are present.</i></li> <li>6. <i>An inspector shall be selected by the City and paid for by the applicant for City inspection of all water, sewer, and roadway infrastructure during construction.</i></li> <li>7. <i>Quigley Tank</i> <ol style="list-style-type: none"> <li>a. <i>Quigley Canyon will be open public access as a result of this project. A fence and other security measures around the tank will be needed.</i></li> <li>b. <i>The drain and over flow for the tank flow down the canyon and under the existing dirt road into the canal. A modified easement for water drainage may be needed.</i></li> <li>c. <i>The City will need to maintain access to the tank though out the construction of the development.</i></li> </ol> </li> <li>8. <i>Distribution system</i></li> </ol>
--	--	--	--

				<p>a. Plans should show main line valves and sizes of mains (most likely 8")</p> <p>b. If served by municipal water, the developer will need to install individual services lines for each lot.</p> <p>c. Separation of irrigation lines from potable water is a desirable component of the proposed design. These lines need to maintain all the same separation requirements as WW. If irrigation systems use potable water, backflow assemblies, per the City Standards, shall be installed and inspected.</p> <p>d. The fire hydrants off of the non-potable water line will need to be painted purple (represents non-potable water)</p> <p>e. The water services should not go through parcels J and K.</p> <p>9. There should not be a dead end main line to feed one service.</p> <p>The City standard above requires the project to connect to municipal water but no connection is proposed. Despite the fact that the applicant is not currently proposing annexation, the Area of City Impact anticipates, over the long-term, that lands will annex to the City. Per the ACI Agreement, subdivisions in the ACI area must comply with City subdivision standards. Staff believes that is safer, and better protects the public to have the fire suppression system served by the City's potable water system not an irrigation line for the following reasons: 1) An irrigation line must be charged all winter long to keep the fire hydrants active and ready for fire suppression; 2) The City's potable water system has greatly superior system storage capacity and flow for fire suppression without the need of a backup generator and pump.</p> <p>To be served by the municipal system, well ownership/operation should be transferred to the City, which would allow for fire protection off of the potable supply system (applicable water rights would also need to be transferred to the City). These new wells should be constructed to deliver 1500 gpm and be platted on well parcels transferred to the City. The well parcels shall provide DEQ compliant size and setbacks. The irrigation system would continue to be a separate system operated by the development.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<b>B.</b>	<b>Townsite Overlay: Within the Townsite Overlay District, where water main lines within the alley are less than six (6) feet deep, the developer shall install insulating material (blue board insulation or similar material) for each and every individual water service line and main line between and including the subject property and the nearest public street, as recommended by the City Engineer.</b>
			<i>Staff Comments</i>	<i>Not Applicable</i>
<b>16.05.050: Drainage</b>				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>16.05.050</b>	<b>Drainage: The developer shall provide drainage areas of adequate size and number to meet the approval of the street superintendent and the city engineer or his authorized representative. (Ord. 1191, 2015)</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>Staff Comments</i>	<p>Generally speaking, the overall concept appears acceptable but specific components such as Catch Basins and Drywells are not compliant with City Standard and will need to be changed.</p> <p>Storm water infiltration beds must be located at least 25' from public water supply components. The scale of the plans makes this difficult to confirm and will need to be evaluated at final design.</p> <p>Additional details, and storm drain area/infiltration calculations, etc. will be required at final design.</p>

<b>16.05.060: Utilities</b>				
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.05.060	<b>Utilities:</b> The developer shall construct each and every individual service connection and all necessary trunk lines, and/or conduits for those improvements, for natural gas, electricity, telephone, and cable television to the property line before placing base gravel for the street or alley.
			<i>Staff Comments</i>	<i>The concept appears to generally address these issues. Additional utility company comment and engineering details will be required at final design.</i>
<b>16.05.070: Parks, Green Space</b>				
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	16.05.070	<b>Parks, Green Space:</b> The developer shall improve all parks and green space areas as presented to and approved by the hearing examiner or commission and council.
			<i>Staff Comments</i>	<i>Development of the proposed Parks and Green space areas are not shown at this time. Development of the sports complex as part of this approval process is highly desirable.</i>
<b>16.05.080: Installation To Specifications; Inspections</b>				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.05.080	<b>Installation to Specifications; Inspections:</b> All improvements are to be installed under the specifications and inspection of the city engineer or his authorized representative. The minimum construction requirements shall meet City Standards or the Department of Environmental Quality (DEQ) standards, whichever is the more stringent.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>Staff Comments</i>	<i>An inspection schedule will be established for any/all components at final design. All infrastructure must meet City of Hailey Specifications and will be further evaluated in greater detail at final design.</i>  <i>The City will need to select an inspector, to be paid for by the applicant, for all water, sewer, and roadway infrastructure during construction.</i>
<b>16.05.090: Completion; Inspections; Acceptance</b>				
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	A.	<b>Installation of all infrastructure improvements must be completed by the developer, and inspected and accepted by the City prior to signature of the plat by City representatives, or according to a phasing agreement. A post-construction conference shall be requested by the developer and/or contractor and conducted with the developer and/or contractor, the city engineer, and appropriate City departments to determine a punch list of items for final acceptance.</b>
			<i>Staff Comments</i>	<i>This would be required if this were to become a municipal project.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	B.	<b>The developer may, in lieu of actual construction, provide to the City security pursuant to Section 3.3.7, for all infrastructure</b>

				<b>improvements to be completed by developer after the final plat has been signed by City representatives. (Ord. 1191, 2015)</b>
			<i>Staff Comments</i>	<i>Completion of all major infrastructure by the developer is preferred over bonding.</i>
<b>16.05.100: As Built Plans and Specifications</b>				
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.05.100	<b>As Built Plans and Specifications: Prior to the acceptance by the City of any improvements installed by the developer, three (3) sets of "as-built plans and specifications" certified by the developer's engineer shall be filed with the city engineer. (Ord. 1191, 2015)</b>
			<i>Staff Comments</i>	<i>As-builts will be required if this becomes a municipal project.</i>

**Planning and Zoning Commission Recommendations to the City Council on Quigley Farms**

1. Wastewater

- a. The design as proposed is not "clearly superior" to the City municipal system. The project shall connect to City sewer system with no onsite treatment or disposal.
- b. *Install gravity collection system per City Standard*

2. Water

- a. The project shall construct and dedicate wells to the City and connect to municipal water for potable in house use. Fire hydrants shall be connected to potable water main only, with fire flows to be provided by the City system.
- b. All irrigation should be provided as proposed by the applicant via infrastructure that is not connected to the City potable water system. Irrigation lines should meet City separation requirements.
- c. Irrigation on residential lots over ½ acre shall be limited to an area of ½ acre maximum.
- d. All construction and operations shall have DEQ and City approval.
- e. The development should install City compliant water vaults at time of development, but meters are not necessary until connected to the City system.
- f. An inspector shall be selected by the City and paid for by the applicant for City inspection of all water, sewer, and roadway infrastructure during construction.

g. Quigley Tank

- a. Quigley canyon will be open public access as a result of this project. A fence and other security measures around the tank will be needed.
  - b. The drain and over flow for the tank flow down the canyon and under the existing dirt road into the canal. A modified easement for water drainage may be needed.
  - c. The City will require access to the tank though out the construction of the development.
- h. Distribution system
- a. Plans should show main line valves and sizes of mains (most likely 8")
  - b. The developer will need to install individual services lines for each lot for potable water.
  - c. The water services should not go through parcels J and K.
- i. There should not be a dead end main line to feed one service.

3. Land Use

- a. The Neighborhood Business Zone will likely be the most applicable zoning district for the parcels identified as mixed-use commercial. The Neighborhood District contemplates a maximum size of three (3) acres and uses that are subordinate to and support the residential nature of the area. The mixed-use commercial parcels should comply with this maximum, including subsequent project phases.
- b. Post-construction re-vegetation is recommended, as approved by the City.

4. Roads

- a. The second emergency access connecting Fox Acres Road to Quigley Road shall be completed with an all-weather/all-season surface for emergency access and maintained year round. This secondary access shall be shown on the Phase 1 plans.
- b. An additional (third) emergency access in the vicinity of the proposed school site/commercial use/Inn site(s) must be shown on the site plan as part of this phase, and constructed prior to construction of any school, commercial or Inn uses. Alternately, these uses could be shown in a future phase, planned to be concurrent with the completion of this emergency access.
- c. A separated bike path connecting from Fox Acres through to the paved portion of Quigley Road should be included in Phase 1 to provide an alternate bike/pedestrian route to the High School and Community Campus.
- d. A striped bike lane is needed on Quigley Road to connect from the Quigley Farm project to the west to the Wood River Trail.
- e. As set forth below, the project should be brought into compliance with City standards:
  - i) No on street parking is allowed during snow removal periods.

