

AGENDA
HAILEY PLANNING & ZONING COMMISSION
Monday, July 14th, 2014
Hailey City Hall
5:30 p.m.

Call to Order

Public Comment for items not on the agenda

Consent Agenda

- CA 1 Motion to approve minutes of June 9th, 2014
- CA 2 Motion to approve Findings of Fact and Conclusions of Law for a Design Review application by Friedman Memorial Airport, represented by Ruscitto/Latham/Blanton, for Design Review of proposed terminal expansion and renovation, located on tax lot 8151, FR SEC 10, 15, 22 (1610 Airport Circle), within the Airport District A Zone.
- CA 3 Motion to approve Findings of Fact and Conclusions of Law for a Design Review application by Friedman Memorial Airport, represented by Ruscitto/Latham/Blanton, for Design Review of a new airport operations, located on tax lot 8151, FR SEC 10, 15, 22 (1616 Airport Circle), within the Airport District A Zone.
- CA 4 Motion to approve Findings of Fact and Conclusions of Law for a Zone Change application by Judy Castle, represented by Gary Slette, for an amendment to the City of Hailey Zoning District Map. Proposed changes would rezone the Woodside Subdivision #9, Lots 11-15, Block 41 (3831, 3841, 3851, 3861, and 3871 Glenbrook Drive) from Business (B) to General Residential (GR) thereby allowing single family dwellings on each lot.
- CA 5 Motion to approve a request from Chip Maguire to extend the Design Review approval for construction of a new single family dwelling at 201 N. 3rd Avenue (Lot 11A, Block 47, of the Hailey Townsite) for an additional 180 days, thereby extending the expiration date from August 12, 2014 to February 8, 2015.
- CA 6 Motion to approve Findings of Fact and Conclusions of Law for a Design Review Exemption application by Wood River Valley Hangar Association, represented by Devon Jolley, for expansion of an existing hangar door and for the addition of two (2) windows, located at 2N 18E SEC 12, Airport Hangar E-18 (2010 Airport Circle), within the Airport District A Zone.
- CA 7 Motion to approve the amended Findings of Fact and Conclusions of Law for the application for a Lot Line Adjustment submitted by Capstone Development, LLC, represented by Greg Bloomfield, to reconfigure Lots 1-4 within Block 4 in the Hailey Croy Addition (206 West Croy Street), thereby creating Lot 1A, Lot 2A, Lot 3A and Lot 4A comprising 4,884 square feet each.

New Business and Public Hearings

- NB 1 Consideration of an application for a Text Amendment to the Hailey Zoning Ordinance submitted by Syringa Mountain School to amend Section 5.4 of the Hailey Zoning Ordinance 532, as amended, to allow for a reasonable number of livestock on private and public school properties within Hailey City Limits for educational purposes only, and by amending Section 2.2 of the

For further information regarding this agenda, or for special accommodations to participate in the public meeting, please contact planning@haileycityhall.org or (208) 788-9815.

Hailey Zoning Ordinance 532, as amended, to revised the definition of Urban Agriculture.

- NB 2 Consideration of a Design Review application by Sun Valley Roasters LLC, represented by Jolyon H Sawrey, for Design Review of an addition to an existing commercial building, located at Hailey Townsite, Block 29, Lots 10 (219 South Main Street), within the Business (B) and Townsite Overlay (TO) Zoning Districts.
- NB 3 Consideration of an application for Preliminary Plat proposal for Sunburst Hills, a Cottage Townhouse Development, to be located at Lots 7-9, Block 62, within Woodside Sub #15 (2541, 2621, 2641 Winterhaven Drive) comprising of 1.78 acres. Current zoning of the property is General Residential. Proposed Preliminary Plat indicates a reconfiguration of existing Lots 7-9 to Lots 1-12, Sunburst Hills Subdivision.
- NB 4 Consideration of a City of Hailey initiated text amendment to Ordinance 532, the Zoning Ordinance, by amending section 8.1.1 and 8.1.2 to clarify the General Provisions and Specific Standards for regulating fences, by amending 8.2.2 to revise the definition of Animated Sign and add a definition for Electronic Message Display, by amending 8.2.6 to prohibit Electronic Message Display Signs, by amending 8.2.7 to revise Design Guidelines and Standards for all signs, and by amending 8.2.8 with the addition of a Sign Matrix.
- NB 5 Consideration of a subdivision application within the Hailey City Area of Impact, contiguous to the Hailey City Limits for Life Springs Subdivision which proposing splitting Tax Lot 8232 into two separate lots. Public hearing will be held with the Hailey Planning and Zoning Commission to seek recommendations to Blaine County on this application
- NB 6 Update on Beekeeping Ordinance 1 year after its adoption, and discussion of urban agriculture trends within City of Hailey
- NB 7 Discussion of Accessory Dwelling Units within LR zones on lots 7,000 square feet or larger (*no documents*)

Old Business

Commission Reports and Discussion

Staff Reports and Discussion

- SR 1 Discussion of current building activity and upcoming projects
- SR 2 Discussion of the next Planning and Zoning meeting: Monday, August 11, 2014
(*no documents*)

Adjourn

Note:

City of Hailey Developmental Impact Fee Committee will meet following this Planning & Zoning Meeting.

For further information regarding this agenda, or for special accommodations to participate in the public meeting, please contact planning@haileycityhall.org or (208) 788-9815.

**MINUTES OF THE
HAILEY PLANNING & ZONING COMMISSION
Monday, June 9th, 2014
Hailey City Hall
5:30 p.m.**

Present: Jay Cone, Janet Fugate, Owen Scanlon, Dan Smith, Regina Korby

Absent: None

Staff: Micah Austin, Kristine Hilt

Call to Order

5:30:08 PM Chair Cone called the meeting to order.

Public Comment for items not on the agenda

None.

Consent Agenda

- CA 1 *Motion to approve minutes of May 12th and May 27th, 2014*
- CA 2 *Motion to approve Findings of Fact and Conclusions of Law for a Design Review application by Capstone Development LLC, represented by Greg Bloomfield, for Design Review of a new single family residence to be located within the Hailey Townsite, Lot 1A of Block 4 (206 West Croy), within the General Residential (GR) and Townsite Overlay (TO) Zoning Districts.*
- CA 3 *Motion to approve Findings of Fact and Conclusions of Law for a Design Review application by Capstone Development LLC, represented by Greg Bloomfield, for Design Review of a new single family residence to be located within the Hailey Townsite, Lot 3A of Block 4 (210 West Croy), within the General Residential (GR) and Townsite Overlay (TO) Zoning Districts.*
- CA 4 *Motion to approve Findings of Fact and Conclusions of Law for a Design Review exemption by Chateau Discount Outlet, represented by Ken Sangha, for exterior painting of a structure located within the Limited Business Zone, Friedman Park Subdivision, Block 2, Lot 11A (960 South Main Street).*

5:31:07 PM Janet Fugate asked to pull CA 4. Janet Fugate motioned to approve the consent agenda items 1-3. Dan Smith seconded and the motion carried. Micah Austin notified the Commissioners that the previously proposed color that was not approved by the Commission. He also notified the Commissioners that the applicant was present and was proposing a new color. Ken Sangha introduced an image to the Commissioners that included his proposed sample held up to the current color of the City of Hailey Welcome Center. Owen Scanlon asked to see the color that the applicant had in hand. Discussion of the Commission included the current proposed color and the preferred color of the building which was to be painted. Regina Korby noted that such a bright color at the entrance of town may not be desirable. Discussion continued to include context, location, surrounding paint colors and more desired color shades. **5:40:21 PM Dan Smith inquired of the current condition of the paint on the building and if the new color choice would be used as a marketing tool as well. Ken Sangha notified the Commission that the building was in need of new paint and discussed trim, landscape and proposed signage on the property. 5:43:07 PM Owen Scanlon motioned to approve the Findings of Fact for Chateau Drug. Regina Korby seconded and the motioned passed 3 to 2 votes.**

New Business and Public Hearings

- NB 1 *Consideration of a Design Review application by John Johnston, represented by Carter Ramsay, for*

Design Review of an addition to an existing single family residence, located at Hailey Townsite, Block 72, Lots 16, 17, FR Lot 18 TL 7421 (648 N. 3rd Avenue), within the Limited Residential-1 (LR-1) and Townsite Overlay (TO) Zoning Districts. (Applicant has withdrawn application, no documents)

[5:44:06 PM](#) Micah Austin notified the Commissioners that the applicant had withdrawn the application and that no documents were present.

NB 2 Consideration of a Zone Change application by Judy Castle, represented by Gary Slette, for an amendment to the City of Hailey Zoning District Map. Proposed changes would rezone the Woodside Subdivision #9, Lots 11-15, Block 41 (3831, 3841, 3851, 3861, and 3871 Glenbrook Drive) from Business (B) to General Residential (GR) thereby allowing single family dwellings on each lot.

[5:44:48 PM](#) Gary Slette, applicant representative, stood to introduce the application to the Commissioners.

[5:47:11 PM](#) Micah Austin notified the Commissioners that the proposed zoning was in compliance with the current Comprehensive Plan for the City of Hailey. He also noted that parcels were originally platted for residential lots and have sat vacant since original platting. An application for residential construction prompted the investigation of the lots and Micah Austin then discovered that the current zoning on the lots was Business. Micah then read the previously submitted public comments. [5:53:53 PM](#) Micah Austin then added that he suggested a eight (8) foot buffer zone with landscaping to be included in the approval as a condition for development and in place prior of the issuance of building permits. This buffer zone was suggested to be located between the commercial lot and Lot 15. [5:56:14 PM](#) Micah Austin then summarized the staff report for the Commissioners. Discussion between the Commissioners and Micah Austin included surrounding zoning and boundaries. [6:04:14 PM](#) Chair Cone opened the meeting for public comment. Lisa McGraw stood in opposition to the zone change and noted numbers from the current listings for single family dwellings and vacant lots. She noted that. Dennis McGraw stood in opposition of the proposed zone change. He added information about a drainage problem in the immediate neighborhood and how this negatively affects his property and those around him. [6:07:41 PM](#) Darren McGraw, neighboring resident, stood in opposition and mentioned a problem on the property existed with noxious weeds. [6:09:20 PM](#) Gary Slette stood and noted that the problems mentioned by the surrounding property owners actually supported a zone change and residential development. He added that the current Comprehensive Plan supported the zone change. [6:12:07 PM](#) Chair Cone closed the public comment. The Commission discussed appropriateness of residential zoning versus business zoning, surrounding lots and uses, value of both scenarios and zones, neighboring property owners and current inventory of business zoning and lots within Hailey. Owen Scanlon and Regina Korby added that they would prefer to keep the five (5) lots as Business Zone. [6:17:40 PM](#) Dan Smith inquired about the initial Business Zone change on the lots and Micah Austin added that he thought it may have been near the year 2000 but was not completely certain. [6:18:16 PM](#) **Owen Scanlon motioned to recommend denial of the application to the City Council to amend the zone district map for the City of Hailey to change the zoning of Lots 11-15, Block 41, Woodside Subdivision No. 9 from Business (B) to General Residential (GR), finding that the project is not compatible with existing conditions in the neighborhood. Regina Korby seconded and the motion passed unanimously.** Micah Austin then added that the hearing would be presented to the City Council next and that the public were encouraged to attend.

NB 3 Consideration of a Design Review application by Friedman Memorial Airport, represented by Ruscitto/Latham/Blanton, for Design Review of proposed terminal expansion and renovation, located on tax lot 8151, FR SEC 10, 15, 22 (1610 Airport Circle), within the Airport District A Zone.