- ii) Maintenance of any landscaping within the right of way shall be the responsibility of the development.
  - iii) Typical Sections should be modified as follows:
    - 70' ROW with CURB & GUTTER: the drainage swale shall be re-designed to meet City standards.
    - 60' ROW with ON STREET PARKING: Sidewalks in these areas will be covered with snow during snow removal activities, unless sidewalks are maintained by the HOA.
    - 50' ROW with CURB & GUTTER: Trees along the curb line may impede snow removal and restrict snow storage; additional snow storage as shown on the 8-25-16 submittal shall be required.
    - 40' ROW: The City does not have a 40' ROW street cross section. These roads should be modified to meet City standards, or a waiver to standards requested.
    - 20' ROW for "Private Drive/Public Alley": While these 20' ROW's are called out as Alleys, they do not meet the City's definition of alley, and are functioning as Private Roads. These roadways should be redesigned to meet either the Alley standards or the Private Road Standards outlined in Title 18. Inverted crowns should be redesigned.
    - Snow storage easements, no less than 10 feet wide, shall be established adjacent to all right of ways and private alleys, on future plats.
  - f. Redesign vehicular circulation system in Blocks 3 and 4 so all lots front on a street and are served by either a public or private street.
  - g. Huckleberry Trail (road) serves the "potential trail head parking area", but is shown as a private road. This area should be privately maintained with a public access easement.
  - h. Roadway cross sections on sheet C-6 shows a "geothermal line" but no geothermal sources are identified.
  - i. Sidewalk widths should meet the standards of Title 18 Mobility Code.
  - j. Sidewalks north of Fox Acres Roundabout should be extended to the edge of the asphalt.
  - k. A pedestrian/nonmotorized connection from Antler Drive into the open space/sports complex should be added to the Phase 1 site plan.
  - l. Details for catch basins and drywells do not adhere to the City Standards and should be modified to conform. Drainage structures need to be re-located so they collect runoff from the curb and gutter directly and so they are accessible during winter months.
  - m. Street signage shall be added at final design.
  - n. Roundabout design should be modified to accommodate loaders and other vehicles with a wider turning radius.
5. Fire protection should be provided from potable water to ensure consistent year round fire flows, adequate backup storage and pressure.

6. Parks and Open Space

- a. Parking and restrooms for the BCSD sports complex needs to be incorporated into the Phase 1 site plan, and the berm separating the existing soccer fields from the proposed sports fields should be removed.
- b. Dedications of public open space should be free of major encumbrances or easements.
- c. The Toe of the Hill Trail should be extended from the BCRD Campus into the project as part of Phase 1.

7. Future Phases

- a. Buildout of future phases should be established as a project maximum.
- b. Tax Lot 8370: clarify that this will not be a developable parcel.

**Motion Language:**

Recommendation to the City Council: Pursuant to the City Area of City Impact Agreement, move to make a recommendation to the City Council on the proposed PUD subdivision shown on the proposed Quigley Farms Preliminary Plat located at SESE Section 3 & NE & N1/2SE Section 10, Map 2N R18E Section 3 & 10, Tax Lot 8368, with the following suggested changes.

Recommendation for Continuation: Move to continue the proposed PUD subdivision shown on the proposed Quigley Farms Preliminary Plat located at SESE Section 3 & NE & N1/2SE Section 10, Map 2N R18E Section 3 & 10, Tax Lot 8368 to the regular Planning and Zoning Commission meeting of September 12, 2016.



QUIGLEY  
MASTERPLAN

OVERALL  
RESIDENTIAL  
PROGRAM

RESIDENTIAL TYPE	#
4 to 5 acre lot	6
3/4 acre lot	28
10,000-12,000 SF lot	41
5,200 to 7,000 SF lot	30
Cottages	19
<b>TOTAL</b>	<b>124</b>

25 MAY 2016





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## HENNESSY COMPANY

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### INTEROFFICE MEMORANDUM

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**TO:** LISA HOROWITZ – CITY OF HAILEY  
**FROM:** DAVID B. HENNESSY  
**SUBJECT:** QUIGLEY FARM PUD  
**DATE:** 8/25/2016  
**CC:** TOM BERGIN, KATHY GROTTO – BLAINE COUNTY

---

Lisa:

We wanted to provide the additional information requested by the Planning & Zoning Commission as well as address issues raised in the Staff Report of dated 8/1/16 in order to ensure that the Commission can agree on recommended comments at the meeting on 9/12/16.

As requested please find attached additional information on the Constructed Wetlands Wastewater treatment system. There was also a question about the impact of the drainfields on the Woodside Well head protection zone. The effluent being introduced into the drainfield will be Class A wastewater effluent which can be used for spray and drip irrigation. Also according to IDWR Well Construction standards Rules Section 35, Public Water Supply wells must be a minimum of 300 feet from a LSAS drainfield and we are approximately 3,000 feet away.

We would also like to address some various items listed in the Staff Report:

1. Square Footage of Core area
  - a. Retail/Commercial – 5,000-7,000 sf
  - b. Non-Profit Space – 2,000-4,000 Sf
  - c. Inn/Hospitality – 3,000–5,000 sf
  - d. Sage School – 24,000sf
2. Road Sections –
  - a. 70' ROW- Drywell and catch basin details can be updated to adhere to City of Hailey standards. The swales are designed to treat the 25-

year design storm before the water is re-introduced to ground water. The swale is not expected to provide this function during the winter months. The drywells will continue to function during the winter months because the infiltrative interface reaches below the anticipated frost line. To reduce the chance of drywell grates at the end of the swales completely freezing shut, we can specify beehive (dome) grates instead of flat grate. Beehive grates will allow water to drain into drywells even during freeze/thaw conditions when flat grates often completely freeze shut. If the drywell is unable to infiltrate the entire melt volume at once, the swale will offer temporary storage. Drainage would be more likely to back up into the road if a catch basin were to drain directly into a drywell without the additional storage provided in the swale as we are proposing.

- b. 60' ROW- Sidewalk snow removal can be the responsibility of the home owners association. A maintenance company will be hired for snow removal.
  - c. 50' ROW- Trees can be spaced so as not to impede snow storage. As snow storage calculations in attached PDF show, the right-of-way and adjacent easements offer more snow storage than required by the City of Hailey. Additional snow storage, if necessary, is available in open spaces. See response to number 6 below.
  - d. 40' ROW- The paved width will be 20' per Blaine County road standards for a private road. There will be 26' of drive-able width including gravel shoulders.
  - e. 20' Alley- Valley road sections are often utilized when space adjacent to paved roadways are unavailable. Space for drainage ditches adjacent to the pavement is not available. With adequate snow removal, ice buildup should be comparable to gutters.
  - f. Huckleberry Trail- We can add a public access easement on the privately maintained road.
3. Lots 7-9 and 12-14 do not front a public street for Fire and EMS access – The proposed alleys meet the minimum road width (20') and corner radius requirements for fire access. A fire access easement can be granted with the private drive easement or parcel on both alleys.
4. Drainage details - Drywell and catch basin details can be updated to adhere to City of Hailey standards. The swales are designed to treat the 25-year design storm before the water is re-introduced to ground water. The swale is not expected to provide this function during the winter months. The drywells will continue to function during the winter months because the infiltrative interface reaches below the anticipated frost line. To reduce the chance of drywell grates at the end of the swales completely freezing shut, we can

specify beehive (dome) grates instead of flat grate. Beehive grates will allow water to drain into drywells even during freeze/thaw conditions when flat grates often completely freeze shut. If the drywell is unable to infiltrate the entire melt volume at once, the swale will offer temporary storage. Drainage would be more likely to back up into the road if a catch basin were to drain directly into a drywell without the additional storage provided in the swale as we are proposing.

5. Alley widths –The proposed alleys, while narrower than the City of Hailey standard, are sized to allow emergency services access. The narrower paved width and lack of curb and gutter will help deter through traffic by differentiating from the roads designed for through traffic.
6. Snow storage in alleys – We can add a 10' public utility and snow storage easement on either side of the road.
7. Snow storage calculations –Within the proposed right-of-ways, we will exceed the required 25% snow storage requirement. See attached for a markup of the sections calculating snow storage. In addition to the available snow storage within the right-of-ways and snow storage easements, the proposed development contains over 11 acres of open space (parcels B, D, F, H, J, K) that can be used to store snow. Please see attached drawing.
8. Cut for Huckleberry Trail – The proposed road alignment for Huckleberry Trail was designed to maintain the downhill roadbed edge. To accommodate for drainage, a cut into the hillside is required. Shifting Huckleberry Trail north and narrowing the proposed lodging parcel would reduce the proposed cut slope. An alternative would be to construct a retaining wall to reduce the disturbance of native landscaping.

Please let me know if you have any questions or need any additional information.

# SNOW STORAGE CALCULATIONS

BY: SKS 7/29/16

1/cw  $\frac{10' + 14' + 14' + 10'}{90'} = 53\% > 25\%$  ✓

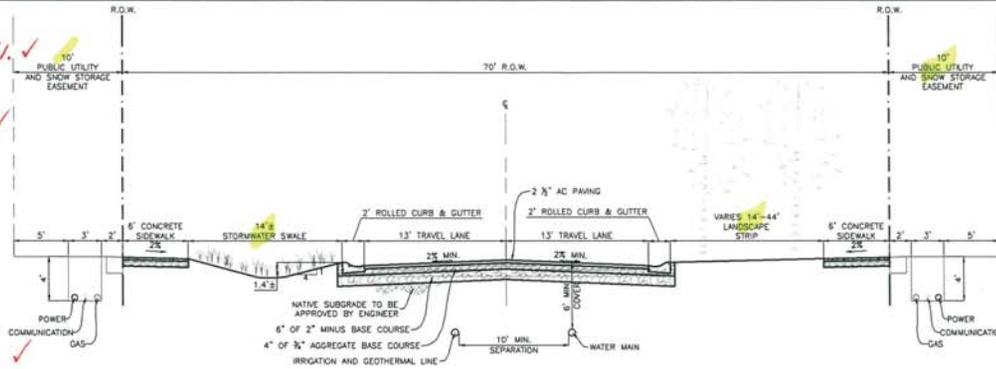
2/cw  $\frac{10' + 1.5' + 1.5' + 10'}{80'} = 28\% > 25\%$  ✓