[6:24:58 PM](#) Chair Cone asked the applicant to introduce the common themes of the terminal building and the airport operations building first then go into detail for each building after that. Nick Latham, Ruscitto/Latham/Blanton, stood and introduced his team to the Commissioners. Dave Mitchell stood and

introduced the project to the Commissioners and included details about the need for improved airport standards that prompted the renovations and improvements to the Friedman Memorial Airport. He also included details about FAA standards and regulations, the plan for compliance with FAA standards, a technical analysis that was conducted, a revised and accepted plan and the proposal for moving forward. [6:33:14 PM](#) Dave Mitchell continued to introduce the project and included the proposed timeline, other Friedman Memorial projects currently open, and upcoming projects in calendar year 2015. [6:35:08 PM](#) Owen Scanlon about why the administration building needed to be relocated. David Mitchell answered that the move of the bypass taxiway affected the administration building and therefore it needed to be relocated. Dave Mitchell added that the projects were all necessary in order to meet the requirements of the FAA safety standards. He continued to introduce the project to the Commissioners and included details of the survey plan, terminal parking plan, airport operations building parking, utility plan and relocation of existing utilities, drainage plan and changes, and the staging and contractor parking plan. [6:45:43 PM](#) Micah Austin noted that the applicant had proposed more parking spaces than what was required. Chair Cone inquired about the future need of parking and the existing parking on site. Dave Mitchell noted that the parking was calculated based on the current and predicted knowledge of flights and the need for parking based on those flight numbers and times. The Commission continued to discuss the current and proposed parking spaces and code requirements for parking in the Airport Zone. Micah Austin summarized the parking requirement criteria and summarized how the applicant proposed to meet certain criteria for compliance. [6:54:42 PM](#) Kurt Eggers, landscape architect, stood to introduce the landscaping plan for the project to the Commissioners. He included details about orientation, existing landscaping on site, and proposed landscaping with details of species, size and location. Main proposed locations include areas on the upper parking lot, stairways to lower parking, and the entrance and exit roadways to and from the airport main parking and terminal area. Kurt then added details about the landscaping for the airport operations building and the screening around the parking lot for that building. [7:01:31 PM](#) Owen Scanlon inquired about the irrigation. Kurt answered that an underground irrigation system would be implemented for all landscaping. Dan Smith inquired about deciduous trees and the appropriateness of those in conjunction with FAA standards. Kurt Eggers answered that a smaller species would be used for safety and only on the opposite far side of the operations building away from the air traffic. [7:03:47 PM](#) Michael Bulls, Ruscitto/Latham/Blanton, stood to introduce the lighting plan for the project. He added details about existing and proposed locations of the lights, illuminations, specific fixture specifications, heights of lights, daily output levels, and automated lighting controls. Michael included details about the terminal lighting plan as well. Owen Scanlon inquired about the lighting over the stairs in the parking lot. Michael notified the Commissioners that existing lighting was sufficient in that area. Michael continued to include details about other site specific lights. [7:13:50 PM](#) Michael Bulls continued to brief the Commission on lighting and FAA requirements and guidelines. [7:19:37 PM](#) Matt Dubbe, Mead & Hunt, stood and introduced the terminal expansion to the Commissioners and included details about the proposed addition, footprint of existing building, design influence and criteria, road reconfiguration, roof pitch and skyline orientation, and color renderings. Matt continued to detail the materials and the location of each based on the colored renderings. [7:29:02 PM](#) Chair Cone inquired further about the location of the proposed materials. Matt Dubbe went over the locations of each again for more clarification. Chair Cone inquired about the glass choice. Matt noted the window specifications and the performance of the glass itself. Chair Cone also inquired about energy efficiency and the FAA standards for terminal energy efficiency requirements. Matt Dubbe noted that the energy efficiency requirements had evolved to be more restrictive and more in conjunction with grant incentives and opportunities. [7:36:03 PM](#) Discussion continued to include glazing, existing and replaceable glazing, tinting of glass, and color choices for the entire building envelope. [7:37:08 PM](#) Micah Austin summarized his staff report and summarized the various departments' comments in relationship to the project. He added comments about construction site safety, parking requirements, signage, bike parking area, and the lack of bulk requirements in the current zoning. Discussion continued to include public art requirements and vehicular circulation. [7:47:29 PM](#) Chair Cone opened the meeting for public comment. None was given and he closed the public comment period. [7:49:31 PM](#) **Regina Korby motioned to approve the design review application submitted by the Friedman Memorial Airport Authority for approval of a terminal expansion and renovation, finding that the project does not jeopardize the health, safety or welfare of the public and the project conforms to the applicable specifications outlined in the Design Review Guidelines, applicable**

requirements of the Zoning Ordinance, Title 18, and City Standards, provided conditions (a) through (h) are met. Janet Fugate seconded and the motion passed unanimously.

NB 4 Consideration of a Design Review application by Friedman Memorial Airport, represented by Ruscitto/Latham/Blanton, for Design Review of a new airport operations, located on tax lot 8151, FR SEC 10, 15, 22 (1616 Airport Circle), within the Airport District A Zone.

[7:50:40 PM](#) Micahel Bulls, Ruscitto/Latham/Blanton, stood to introduce the airport operations building to the Commissioners and included details about the design goals, solar orientation, access to the runway, snow removal equipment storage, and public access to the parking area from the operations building. Michael Bulls showcased the proposed building with color renderings and included details about aesthetic appeal, building height and dimensions, sunshade devices, exterior materials and locations of each. He added information about the energy efficiency of the building, landscaping specifics, and interior specifics and layout. [8:01:44 PM](#) Owen Scanlon inquired about the water retention and run off. Michael added that a perimeter drain would be in place along with dry wells on site. He then added details about the necessary fill in order to secure the building foundation and service road. [8:07:04 PM](#) Micah Austin summarized the staff report and included details about compliance in regards to parking requirements, bike racks, solar gain, landscaping, snow storage, energy efficiency, glazing, and screening. [8:13:32 PM](#) Owen Scanlon inquired about the choice of the species of pine. Kurt Eggers notified him that the Pine was most appropriate. [8:14:29 PM](#) Chair Cone opened the meeting for public comment. None was given and he then closed the public comment period. [8:14:36 PM](#) **Janet Fugate motioned to approve the design review application submitted by the Friedman Memorial Airport Authority for approval of a new Airport Operations Building, finding that the project does not jeopardize the health, safety or welfare of the public and the project conforms to the applicable specifications outlined in the Design Review Guidelines, applicable requirements of the Zoning Ordinance, Title 18, and City Standards, provided conditions (a) through (h) are met. Owen Scanlon seconded and the motion carried unanimously.**

NB 5 Consideration of a City of Hailey initiated text amendment to Ordinance 532, the Zoning Ordinance, by amending section 8.1.1 and 8.1.2 to clarify the General Provisions and Specific Standards for regulating fences, by amending 8.2.2 to revise the definition of Animated Sign and add a definition for Electronic Message Display, by amending 8.2.6 to prohibit Electronic Message Display Signs, by amending 8.2.7 to revise Design Guidelines and Standards for all signs, and by amending 8.2.8 with the addition of a Sign Matrix.

[8:22:32 PM](#) Micah Austin introduced the proposed ordinance to the Commissioners and included the history and purpose for the changes. He added specifics about each of the changes for the Commissioners. Discussion included visibility of fence heights at intersections for safety standards and the need for a proportional standard for all public and private streets. [8:33:36 PM](#) Micah Austin further summarized proposed changes and included details about the City sign code. Details included information about electronic message display signs, animated signs and prohibited signs. Discussion included definitions of window signs, wall signs, and signs generally. Details of sign area calculation specifics were added to the ordinance for clarification. [8:46:12 PM](#) Micah Austin and Ned Williamson then summarized the changes and added that a sign matrix was added for clarification of each sign allowed with the City of Hailey. [8:47:57 PM](#) Chair Cone opened the meeting for public comment. Tony Evans, Idaho Mountain Express, stood and inquired about prohibition of barrier fences. Micah Austin clarified. [8:49:33 PM](#) Chair Cone closed the public comment period. [8:49:48 PM](#) **Owen Scanlon Motion to continue the public hearing upon the proposed amendment to Section 8.2 July 14th. Janet Fugate seconded and the motion carried unanimously.**

Old Business

Commission Reports and Discussion

Staff Reports and Discussion

SR 1 *Discussion of current building activity and upcoming projects*

[8:51:43 PM](#) Building activity was summarized for the Commissioners and it was noted that building activity has decreased.

SR 2 *Discussion of the next Planning and Zoning meeting: Monday, July 14, 2014
(no documents)*

[8:52:15 PM](#) Micah Austin added information about the Development Impact Committee and he noted that most of the committee has resigned. Micah Austin added that the Planning & Zoning Commissioners are encouraged to accept the responsibility. Discussion continued to include the requirements for the Committee and Idaho Statute. Chair Cone inquired in the Developmental Impact Advisory Committee annual meeting could be held on the next Planning & Zoning Meeting on July 14th, 2014. [8:55:21 PM](#) Micah added that there was a training that was available for the Commissioners and City staff on the upcoming Monday, June 16th.

Adjourn

[8:56:49 PM](#) Regina Korby motioned to adjourn. Owen Scanlon seconded and the motion carried unanimously.

FINDINGS OF FACT, CONCLUSIONS OF LAW AND DECISION

On June 9, 2014 the Hailey Planning & Zoning Commission considered an application for Design Review submitted by Friedman Memorial Airport Authority for alterations and additions to the current Terminal building including a major expansion, renovation, and site plan alterations to the Friedman Memorial Airport located at the Friedman Memorial Airport, FR Section 10, 15, and 22 (Tax Lot 8151), within the Airport District (A). The Commission, having been presented with all information and testimony in favor and in opposition to the proposal, hereby makes the following Findings of Fact, Conclusions of Law and Decision.

FINDINGS OF FACT

Applicant: Friedman Memorial Airport Authority

Request: Design Review of a remodel and addition to the passenger terminal at the Friedman Memorial Airport

Location: Friedman Memorial Airport, FR Section 10, 15, and 22 (Tax Lot 8151)

Zoning: Airport (A)

Notice:

Notice of the public hearing on June 9, 2014 was published in the Idaho Mountain Express on Wednesday, May 21, 2014. Notice was mailed to all adjoining property owners on Wednesday, May 21, 2014.

Application:

The Friedman Memorial Airport Authority (FMAA) requests design review approval of a major renovations and additions to the airport terminal to better accommodate passengers, staff, security, and operations of all terminal activities. The new additions to the terminal comprise a total of 14,940 square feet, expanding the total terminal square footage to 32,252. The total footprint of the proposed building, which includes the current terminal, will be 31,454 square feet. The terminal has been designed to accommodate the expanding needs at the airport. The height of the building is 26' 4 3/8" at its maximum with 18' roof heights over the majority of the structure where flat roofs exist. All heights for the terminal building meet FAA compliance and the terminal has been designed according to FAA specifications. All mechanical equipment as proposed is screened and placed out of public view. The design incorporates awnings, varied wall materials, contrasting/complimentary colors, landscaping, and a broken roof line all contribute to a well-designed structure to serve a practical and necessary function.

Procedural History

On June 9, 2014, the Hailey Planning and Zoning Commission held a public hearing to consider the request from the Friedman Memorial Airport Authority for design review of major additions and modification to the existing terminal building located at the airport. After holding a public hearing and deliberation among the Commissioners, the Planning and Zoning Commission voted unanimously to approve the request without conditions.