3/cw  $\frac{10' + 10'}{40'}$  (EASEMENTS TO BE ADDED) = 50% > 25% ✓

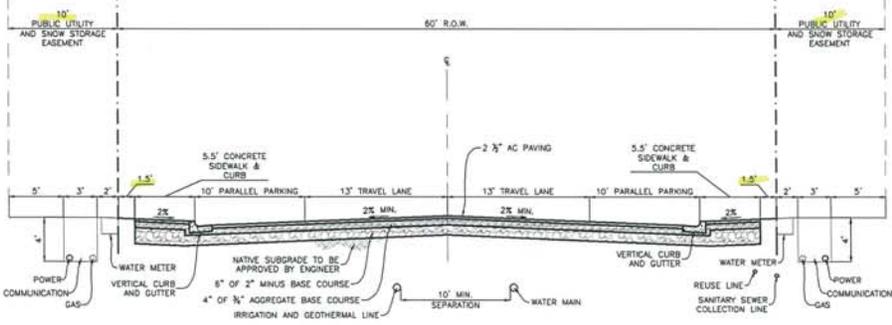
4/cw  $\frac{3' + 3'}{20'} = 30\% > 25\%$  ✓

5/cw  $\frac{10' + 4' + 4' + 10'}{70'} = 40\% > 25\%$  ✓

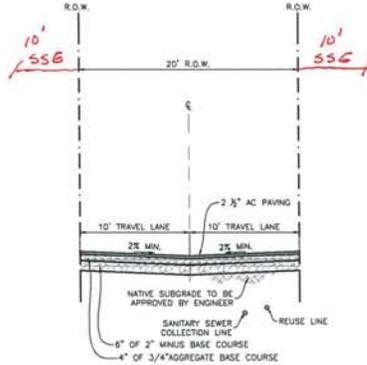
6/cw  $\frac{10' + 8' + 8' + 10'}{60'} = 60\% > 25\%$  ✓



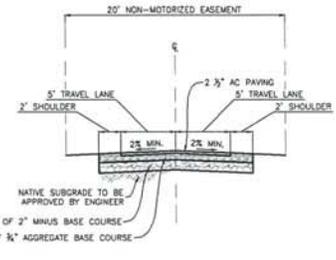
1 70' R.O.W. - CURB & GUTTER  
SCALE: 1/8" = 1'-0"



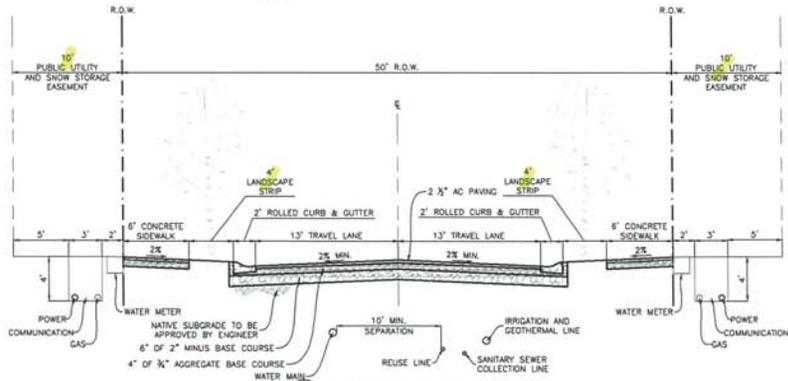
2 60' R.O.W. - ON STREET PARKING  
SCALE: 1/8" = 1'-0"



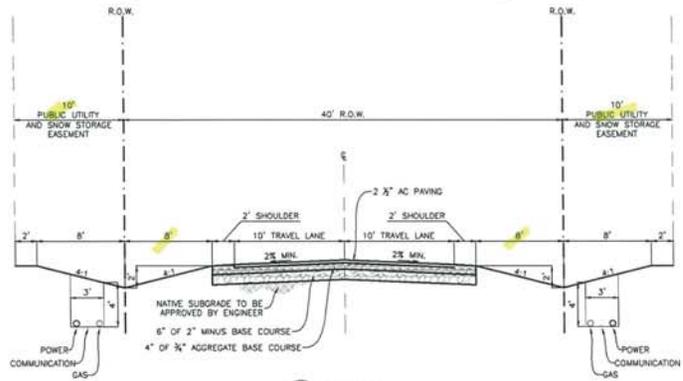
3 20' PUBLIC ALLEY  
SCALE: 1/8" = 1'-0"



4 10' MULTI-USE PATH  
SCALE: 1/8" = 1'-0"



5 50' R.O.W. - CURB & GUTTER  
SCALE: 1/8" = 1'-0"



6 40' R.O.W.  
SCALE: 1/8" = 1'-0"

NO.	DESCRIPTION	DATE	BY



PREPARED BY: BENCHMARK ASSOCIATES, P.A.  
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PRELIMINARY ROAD SECTIONS  
QUIGLEY FARM P.U.D.  
T2N, R18E, SEC 10, B.M., BLAINE COUNTY, IDAHO  
PREPARED FOR: HENNESSY COMPANY

DRAWN BY: SKS  
DESIGNED BY: JDS  
CHECKED BY: SB  
DATE: 02/19/16  
PROJECT NO.: 15222

SHEET NUMBER

C-6

P.U.D. SUBMITTAL 02/19/16

August 2016

**QUIGLEY FARM  
WASTEWATER TREATMENT SYSTEM  
HAILEY, IDAHO**

Wastewater Infrastructure  
Summary Information





## Design Team Contributors:

Biohabitats, Inc.

SERA Architects

WH Pacific

Benchmark Associates

Butler Associates, Inc.

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# 1 Overview

## 1.1 Background and Purpose

Quigley Farm is a rural neighborhood development dedicated to building a thriving and restorative community through collaborations and partnerships that produce sustainable economic, social, and environmental benefits. Quigley Farm is located within Quigley Canyon, which lies immediately adjacent to the eastern edge of the City of Hailey. The project encompasses over 1,500 acres of mixed agricultural, sage, and forested land nestled into the rolling hills and smaller side canyons that comprise the foothills of the Pioneer Mountains. The site has a water right from 1880 and perennial stream flowing through the property.

The full buildout of the development includes the following elements:

- Residential: 136 lots, ranging in size from 5,000 square feet to 5 acres.
- Mixed Use Village Core: 40,000 square feet of retail and office space
- Inn/Hotel: 25 keys
- Sage School: Relocating existing school to serve grades 6-12 and 4 associated residential lots for school housing
- Higher Education Campus
- Event Space: capacity for 250 guests
- Community Park: ballfields and green space
- Trailhead: access to trails with public restrooms open year round
- Greenhouses and Community Agriculture

Construction of the development will occur in three phases. Phases 1 and 2 are located on the west side of the development and encompass the majority of the project elements listed above. Phase 3 consists of three clusters of eight larger residential lots on the northeast side of the development. Figure 1 depicts the three phases.

Although the project site is located outside of the city limits of Hailey, there is an existing city owned well and water infrastructure on the site and the project will utilize either the municipal water supply or a privately owned well onsite. The project will follow the Idaho Administrative Code (IDAPA) 58.01.03 – Individual/Subsurface Sewage Disposal Rules, IDAPA 58.01.16 – Wastewater Rules, IDAPA 58.01.17 – Recycled Water Rules, and *Idaho Department of Environmental Quality (IDEQ) Technical Guidance Manual for Individual and Subsurface Sewage Disposal Systems* (April 2015) for large soil absorption systems (LSAS).

Wastewater will be collected, treated, and dispersed or reused onsite. Wastewater will be collected from residences, commercial properties, schools, and public restrooms via a Small Diameter Collection System that utilizes STEP/STEG technology (septic tank effluent pumping / septic tank effluent gravity tanks). There will be a community-scale wastewater treatment system located near the Community Park/Ballfields to serve Phases 1 and 2, engineered to produce a Class A non-potable water supply. Residential lots in Phase 3 will be served by one of three cluster wastewater treatment systems. The wastewater treatment systems will be sized and designed to meet the demands of the full buildout. Construction of wastewater infrastructure will likely occur with the phases of development, with additional land area set aside to meet future development.



Figure 1. Quigley Farm Development Phasing

## 1.2 Wastewater System Effluent Criteria

Wastewater will be treated onsite to a Class A standard for reuse as a non-potable water supply. Possible uses of the non-potable supply include spray and subsurface drip irrigation, toilet flushing for the school, commercial, and public restrooms, and for making artificial snow. The Class A wastewater effluent reuse standard is detailed in the Idaho Administrative Code (IDAPA) 58.01.17, Recycled Water Rules, and is outlined below.

### Water Quality Requirements:

- Median number of total coliform organisms does not exceed 2.2 organisms/100mL, as determined from the bacteriological results of the last 7 days for which analyses have been completed. No sample shall exceed 23 organisms/100mL in any confirmed sample.
- Daily arithmetic mean of all measurements of turbidity shall not exceed 2 NTU, and turbidity shall not exceed 5 NTU at any time.
- Total nitrogen shall not exceed 30 mg/L for residential irrigation and other non-recharge uses.
- pH shall be between 6.0 and 9.0.
- BOD<sub>5</sub> shall not exceed 10mg/L for residential irrigation and other non-recharge uses.