General Requirements for all Design Review Applications				
Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.5 (B)	Complete Application
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Department and Boards/Commissions Comments	<p>Life/Safety:</p> <ul style="list-style-type: none"> - Police Department <ol style="list-style-type: none"> 1. No concerns with presented design review request. - Fire Department <ol style="list-style-type: none"> 1. With proposed plan, a dry suppression system is required. 2. Lower access drive to the lower parking lot should be a minimum of 20' wide.
				<p>Water</p> <ul style="list-style-type: none"> - The City of Water Department requires an EZ Valve installed North of our pressure reducing station (PRV). This would allow the Airport to relocate our water main at the airport terminal without having to bypass the PRV, and increasing water pressures in south Woodside over 100 psi. - The conditions the Water Department are requiring, are: <ol style="list-style-type: none"> 1. They must dig up the water main at a location convenient for the Airport and the water dept. to confirm Water Main size and to install EZ Valve 2. Trench must be adequate for EZ valve installation 3. Must hire EZ valve contractor to install the valve (i.e. Ferguson Waterworks) 4. Once valve is installed a valve box must be supplied and installed by the airport following City standards. Also all bedding and back fill must follow City standards and is the responsibility of the airport. The water department must inspect bedding and valve box placement over the valve to ensure we can turn valve on and off.
				<p>Sewer</p> <ul style="list-style-type: none"> - Manholes should be offset from the center of the access drives to match City Standards - The private lift station and accompanying private pressure line should be abandoned. No concerns with closing the lift station. - A 20' PUE may be necessary to obtain by the developer if they

				<p>decide to hook into the city sewer main through adjacent private property.</p> <ul style="list-style-type: none"> - The service line shown on the plans running west from the new terminal building will need to gravity feed across a parking lot to a gravity sewer line. Currently, it is shown connecting into the pressure sewer line, which will be abandoned with the lift station. <p>Building:</p> <ul style="list-style-type: none"> - More detail is needed to ensure that proper safety precautions are taken during construction such as adequate separation between the public and the construction areas. - Plans are not adequate to show that safety issues have been addressed. This will not hold up Design Review, but the applicant is advised that more precautions are necessary to obtain a building permit - <p>Streets:</p> <ul style="list-style-type: none"> - No Concerns. No city streets are included or altered in this design review request. <p><u>Boards and Commissions</u></p> <p>Tree Committee:</p> <ul style="list-style-type: none"> - N/A <p>Parks and Lands Board</p> <ul style="list-style-type: none"> - N/A <p>Hailey Arts and Historic Preservation Commission:</p> <ul style="list-style-type: none"> - N/A
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	8.2 Signs	<p>8.2 Signs: The applicant is hereby advised that a sign permit is required for any signage exceeding four square feet in sign area. Approval of signage areas or signage plan in Design Review does not constitute approval of a sign permit.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Signage for coordination of airport activities are not subject to the regulations of Section 8.2 (i.e. "Arrivals", "Departures," etc.)</i> - <i>Any commercial signage will be subject to the sign regulations in Section 8.2.</i> - <i>No commercial signage is proposed with this application.</i> - <i>The applicant has indicated that any additional commercial signage will be submitted to the City for approval under a separate signage permit application.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	9.4 On-site Parking Req.	<p>See Section 9.4 for applicable code.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Section 9.4 of the Zoning Ordinance does not call out airport terminals or airport operations for specific parking requirements. In</i>

				<p><i>absence of any specific regulations, the code requires 1 space for every 1,000 square feet of building space. Under this standard the new terminal proposed at 32,252 square feet will require 32 spaces. The applicant has indicated that they are providing a total of 338 spaces. This is in violation of Section 9.4.6, however the applicant is proposing to meet this requirement according to the criteria listed in 9.4.6.1.</i></p> <ul style="list-style-type: none"> - <i>See below for an analysis of this section of code.</i> - <i>With no standardized parking requirements for airport terminals, the applicant has been advised to present a parking plan to the Planning and Zoning Commission that will show that all parking needs for the terminal and airport activities have been satisfied.</i> - <i>The applicant has submitted the following analysis for approval:</i> <p><u>Applicant's Remarks:</u> Code does not address airport usage. A number of spaces will be eliminated by the terminal addition, and an analysis of spaces required was completed. The usage of the parking lot is unique, for the following reasons:</p> <ul style="list-style-type: none"> o Paid parking, in contrast with most public parking in the City of Hailey. o Approximately 110 of the existing parking spaces are reserved for rental cars, employees (taxis, airport staff, airlines, TSA, etc.). o Parking demand varies widely, depending on the time of year. Peak times are typically in July, August, February and March, with other peaks on holiday travel days. o Most (approximately 79%) of the vehicles that enter the parking lot leave within 30 minutes. It is assumed that most of these vehicles only pick up or drop off a passenger, not parking at all. <p>For purposes of this analysis, historical data was obtained from airport staff and evaluated. This data reveals that the peak overnight parking demand in the past three years was 152 cars, which occurred on a March 2014 weekend and was unusually high. Data from parking lot tickets was also evaluated, but it was determined that 152 cars is a conservative number, appropriate for this analysis.</p> <p>The parking lot was designed with the new terminal footprint. The orientation of parking spaces will be changed from angled to perpendicular, using 9'X18' spaces, as permitted in City code. In addition to parking requirements, accommodation for taxis, buses, rental car returns and other unique airport functions were also considered in the parking lot layout.</p> <p>The current parking lots (upper and lower) include 338 total spaces, including reserved space and ADA spaces. Approximately 30 spaces</p>
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			<p>are lost in the winter for snow storage. It should be noted that these total spaces include airport administration parking, which will be accommodated at the Airport Operations Building after the projects are completed.</p> <p>The proposed parking lot includes 326 spaces in the summer and 315 in the winter. (An additional 17 spaces will be provided at the Airport Operations Building for airport staff and visitors.) This total parking was compared with the peak demand of 110 reserved spots plus 152 peak overnight parked cars, with adequate space to accommodate the anticipated demand.</p> <p>The Airport recognizes that demand for airport parking may increase over time. Space to construct additional parking is available to the west of and at the north end of the lower lot, and such construction will be completed when necessary.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>9.4.6 <i>(Parking in excess of 200% of required spaces)</i></p> <p>No use shall provide on - site parking for more than two hundred percent (200%) of the number of spaces required by this Ordinance unless permitted by specific action of the Commission. Applications for parking in excess of that normally permitted will be heard by the Commission as part of other applications, or, where no other application is pertinent, under the notice and hearing procedures set forth for Design Review.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - <i>The requirement is 32 spaces, based on the square footage of the building and the default parking requirement of 1 space for 1000 square feet of gross building area (9.4.A.4)</i> - <i>The applicant proposes 326 spaces, which is ten times the required amount. The code restricts any parking in excess of 200% of the require number but allows for the Commission to permit such when the applicant can meet certain criteria. If the applicant can meet four of six specific criteria, the parking may be allowed in excess of 200%.</i> - <i>See below for how the applicant proposes to meet this standard.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>9.4.6 1 <i>Criteria for allowing parking in excess of 200%</i></p> <p>The Commission shall consider the following criteria when evaluating any application for parking in excess of that normally permitted. Applicants are required to satisfy at least 4 of the following criteria:</p> <ol style="list-style-type: none"> 1. The excess parking area will be commonly used for public interests such as park and ride or car pool lots. The property owner will be permitted to reserve the use of the parking area 12 days in any calendar year. 2. The excess parking area provided would relieve or help to relieve a substantial shortage of parking within an 800 foot radius. 3. The excess parking area will not be adjacent to a public right - of way, and will be separated from the right-of-way by a building. 4. The excess parking area is part of an overall development scheme which compensates for insufficient parking in other portions of the same development. 5. The excess parking area will be used as an alternate facility, such as a basketball court or skateboard park, when not in use as an overflow parking area. The property owner will be permitted to reserve the use of the parking area 12 days in any calendar