### Monitoring Requirements:

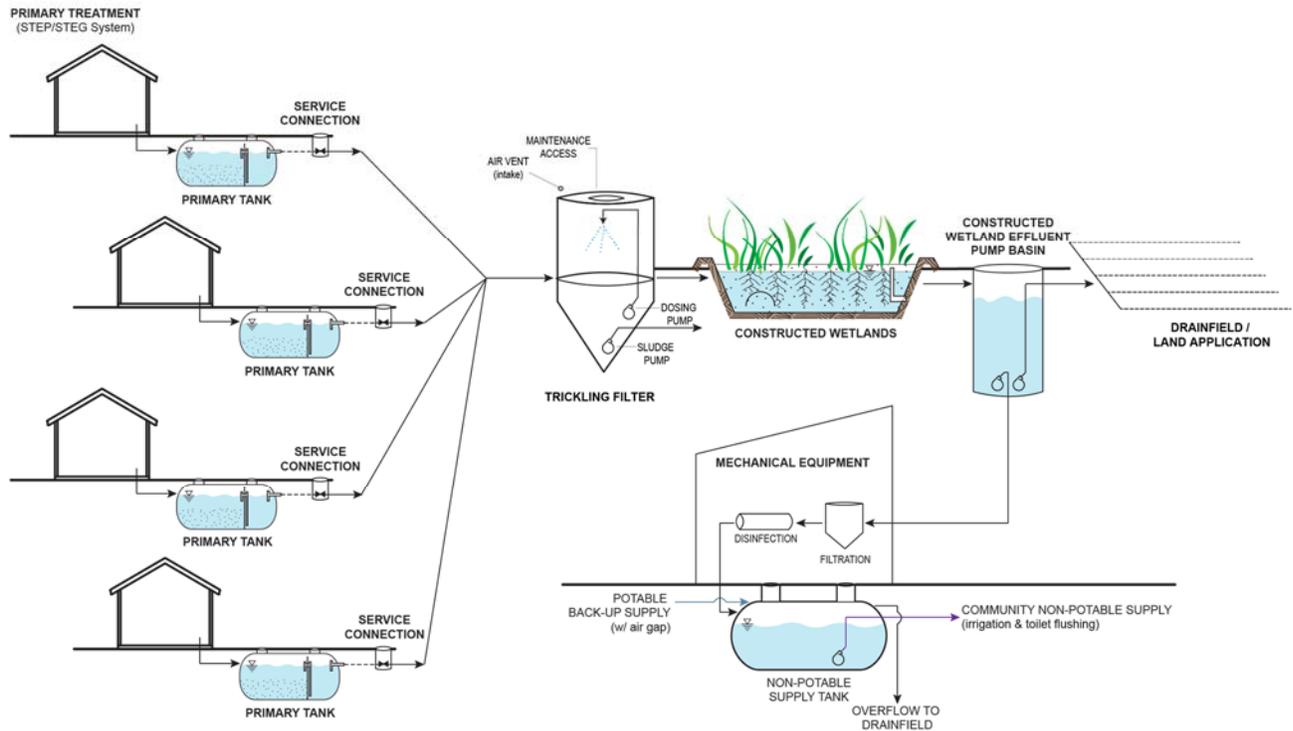
- Monitoring for total coliform organisms shall occur daily, but may be decreased if still protective of public health per the requirements in IDAPA 58.01.17.601.
- Monitoring for turbidity must occur continuously.
- Monitoring for total nitrogen and BOD<sub>5</sub> must occur weekly, with the arithmetic mean calculated monthly.
- Monitoring for pH occur daily via a grab sample, or continuously with an inline pH meter.

Based on the current development program it is expected that on average approximately 4,953 gpd of treated wastewater will be reused for toilet flushing with any remaining treated wastewater used for seasonal irrigation between May and October. The community large soil absorption system (LSAS) is designed to handle 100% of the effluent. The system will include a valve to discharge treated wastewater not used for non-potable reuse or reuse water that doesn't meet regulatory requirements to a large soil application system (LSAS).

## 2 Wastewater Infrastructure Approach

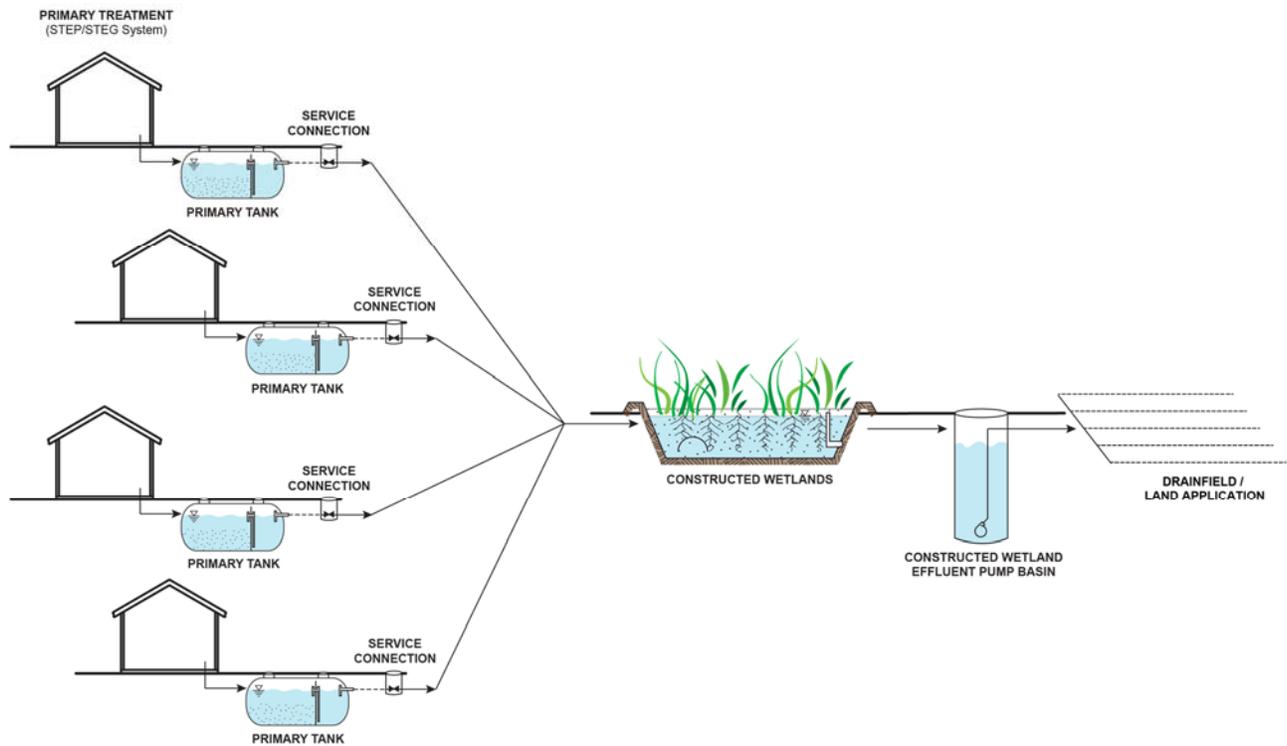
The wastewater infrastructure approach is composed of collection, treatment, and reuse/dispersal systems. The collection and primary treatment is achieved by a small diameter collection system using primary treatment tanks with effluent filters at each lot or group of lots. The secondary treatment system consists of processes designed to biologically remove BOD, TSS, and nutrients. The reuse system consists of microfiltration, disinfection, and repressurization equipment. The dispersal system drainfield consists of infiltrators or traditional gravel and perforated pipe.

The small diameter collection system routes pre-treated wastewater from the primary tanks to either the community-scale system (Phases 1 and 2) or one of the localized cluster treatment systems (Phase 3). The community-scale treatment system treats wastewater with a trickling filter and constructed wetland prior to polishing, disinfection, and reuse. The portion of treated wastewater not needed for reuse, as well as all treated wastewater from the localized cluster systems will be land applied using a large soil absorption system (LSAS) or localized drainfield, each sized to dispose of 100% of the forward flow. Figure 2 presents a schematic of the community-scale system design for Phases 1 and 2.



**Figure 2. Phase 1 & 2 treatment schematic**

Wastewater in Phase 3 will be treated with a constructed wetland prior to land application. Figure 3 is a schematic of one cluster system design for Phase 3.



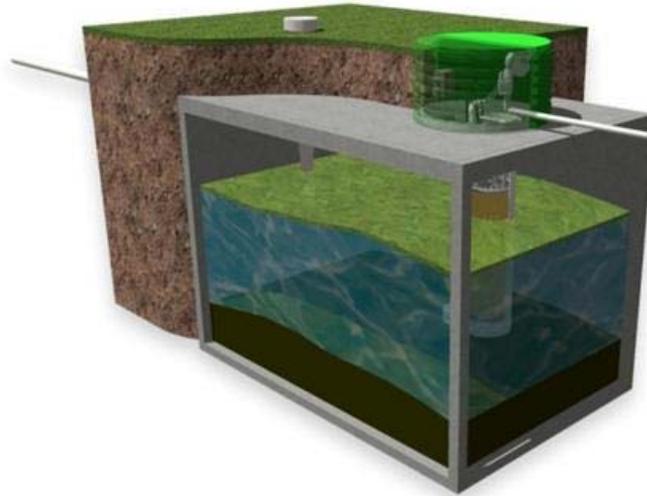
**Figure 3. Phase 3 treatment schematic**

The following sections briefly describe the wastewater infrastructure components.

## 2.1 Collection – Small Diameter Collection System

### PHASES 1, 2, 3

Each lot, or group of lots, will include a primary treatment tank (septic tank, Figure 4) to remove settable solids and equalize flow going to either the main treatment system or localized cluster system. The primary tanks have two principal functions, 1) separation of liquid and solid waste and 2) anaerobic digestion of collected sludge/solids. Solids and sludge that accumulate will be collected in the front portion of the tank and will need periodic removal by a licensed hauler. Quigley Farm will have an easement to each of the primary tanks, and manage the solids removal. Each tank passively vents back through the building plumbing roof vents.



**Figure 4. Typical septic tank**

Effluent from each septic tank will gravity flow or be pumped to the treatment system via a network of small diameter pipes (generally 2 ½" – 6"). Where septic tanks can be installed above the hydraulic grade line of the collection system, effluent will flow by gravity to the secondary treatment area. Each septic tank effluent gravity (STEG) installation will have an effluent screen, which filters solids and reduces treatment and maintenance costs. STEG screens are removable for infrequent cleaning.

When gravity flow from the septic tank to the treatment system is not possible, a removable high-head submersible pump will be installed in a screened vault in the septic tank to pump effluent to the secondary treatment location. Septic tank effluent pump (STEP) systems equalize the septic tank discharge and can operate over variable grades.