				<p>year. 6. The excess parking area will surfaced with an alternative and attractive material.</p>
			<p><i>Findings</i></p>	<p>- <i>The applicant proposes to meet criteria numbers 1, 2, 3, and 4. The applicant has provided the following for explanation:</i></p> <p>Terminal Parking</p> <p>Per City of Hailey Ordinance 9.4.A.3, development projects with a specific use not listed within the zoning ordinance (Article 9) are required to provide (1) one parking space per 1,000 SQ. FT. of building area.</p> <p>This requirement, when applied to the proposed Terminal Expansion, results in a required parking count of 31 spaces. The proposed overall square footage of the Terminal Expansion is 31,555 SQ. FT. City of Hailey Ordinance 9.4.6 states that no project may provide more than 200% of the parking spaces required, which in this circumstance would result in a maximum allowable parking count of 61 spaces.</p> <p>In order to provide an efficient and fully functional airport, this project requires that additional parking above and beyond the allowable 61 spaces be provided on-site. The design for the new airport parking area proposes to provide a total of 326 parking spaces during summer months, and 315 parking spaces during winter conditions. This assessment of parking needs has been based on historic data obtained from parking ticket counts that demonstrate the typical peak demand experienced by the airport parking area. The observed historic peak over-night parking demand is 152 spaces, with an additional 110 spaces held in reserve to accommodate rental car services and airport employee parking requirements.</p> <p>This statement is intended to clarify for the Planning & Zoning Commission, the need for additional parking in excess of the 61 allowable spaces permitted by the City of Hailey Parking Ordinance. In response, the applicant will show that it meets at least 4 of the 6 criteria required within subsection 9.4.6.1 of the City of Hailey Parking Ordinance. Criteria Number</p> <p>1) The airport parking area is a publicly available parking area, which operates 24 hours a day, throughout the entire year. This parking area provides vehicle storage for passengers traveling to and from the Wood River Valley, in essence operating as a park and ride lot of the traveling public. Users of the Airport parking lot are not prohibited to flying passengers and any who wish to use this facility may do so at their own expense.</p>

				<p>2) The airport parking area provides additional over-flow parking, when needed, for many other facilities that operate within an 800 FT. radius of the airport site. This includes the St. Luke’s medical center, adjacent private airplane storage hangars, and the multitude of light industrial developments within the airport west development area. The excess amount of available parking within the proposed airport parking area has the potential to enhance effective circulation throughout the area and on the adjacent public streets.</p> <p>3) The airport site, as shown in the submitted design review submittal drawings, is separated from adjacent public streets and City of Hailey right of way areas by a distance of approximately 450 FT. or more. In addition to this separation, the airport site is separated from the public right of way by the adjoining airport west development area; including the Blaine Country Correctional Facility, St. Luke’s Medical Center, and several smaller light industrial developments. This separation provides physical and visual separation of the proposed airport parking area from the public right of way, as required by the provisions of the City of Hailey Parking Ordinance.</p> <p>4) The proposed airport parking area is a critical component to the overall design and functionality of the airport site as a whole. The parking area serves to provide vehicle parking for many functions within and outside of the airport site, including adjacent airplane hangars, rental cars service providers, and the traveling public. The nature of safety and circulation restrictions required by the Federal Aviation Administration create a condition where providing fewer parking spaces that are more distributed throughout the airport site is unfeasible. In order to provide and operate an effective and safe airport, the project proposes to provide parking in proximity to the airport terminal.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>9.4.7 (A) Bicycle Parking Req.</p> <p>All multi-family residential and commercial development including new construction and additions, must provide at least three (3) bicycle parking spaces or bicycle spaces equivalent to twenty five (25%) of the required number of vehicle parking spaces, whichever is greater</p>	
			<p>Findings</p> <ul style="list-style-type: none"> - The requirement is for 32 parking spaces, therefore 8 bike racks are required. - The site plan shows eight bicycle parking spaces and is compliant. 	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>8B.4.1 Outdoor Lighting Standards</p>	<p>8B.4.1 General Standards</p> <p>a. All exterior lighting shall be designed, located and lamped in order to prevent:</p> <ol style="list-style-type: none"> 1. Overlighting; 2. Energy waste; 3. Glare;