STEP & STEG collection systems do not require manholes or vacuum trucks for cleaning. "Pigging" ports and one- and two-way cleanouts are installed along with air release valves where site conditions dictate. Figure 9 is a rendering of a STEP/STEG small diameter collection system.



**Figure 5. Small diameter collection system**

## 2.2 Secondary Treatment – Trickling Filter

### PHASES 1 & 2

Wastewater from the STEP/STEG collection system gravity flows to two to three trickling filter units (Figure 10). Trickling filters are very efficient in reducing soluble BOD, converting ammonia to nitrate, and when recirculation is employed, denitrifying wastewater effluent. Each unit will be a Bioclere™ model 36/30, and will be approximately 12 feet (4 meters) in diameter and 22 feet (7 meters) tall (mostly buried). Wastewater inside each trickling filter is pressure distributed over manufactured PVC randomly packed media to remove or convert constituents. The PVC media offers a large surface area to volume ratio, and provides for movement of air in the Bioclere tower. The cylindrical waterproof tower has a built in clarifier that collects sludge sloughing from the media. There is approximately 1,059 ft<sup>3</sup> of media with a surface area of 42.5 ft<sup>2</sup>/ft<sup>3</sup> in the each waterproof tower.



**Figure 6. Typical trickling filter**

Each trickling filter contains three pumps. Two pumps form a duplex dosing system to spray wastewater over the packed plastic media. The third pump pumps accumulated sludge back into the companion anoxic tank for settling/digestion and to aid in denitrification.

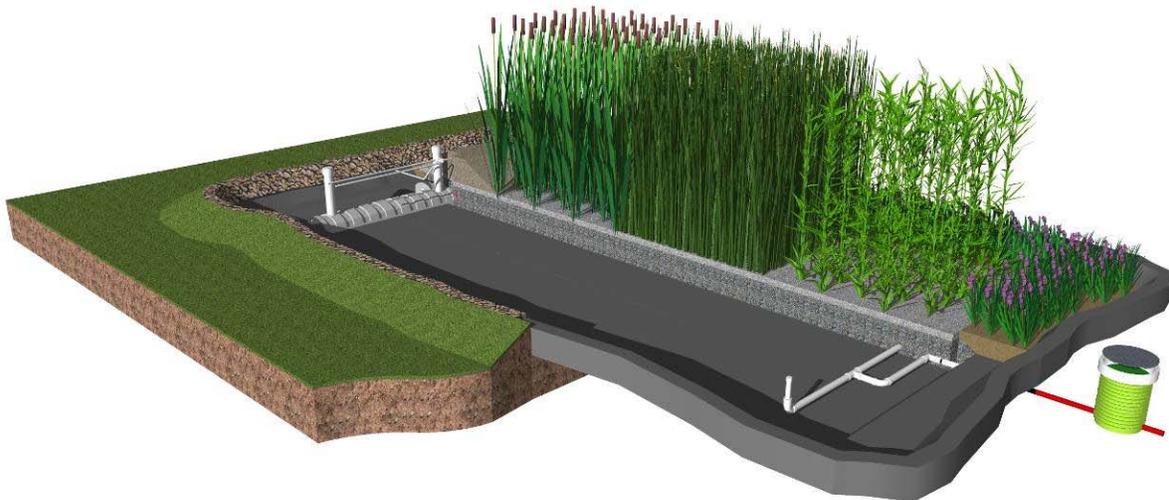
## 2.3 Secondary Treatment – Horizontal Subsurface Flow (HSSF) Constructed Wetland

### PHASES 1, 2, 3

Horizontal subsurface flow (HSSF) constructed wetlands are lined gravel filters planted with native wetland plant species (Figure 7). Influent enters through a buried infiltrator at the head of the wetland cell. Water moves horizontally through the gravel, typically 4-6" below the surface, which ensures minimal risk of odor and safety issues. As water moves through the gravel and plant roots, attached-growth bacteria breakdown and/or remove BOD, suspended solids, and nitrogen. An HDPE (high density polyethylene) or PPE (polypropylene) liner will be used to provide a water-tight cell. Each cell has a freeboard (space above the

gravel) of at least 12". HSSF wetlands can be operated successfully for 40 years or more without media replacement if used in conjunction with a pretreatment system as is designed here. Design of native planting consider at minimum climate fluctuations, disease potential, seasonal conditions, and aesthetics.

Nitrogen and BOD removal are temperature dependent processes, and cold climate systems need to be larger and deeper to assure proper treatment and reduce the potential for short-circuiting. The constructed wetland will include a layer of mulch above the gravel layer to provide insulation during the cold winter months. Site specific climate data from late winter will be used to calculate the heat flow resistance provided by annual snow fall and snow cover, to determine the thickness of the mulch layer required to prevent ice formation.



**Figure 7. Horizontal subsurface flow constructed wetland**

The community-scale wastewater treatment system constructed wetland will have a combined footprint of 32,150 square feet (Phase 1 = 11,870 square feet, Phase 2 = 20,280 square feet), and will be located adjacent to the athletic fields. This is an ideal location at the low point of the project site, and should maximize the number of properties that can gravity flow to the system. Each cluster system constructed wetland will have a footprint of 800 square feet, and will be located down gradient of each cluster to allow for gravity flow from the STEP/STEG system.

Effluent from the community-scale treatment system will gravity flow from the constructed wetland to the constructed wetland effluent pump basin, where it will either be pumped to the reuse system or to subsurface disposal. Effluent from the cluster treatment systems will gravity flow to a constructed wetland effluent pump basin to be pressure distributed to the subsurface disposal system.

## 2.4 Reuse – Filtration, Disinfection and Controls

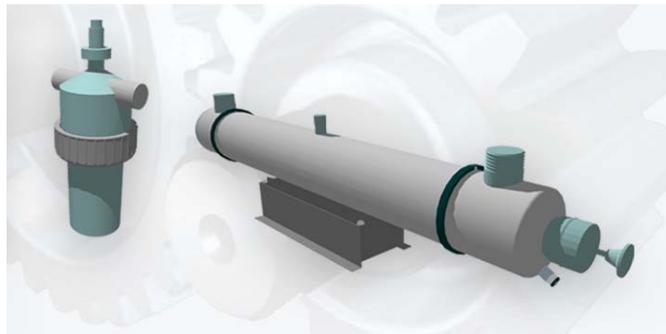
### PHASES 1 & 2

Filtration and disinfection is the final polishing step to achieve reuse water quality requirements (Figure 8). Effluent will flow through the equipment and be delivered to the non-potable supply tank. Filtration could include cloth media filters, mechanical screen filters, cartridge filters or a combination to remove suspended solids in order to manage turbidity and enable effective disinfection by ultraviolet radiation. A turbidimeter, with bubble removal system and self-cleaning lens, will sample the effluent at this point to

check for compliance with IDEQ water reuse standards. Recycled water is automatically diverted to the LSAS via a 3-way valve if clarity is not up to the desired standard.

An ultraviolet (UV) radiation unit will provide the principal disinfection of the effluent to destroy bacteria and viruses. Treated effluent is pumped through the UV disinfection system at a fixed rate. Uniform flow through the UV unit allows for a predictable level of disinfection. Intense ultraviolet light destroys fecal coliform bacteria and other pathogens as the water flows along the length of the UV light tube. The clarity of water and quantity of suspended solids in the water column affects the light intensity and disinfection rate in the unit. Typical removal rates are 99.99% (4 log).

The UV unit will be followed by an automated valve to reject water to the LSAS if the permissible turbidity level is exceeded. Water quality in the non-potable supply tank and distribution piping will be maintained by injecting chlorine into the treated effluent prior to entering the tank.



**Figure 8. Mechanical filtration and UV disinfection unit**

The wastewater mechanical building, located adjacent to the constructed wetlands, will house the filtration and disinfection equipment.

## 2.5 Reuse – Non-potable Supply Tank

### PHASES 1 & 2

A non-potable supply tank will store treated, disinfected water for reuse in the development to meet commercial toilet flushing and community spray or subsurface drip irrigation. During winter months when there is an excess of treated wastewater available, the Class A water may be used for snowmaking. Should there be a shortage of treated wastewater available for community demands, potable make-up water is supplied to the tank with appropriate backflow prevention.

## 2.6 Dispersal – Land Application to Subsurface Disposal

### PHASES 1, 2, 3

A large soil absorption system (LSAS) is the dispersal method for treated effluent from Phases 1 and 2 in excess of what is required to meet the community non-potable demand. The LSAS rules outlined in IDAPA 58.01.03 require that a minimum of two disposal systems sized to treat 100% of the daily flow be installed, for a total of 200% of the land area required to infiltrate the daily flow. Additionally, a drainfield replacement area sized to treat 100% of the daily flow must be protected for future use. The proposed

dispersal system is a gravelless drainfield with infiltration chambers installed within the trenches. Wastewater effluent will be pressure distributed to the drainfield modules via a set of duplex pumps in the constructed wetland effluent pump basin.

A localized subsurface trench drainfield is the dispersal method for all treated effluent from each cluster system in Phase 3. Each localized system will require one primary drainfield and reserved replacement area, sized to treat the daily flow of 1,005 gallons per day.

The subsurface dispersal system is sized according to soil loading rates. Based on the sieve analysis, B-2 soils are present on site and will be loaded at 0.6 gallons/sq.ft/day, using the hydraulic loading rates for secondary biological treatment systems described in the IDEQ Individual and Subsurface Sewage Disposal System Technical Guidance Manual (IDEQ TGM, Jan 2016). Table 9 is a summary of the required drainfield absorption area for the main wastewater treatment system and each localized cluster system. The use of gravelless drainfield technologies allows for a 25% reduction in installed absorption area, per Section 4.11 of the IDEQ TGM (Jan 2016). However, 100% of the required area must be protected for future use.



**Figure 9. Gravelless drainfield infiltrator.**

**Table 9 – Drainfield absorption area**

<i>SYSTEM</i>	<i>DAILY FLOW (gallons)</i>	<i>ABSORPTION AREA<sup>4</sup> – Design Soil Group B-2 (sq. ft)</i>	<i>TOTAL LAND AREA REQUIRED<sup>5</sup> – Design Soil Group B-2 (sq. ft)</i>
<b>Community-Scale Treatment System (Phases 1 &amp; 2)<sup>1,2</sup></b>	24,900	41,500	125,100
<b>Localized Cluster System (Phase 3)<sup>3</sup></b>	1,050	1,750	4,800

<sup>1</sup>Community-scale treatment system falls under Large Soil Absorption System Rules (LSAS). Maximum module size is 10,000 gallons per day. Multiple modules will be required.

<sup>2</sup>LSAS systems are required to have a minimum of two disposal systems installed, each sized to accept the daily design flow. Absorption areas shown are for one disposal system.

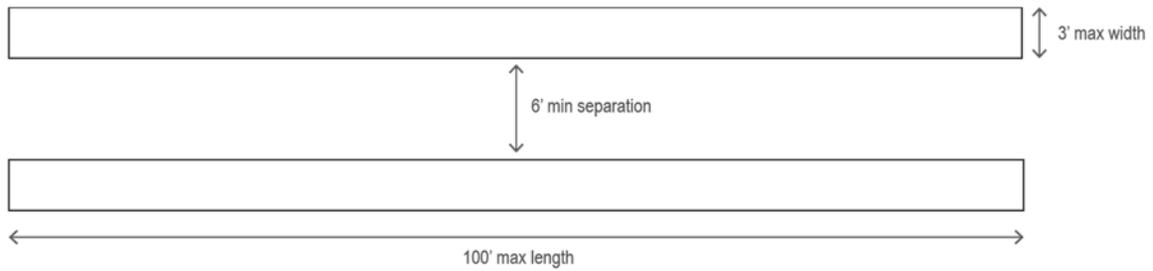
<sup>3</sup>Daily flow and absorption area is for one localized cluster system, designed to treat eight residences. Each of the three clusters will require a drainfield with the absorption areas listed.

<sup>4</sup>Absorption area only includes required trench bottom to treat daily flow.

<sup>5</sup>Total land area required includes trench bottom and minimum 6' separation between trenches.

Drainfields that exceed 1,500 ft<sup>2</sup> of total trench bottom and LSAS systems are required to have pressure distribution. This system consists of small-diameter pipe with small orifices (1/8" holes) drilled every two feet along the pipe. Pressure distribution systems maintain a uniform application of effluent across each

trench, and improve the performance and life span of the system. Trenches will be a maximum of 3 feet wide and 100 feet long. There is a minimum 6 feet separation between each trench (Figure 12).



**Figure 10. Typical drainfield trench plan.**

The drainfield requires a minimum of 12” of cover. No other surface activities (parking, driving, stormwater infiltration, etc. should occur above this area.

### 3 Phase 1 Planned Urban Development

The Quigley Farm design team is preparing Phase 1 documents to submit to Blaine County for a Planned Urban Development (PUD). Figure 11 depicts the general location of the wastewater treatment system components to meet the demands of Phase 1 only. During the first phase of construction, a wastewater treatment and reuse system will be installed to meet the full demands of the phase programming with additional land area set aside for future development.



**Figure 11. Phase 1 wastewater treatment component locations for PUD (reserve drainfield area located northeast of future school)**

## 4 Operations and Maintenance

An inspection and maintenance program should consist of routine inspection and periodic maintenance of the wastewater treatment and reuse system components. An outline of routine inspection activities are listed below and are typically performed by a facility personnel or third party operator. For this type of system you'd see a staff requirement of less than a quarter person.

COMPONENT	WEEKLY MAINTENANCE ACTIVITY
<b>PRIMARY TREATMENT TANKS</b>	<ul style="list-style-type: none"> <li>- Check to ensure lids are secure.</li> <li>- Check forward flow rate and adjust pump control timers and valves, as necessary.</li> </ul>
<b>TREATMENT UNIT</b>	<ul style="list-style-type: none"> <li>- Check to ensure lids are secure (filters).</li> <li>- Check to ensure berm and liner integrative are maintained (wetlands).</li> </ul>
<b>FILTRATION &amp; DISINFECTION</b>	<ul style="list-style-type: none"> <li>- Check and compare data logs of turbidimeter, flow meter, and reject valve to ensure proper operation.</li> <li>- Check UV lamp output and clean lamps using manual wipers.</li> <li>- Check ozone systems for proper function.</li> <li>- Check Ambient Ozone Monitor and ozone system alarms.</li> </ul>
<b>CONTROLS</b>	<ul style="list-style-type: none"> <li>- Check for system parameters at control panel.</li> <li>- Check flow rates and daily totals.</li> </ul>

COMPONENT	MONTHLY ACTIVITY - Perform these tasks <i>in addition to</i> the weekly tasks
<b>FLOW SPLITTER</b>	<ul style="list-style-type: none"> <li>- Visually inspect flow over weirs and adjust and clean to maintain equal flow to each outlet.</li> </ul>
<b>TREATMENT UNITS</b>	<ul style="list-style-type: none"> <li>- Visually inspect spray nozzle pattern, color and thickness of growth on media.</li> <li>- Check general condition of fans, pumps, wiring, splice boxes, latches, gaskets.</li> <li>- Check pump timer settings. Adjust recycle timers with to average daily influent.</li> <li>- Check float switches for proper function.</li> <li>- Visually inspect level control &amp; water, adjust and clean as necessary (wetlands).</li> </ul>
<b>FILTRATION &amp; DISINFECTION</b>	<ul style="list-style-type: none"> <li>- Inspect and clean ozone generators' compressor air filter.</li> <li>- Clean interior of control cabinet with compressed air.</li> </ul>

<b>COMPONENT</b>	<b>QUARTERLY ACTIVITY</b> - Perform these tasks <i>in addition to</i> the monthly tasks.
<b>PRIMARY TREATMENT TANKS</b>	<ul style="list-style-type: none"> <li>- Check effluent filters and clean as necessary.</li> <li>- Check pump inlet screens and clean as necessary.</li> <li>- Check float switch function or level sensor accuracy and clean lens.</li> <li>- Verify sludge and scum depth readings with core sampler.</li> </ul>
<b>TREATMENT UNIT</b>	- Remove and inspect pumps/pump basins for clogging (filters).
<b>FILTRATION &amp; DISINFECTION</b>	- Replace air filters on ozone generator.
<b>REUSE TANK</b>	Visually inspect tank and float switches.

<b>COMPONENT</b>	<b>ANNUAL ACTIVITY</b> - Perform these tasks <i>in addition to</i> quarterly tasks.
<b>FILTRATION &amp; DISINFECTION</b>	<p>Ozone System, annually or every 8,750 hours:</p> <ul style="list-style-type: none"> <li>- rebuild ozone generator air compressors.</li> <li>- replace ozone system check valves, solenoid valves and o-rings</li> </ul> <p>UV System: replace all lamps and quartz sleeves if necessary</p>
<b>CONTROLS</b>	- Calibrate influent and effluent flow meters as well as level indicators.

<b>SYSTEM COMPONENT</b>	<b>AS-NEEDED MAINTENANCE ACTIVITY</b>
<b>TREATMENT UNIT (wetlands)</b>	<ul style="list-style-type: none"> <li>- Cut and remove dead vegetation in the late fall.</li> <li>- Thin plant species that become very dense.</li> <li>- Remove dead plant material that is matted and start inhibiting new growth</li> <li>- Adjust Water Level in wetland to maintain 3" - 6" of gravel over water level.</li> <li>- Fill in areas of wetland that have low gravel spots.</li> </ul>
<b>DISINFECTION EQUIPMENT</b>	Follow manufactures maintenance instructions for maintenance and troubleshooting of all disinfection equipment.
<b>CONTROLS</b>	Inspect and resolve all alarm conditions.



# Cold Climate Constructed Wetlands



© Scott Wallace

## WHAT ARE “CONSTRUCTED WETLANDS?”

In wetland ecosystems, there are biological, chemical, and physical processes that naturally clean and filter water. Constructed wetlands are designed to mimic natural wetlands. They use wetland plants, soils, and microorganisms to clean water in a way that is often less expensive than more traditional water treatment systems. When properly designed, built, and operated, constructed wetlands provide high quality water treatment, while also adding beauty and habitat to your site.

## GREAT, BUT DO THEY WORK IN COLD CLIMATES?

Constructed wetlands work in all kinds of climates. All wastewater treatment is biologically based, and biological reactions slow down at low temperatures. The challenges presented in locations that experience cold seasons (temperatures below 0° C) are addressed through specific design considerations intended to negate any issues with performance.

## WHAT VEGETATION IS APPROPRIATE FOR COLD CLIMATE CONSTRUCTED WETLANDS?

Cold climate constructed wetlands use native wetland vegetation to help naturally treat wastewater. In the winter, when plants are dormant, their roots still provide surface area for beneficial attached growth bacteria that aid in the treatment process. Areas with greater emergent wetland vegetation, for example cattails and rushes, tend to accumulate more snow around the dormant plants, which provides helpful insulation.

## WHAT HAPPENS IF WE EXPERIENCE EXTREME COLD CONDITIONS?

If snow cover is limited or if extreme cold conditions are expected, additional challenges may be encountered:

- Ice formation could cause hydraulic short-circuiting or failure.
- Nitrogen and BOD removal are temperature dependent processes; cold climate systems need to be larger and deeper to assure proper treatment.