				<p>4. Light Trespass; 5. Skyglow.</p> <p>b. All non-essential exterior commercial and residential lighting is encouraged to be turned off after business hours and/or when not in use. Lights on a timer are encouraged. Sensor activated lights are encouraged to replace existing lighting that is desired for security purposes.</p> <p>c. Canopy lights, such as service station lighting shall be fully recessed or fully shielded so as to ensure that no light source is visible from or causes glare on public rights of way or adjacent properties.</p> <p>d. Area lights. All area lights are encouraged to be eighty-five (85) degree full cut-off type luminaires.</p> <p>e. Idaho Power shall not install any luminaires after the effective date of this Article that lights the public right of way without first receiving approval for any such application by the Lighting Administrator.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - All parking lot lights are proposed at a maximum height of 17', which is compliant with Section 8B.4.3(a). - All exterior lighting proposed meets the requirements of Article 8B. - The Federal Aviation Administration (FAA) requires dark-sky compliant light fixtures, which complement the City of Hailey's night sky ordinance. - Refer to Sheets 1.6, 1.6A, and 1.6B for overall site exterior lighting and Sheets 3.0, 3.1, 3.4 and 3.5 for exterior lighting specific to the Airport Operations Building.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.7 (A) Required Street Improvements Required	<p>Sidewalks and drainage improvements are required in all zoning districts, except as otherwise provided herein.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - This requirement is applicable only for projects fronting a public street. - All improvements will be located on property owned by the FMAA and do not involve encroachments or improvements to public rights-of-way. - The applicant is proposing a sidewalk on the north side of the project to provide safe pedestrian access from the parking area to the main building entrance and to the proposed bike parking area.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.7 (B) Required Water System Improvements	<p>In the Townsite Overlay District, any proposal for new construction or addition of a garage accessing from the alley, where water main lines within the alley are less than six (6) feet deep, the developer shall install insulating material (blue board insulation or similar material) for each and every individual water service line and main line between and including the subject property and the nearest public street, as recommended by the City Engineer.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - N/A
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>Bulk Req.</i>	4.13.6 <i>Bulk Requirements.</i>

			Findings	<ul style="list-style-type: none"> - According to Section 5.4 of the Zoning Ordinance, all bulk requirements in the Airport Zone are, “Subject to FAA regulations and 14 CFR, Chapter 1, Subchapter E, Part 77, Objects Affecting Navigable Airspace, as amended.” - The applicant has submitted the following in regards to meeting FAA bulk requirements <p><i>Friedman Memorial Airport, Facility Siting Requirements</i></p> <p><i>The Terminal and AOB projects proposed have been developed to be in compliance with applicable FAA criteria for building heights and setbacks as defined in FAA AC 150/5300-13A, and 14 CFR Part 77 SAFE, EFFICIENT USE, AND PRESERVATION OF THE NAVIGABLE AIRSPACE.</i></p> <p><i>FAA AC 150/5300-13A defines the safety areas for Friedman Memorial Airport in relation to design aircraft. Based on this criteria, maximum heights and setbacks are established to protect the airspace around the airfield as defined in 14 CFR Part 77, with only items fixed by function desired to penetrate the surfaces without lighting or other adjustments to airspace. Examples of items fixed by function include control towers and runway lighting.</i></p> <p><i>Based on proximity to the runway, the terminal is the most critical structure, and has been designed to accommodate relevant height restrictions. The AOB is well below typical surface requirements based on distance from runway. Necessary documentation with the FAA will be completed prior to construction to include FAA Form 7460 Notice of Proposed Construction for concurrence on our interpretation of the applicable criteria.</i></p>
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Title 18: Mobility Design Ordinance Requirements			
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<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.010 Street Classifications, Types, and Designations	
			Findings	- The requirements of Title 18 are not applicable to this application because this does not involve an “Infrastructure Project” as defined by 18.04.015.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.012 Street Design and Guideline Standards	
			Findings	- N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.016 Traffic Calming	
			Findings	- N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.022 Pedestrian Facility Design Guidelines and Standards	

			<i>Findings</i>	- N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.024 Bicycle Facility Design Guidelines and Standards	
			<i>Findings</i>	- N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.26 Street Tree Guidelines and Standards	
			<i>Findings</i>	- N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.028 Streetscape Elements Guidelines and Standards	
			<i>Findings</i>	- N/A

Design Review Requirements for Non-Residential, Multifamily, and/or Mixed Use Buildings within the City of Hailey				
1. Site Planning: 6A.8 (A) 1, items (a) thru (n)				
Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1a	<p>The location, orientation and surface of buildings shall maximize, to the greatest extent possible sun exposure in exterior spaces to create spaces around buildings that are usable by the residents and allow for safe access to buildings</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The proposed Terminal Building faces west and south, maximizing solar gain from south and west facing windows. The design utilizes windows and doors to break up all sides to serve the functional purpose of the building and public safety.</i> - <i>The Terminal is designed to accommodate both arrivals and departures with a secured and unsecured side of the Terminal. Windows and solar exposure are present in all areas of the terminal.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1b	<p>All existing plant material shall be inventoried and delineated, to scale, and noted whether it is to be preserved, relocated or removed. Removal of trees larger than 6 inch caliper proposed to be removed require an arborist review. Any tree destroyed or mortally injured after previously being identified to be preserved, or removed without authorization, shall be replaced with a species of tree found in the Tree Guide and shall be a minimum of 4 inch caliper.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>No plant material whatsoever exists where the terminal addition</i>

				<p><i>will be sited.</i></p> <ul style="list-style-type: none"> - <i>No existing plant material is located on the current site, which is dominated by asphalt and structures.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1c	<p>Site circulation shall be designed so pedestrians have safe access to and through the site and to building.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Sidewalks, crosswalks, parking lot lighting, and open spaces have all be integrated into the design to ensure appropriate pedestrian safety standards.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1d	<p>Building services including loading areas, trash storage/pickup areas and utility boxes shall be located at the rear of a building; the side of the building adjacent to an internal lot line may be considered as an alternate location. These areas shall be designed in a manner to minimize conflict among uses and shall not interfere with other uses, such as snow storage. These areas shall be screened with landscaping, enclosures, fencing or by the principal building.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>All mechanical facilities for the terminal are located within the building or entirely screened from public view</i> - <i>The existing trash pickup location will be screened and cleaned up.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)1e	<p>Where alleys exist, or are planned, they shall be utilized for building services.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>No alleys exist on site or adjacent to this project.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)1f	<p>Vending machines located on the exterior of a building shall not be visible from any street.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>No vending machines are proposed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1g	<p>On-site parking areas shall be located at the rear of the building and screened from the street. Parking and access shall not be obstructed by snow accumulation. (NOTE: If project is located in Airport West Subdivision, certain standards may apply that are not listed here. See code for details.)</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Not applicable as the project is not located on a public street.</i> - <i>Due to airport security requirements and airport operations, this is not applicable.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)1h	<p>Access to on-site parking shall be from the alley or, if the site is not serviced by an alley, from a single approach to the street to confine vehicular/pedestrian conflict to limited locations, allow more buffering of the parking area and preserve the street frontage for pedestrian traffic.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>No alleys exist adjacent to this project.</i> - <i>No street frontage.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1i	<p>Snow storage areas shall be provided on-site where practical and sited in a manner that is accessible to all types of snow removal vehicles of a size that can accommodate moderate areas of snow.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The airport has ten total areas designated for snow storage with a total storage capacity of 37,470 square feet.</i> - <i>The terminal parking area requires 31,164 square feet and 34,886</i>