Strategies to address these challenges may include increasing treatment area, insulating from heat loss, deepening installation for freeze protection, and/or recirculating the water to keep it from freezing.

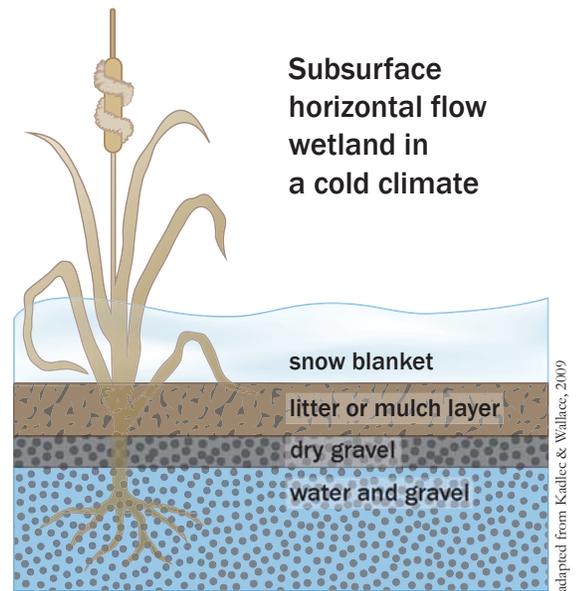


The constructed wetlands at the Omega Center for Sustainable Living in summer and winter

© Biohabitats, Inc.

## HOW ARE CONSTRUCTED WETLANDS DESIGNED FOR COLD CLIMATES?

The temperature in a wetland is controlled by ground heat from the earth and loss of heat to the environment. Heat losses can be minimized during cold temperatures by insulation (e.g., vegetation litter, snow, mulch, dry gravel) to preventing ice formation and freezing. A combination of sources are available to provide insulation to a constructed wetland, including vegetation litter, snow, mulch, and dry gravel. The thickness of the mulch layer is calculated/ modeled by determining the overall heat flow resistances of each material using the layer thickness and thermal conductivity. Site-specific climate data from late winter is used to calculate the heat flow resistance provided by annual snow fall and snow cover. February is typically the time of year when wetland water temperatures are still quite cold, but daytime solar radiation is increasing and melting snow cover, thus increasing the potential for ice to form on the wetland surface at night.



## WHERE ARE COLD CLIMATE CONSTRUCTED WETLANDS BEING USED SUCCESSFULLY?

Biohabitats has included a climate zone map showing sites that use cold climate constructed wetlands as a major wastewater treatment component. We have also included project profiles and performance data from some of our cold climate treatment systems.



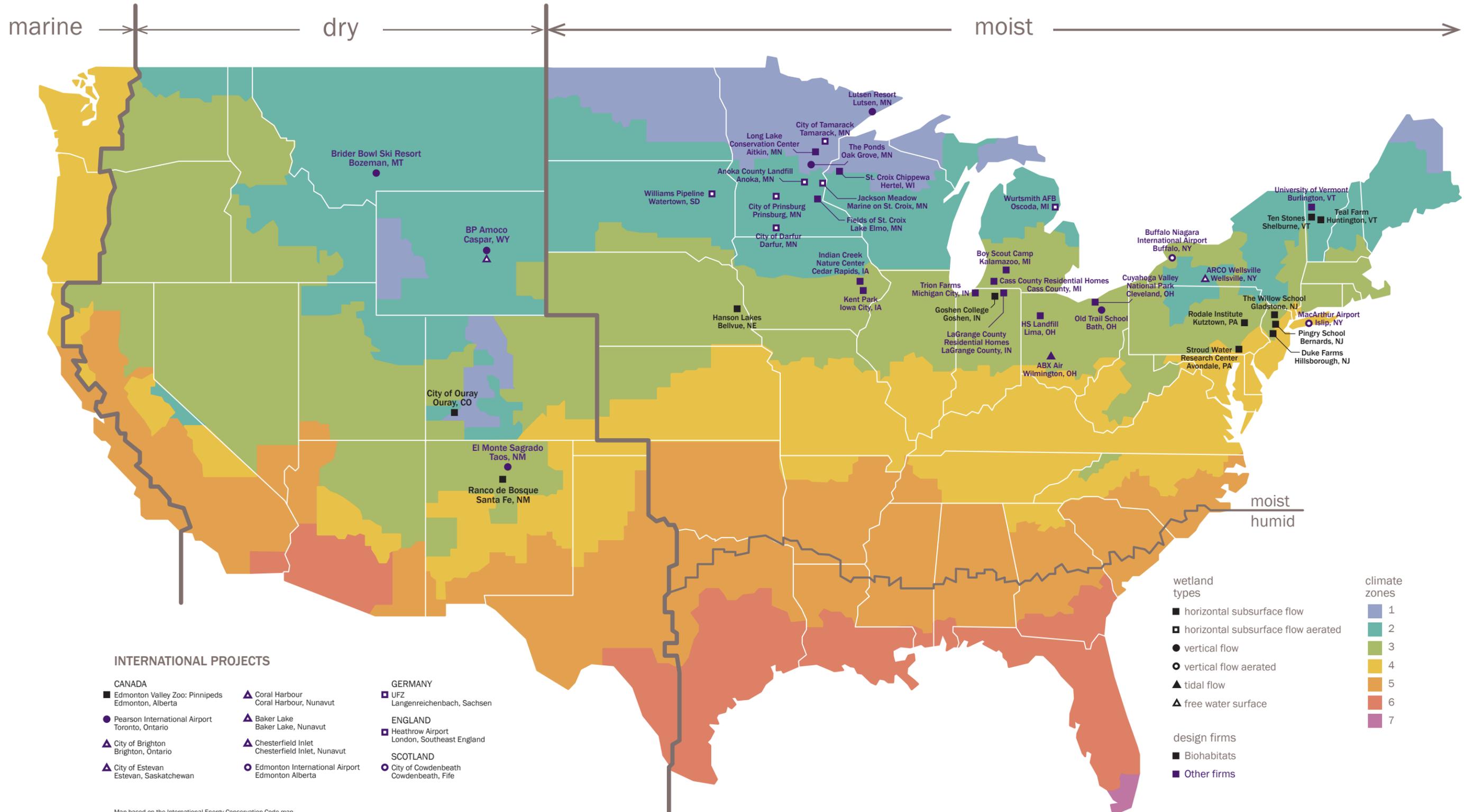
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## ADDITIONAL RESOURCES

- Mander, Ü. and Jenssen, P. (eds.) (2003) *Constructed Wetlands for Wastewater Treatment in Cold Climates*.
- Wallace, S. (2009) Constructed wetlands – how cold can you go? *Canadian Water Treatment*. pgs 14-15.
- Kadlec R. and Wallace S. (2009) *Treatment Wetlands, 2nd Edition*. Section 4.4 Cold Climates.

# COLD CLIMATE CONSTRUCTED WETLAND PROJECTS

## by climate zone



Map based on the International Energy Conservation Code map

OMEGA INSTITUTE

# Omega Center for Sustainable Living

Rhinebeck, New York



The Omega Center for Sustainable Living (OCSL) is a new wastewater treatment and environmental education facility located at the Omega Institute for Holistic Studies, a New York-based wellness center.

BNIM Architects designed a new 6,200 square foot facility on a 4.5 acre site to house the ecological wastewater treatment facility and provide the dual functions of education and inspiration. The natural wastewater treatment facility was designed together with John Todd Ecological Design (JTED) to replace the Institute's existing wastewater infrastructure, which consisted of a network of aging septic systems.

The 52,000 gallon per day wastewater treatment system is a low energy and low maintenance system with six treatment steps. Components include primary treatment, an anoxic tank, subsurface flow constructed wetlands, an indoor aerated lagoon, a recirculating sand filter and subsurface dispersal.

The effluent water quality meets all of the State of New York water quality standards for the land application/sub-surface disposal of treated effluent. This will allow the Institute to potentially reuse the water to irrigate 2.5 acres of the campus using a pressure-dosed subsurface irrigation system in the future.

The Omega Institute will use the wastewater facility as a teaching tool in their educational program designed around the ecological impact of their campus. The facility has been operational since July, 2009. The facility was awarded the AIA COTE 'Top 10 Green Projects' and LEED Platinum certification in 2010. The building was also one of the world's first (one of two) certified Living Buildings.

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*ecological restoration*  
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TEN STONES COMMUNITY

# Ten Stones Community Wastewater Treatment

Burlington, Vermont



*The first constructed wetlands wastewater treatment system for a subdivision to be built in Vermont, Ten Stones Community paved the way for many others.*

**B**iohabitats designed an innovative constructed wetland wastewater treatment system for the Ten Stones community subdivision. The developer's original design called for a more traditional leachfield with a large area set aside for replacing the eventually failing field. No city sewer infrastructure was nearby and it would have been cost prohibitive and impractical to connect to centralized sewer infrastructure. As an alternative to the leachfield or sewer connection, Biohabitats proposed a constructed wetlands to treat the 3,900 gallons per day of effluent and then dose the field.

The resulting system became the first constructed wetlands for wastewater treatment for a subdivision in Vermont,

allowing for monitoring which helped regulators understand the system.

In response to client objectives and site conditions, Biohabitats developed a low-cost solution which blends with site aesthetics, and addresses site challenges such as shallow soils over bedrock, hilly terrain, and limited space—all while meeting state water quality standards for ground discharge. The wetland enhances the site and property values with ornamental vegetation and low operation and maintenance costs for the homeowners.

## SERVICES

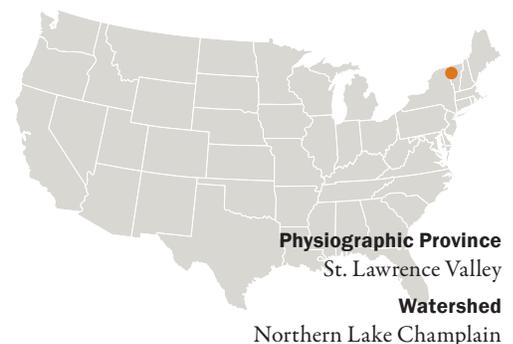
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THE WILLOW SCHOOL

# The Willow School: Campus Water Infrastructure

Gladstone, New Jersey



from top: Constructed wetland for wastewater treatment; Sunflowers growing in wastewater treatment dosing field; Newly installed 10,000 gallon cistern component of the harvested rainwater and treatment system

The Willow School, located on 34-acres of the New Jersey countryside, upholds a strong commitment to fostering academic excellence, a passion for learning, and an ethical approach to all relationships. The school views sustainability as a key element in students' relationships with the natural and with each other. Children learn to share intellectual resources with peers to sustain a community. They also learn to share, respect, and conserve nature's resources. Its buildings, landscape, curriculum, and programmatic advancements are helping to create a new generation of ecologically literate citizens.

Biohabitats helped the Willow School design sustainable water infrastructure that is in line with the school's founding philosophies. Water infrastructure for the campus includes onsite wastewater treatment and reuse, stormwater management, and rain harvesting and reuse. Current campus buildings include a LEED certified Gold Classroom Building (2004) and a LEED certified Platinum Art Barn (2005). Willow has built a Health, Wellness, and Nutrition Center that is seeking Living Building Challenge certification. Biohabitats has designed a rain water harvesting system and will reuse the water in the landscape and for toilet flushing.

**STORMWATER**  
Instead of merely reducing the impact of stormwater runoff, Biohabitats designed a system of vegetated swales to replace expensive concrete pipes and catch basins. This, in combination with reduced paving and increased natural ground covers, helped recreate the

original sponge-like character of the site. A detention pond with wetland plantings saved money and improved the environmental performance of the site significantly beyond that required by a town ordinance.

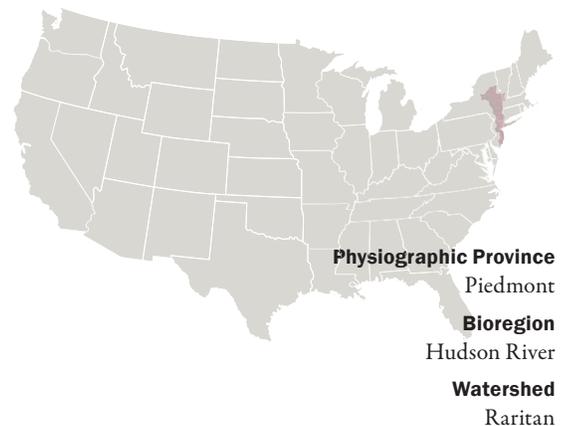
**WASTEWATER**  
In addition, all human waste generated onsite is treated with an integrated system of constructed wetlands, trickle filter, and a sand filter. The treatment cycle is so effective that treated effluent can be reused on site for irrigation and protective groundwater recharge.

**RAIN HARVESTING**  
Rain is harvested from the Art Barn Roof and the Health, Wellness, and Nutrition Center. Rain is treated in the buildings and reused as a non-potable supply for toilet flushing. Biohabitats currently has a maintenance contract to support routine and preventative maintenance on the treatment and reuse system.

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# Groundwater Discharge Compliance on a Small Municipal Budget

## *The Otis, MA Municipal Wastewater Treatment Plant*

The town of Otis Massachusetts qualified for Rural Utility Service (RUS) funding from the Department of Agriculture in 1999 to provide sewer for a portion of the community. Their engineering firm Camp Dresser & McKee (CDM) selected an Aquapoint Bioclere™ wastewater treatment system because of the stability of the fixed-film treatment process and the low installation and operating costs. Bioclere’s compact footprint also minimized land usage and the impact on the surrounding community. The treatment units are sealed, insulated and fed with warm air from the system’s control building to combat the cold 5°C water that enters from an aged collection system in the winter months. The system is owned and operated by the municipality as part of a distributed network of wastewater infrastructure.

The project was permitted under Massachusetts Department of Environmental Protection (MADEP) Ground Water Discharge Standards requiring a level of treatment that would preserve the life of the drain field and reduce nutrient loading to the groundwater. Effluent from the dual train Bioclere trickling filter process is polished through a denitrifying deep bed sand filter and UV disinfection prior to discharge.



### DESIGN CHARACTERISTICS

	INFLUENT	EFFLUENT
DESIGN FLOW (GPD)	30,000	30,000
BOD <sub>5</sub> (mg/l)	250	< 30
TSS (mg/l)	250	< 30
TKN (mg/l)	50	-
AMMONIA-N (mg/l)	40	< 2
NITRATE-N (mg/l)	-	< 5
TOTAL-N (mg/l)	-	< 10
FECAL (mpn/100ml)	-	< 200

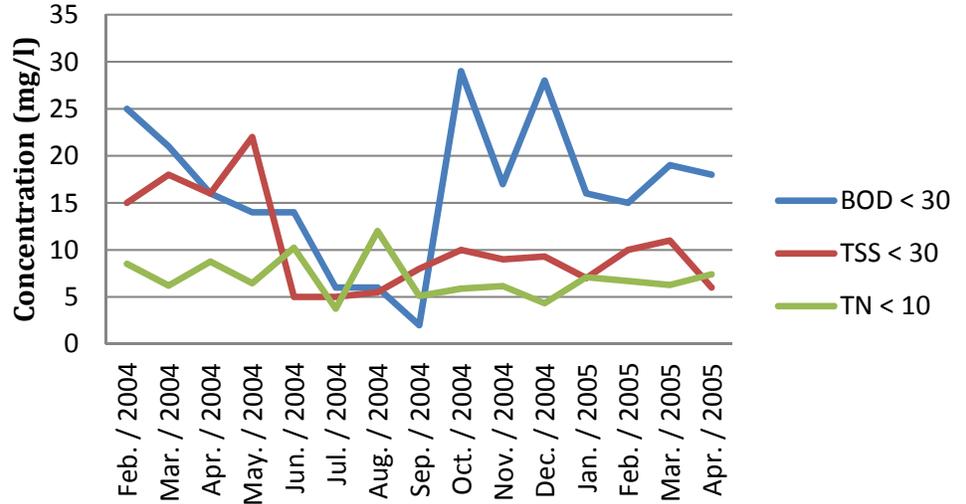


## System Layout & Performance

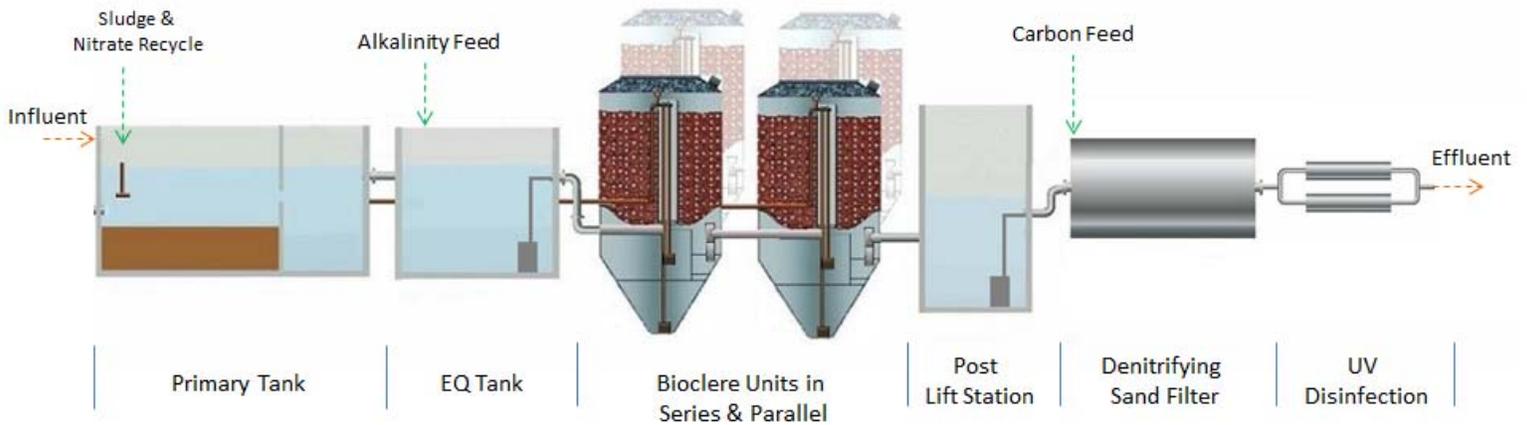
### Otis, MA – Community WWTP

DATE	BOD <sub>5</sub> (mg/l)	TSS (mg/l)	TN (mg/l)
Mar. / 2004	21	18	6.21
Apr. / 2004	16	16	8.76
May. / 2004	14	22	6.46
Jun. / 2004	14	5	10.24
Jul. / 2004	6	5	3.76
Aug. / 2004	6	5.5	12
Sep. / 2004	2	8	5.1
Oct. / 2004	29	10	5.88
Nov. / 2004	17	9	6.16
Dec. / 2004	28	9.3	4.33
Jan. / 2005	16	7	7.1
Feb. / 2005	15	10	6.7
Mar. / 2005	19	11	6.27
Apr. / 2005	18	6	7.4
<b>AVG.</b>	<b>16.4</b>	<b>10.5</b>	<b>7.0</b>

### Otis, MA Performance Data



System commissioned 1999  
 Detectable limit for BOD & TSS = 2 & 5 mg/l respectively  
 Data from MA DEP Records



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