				<p><i>square feet has been provided in areas “A” thru “I”.</i></p> <ul style="list-style-type: none"> - <i>The Airport Operations Building parking area requires 2,138 square feet and 2,584 square feet has been provided with Area “J” located directly adjacent to the AOB parking area.</i> - <i>All snow storage areas are practically sited and are accessible to all types of snow removal equipment.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1j	Snow storage areas shall not be less than 25% of the improved parking and vehicle and pedestrian circulation areas.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The airport has a total of 133,204 square feet of parking areas. To meet the snow storage needs of these parking area, ten total areas designated for snow storage with a total storage capacity of 37,470 square feet have been provided. The following is breakdowns of requirements for each parking area:</i> <ol style="list-style-type: none"> 1. <i>The terminal parking area comprises 124,654 square feet, resulting in 31,164 square feet required for snow storage. 34,886 square feet has been provided utilizing Areas “A-I” as illustrated on Sheet 1.1.</i> 2. <i>The Airport Operations Building parking area comprises 8,550 square feet and requires 2,138 square feet. 2,584 square feet has been provided with Area “J” located directly adjacent to the AOB parking area, as illustrated on Sheet 1.1.</i> -
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1k	A designated snow storage area shall not have any dimension less than 10 feet.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Adequate snow storage has been provided.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1l	Hauling of snow from downtown areas is permissible where other options are not practical.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>All snow will be retained on-site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1m	Snow storage areas shall not impede parking spaces, vehicular and pedestrian circulation or line of sight, loading areas, trash storage/pickup areas, service areas or utilities.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>All snow storage areas have been planned to accommodate access, safety, trash pickup services, and utility services.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1n	Snow storage areas shall be landscaped with vegetation that is salt-tolerant and resilient to heavy snow.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The proposed snow storage areas are proposed for natural grass species seeding that are hardy, moderately salt tolerant, and resilient to heavy snow.</i>
2. Building Design: 6A.8 (A) 2, items (a) thru (m)				

Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2a	The proportion, size, shape and rooflines of new buildings shall be compatible with surrounding buildings.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The surrounding buildings are mainly aircraft hangars, the airport terminal, storage facilities, and airport operations buildings. Adjacent to this site are a variety of industrial and commercial uses.</i> - <i>The proportion, size, shape, and rooflines of the proposed structure are compatible with surrounding buildings.</i> - <i>The variation of the building heights is intended to reduce the bulk massing of the building and provide visual interest. The proportion, shape, and size of the rooflines are consistent with existing aircraft hangars and service buildings on the airport site, and also consistent with buildings located in the surrounding area within the Airport Way Subdivision.</i> - <i>The choice of materials for the façade of the building matches the surrounding buildings in Airport West and within the airport campus.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2b	Standardized corporate building designs are prohibited.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The design is not a standard corporate design.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2c	At ground level, building design shall emphasize human scale, be pedestrian oriented and encourage human activity and interaction.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The design breaks up the scale of the building with a varied roofline, awnings, materials, and other aspects that reduce the overall scale.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2d	The front façade of buildings shall face the street and may include design features such as windows, pedestrian entrances, building off-sets, projections, architectural detailing, courtyards and change in materials or similar features to create human scale and break up large building surfaces and volumes.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Not applicable. No street is adjacent to this site.</i> -
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2e	Any addition onto or renovation of an existing building shall be designed to create a cohesive whole.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The project is for new construction.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2f	All exterior walls of a building shall incorporate the use of varying materials, textures and colors.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The proposed design succeeds by incorporating varying materials, textures and colors to create a cohesive design that is appealing and scaled down.</i>

				<ul style="list-style-type: none"> - <i>The exterior walls of the proposed Terminal incorporate the use of varying materials, textures, and colors.</i> - <i>The exterior wall cladding proposed for the project is a mix of metal wall panels with varying color and profiles proportional to the scale and different massing components of the Administration and ARFF and Snow Removal area.</i> - <i>Stone wainscoting accents the building and porte-cochere.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2g	<p>Exterior buildings colors and materials shall be integrated appropriately into the architecture of the building and be harmonious within the project and with surrounding buildings.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - <i>The proposed building colors and facades are compatible with existing buildings at the airport and within the Airport West subdivision.</i> - <i>This building will be a good addition to the airport and will make for a more appealing experience at the terminal</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2h	<p>Flat-roofed buildings over two stories in height shall incorporate roof elements such as parapets, upper decks, balconies or other design elements.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - The design incorporates several roof lines and designs with awnings and varied materials breaking up the flat roofs. - The pitched roof of the entry ways are designed to break up the scale of the flat roof.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2i	<p>All buildings shall minimize energy consumption by utilizing alternative energy sources and/or passive solar techniques. At least three (3) of the following techniques, or an approved alternative, shall be used to improve energy cost savings and provide a more comfortable and healthy living space:</p> <ul style="list-style-type: none"> i) Solar Orientation. If there is a longer wall plane, it shall be placed on an east-west axis. A building’s wall plane shall be oriented within 30 degrees of true south. ii) South facing windows with eave coverage. At least 40% of the building’s total glazing surface shall be oriented to the south, with roof overhang or awning coverage at the south. iii) Double glazed windows. iv) Windows with Low Emissivity glazing. v) Earth berming against exterior walls vi) Alternative energy. Solar energy for electricity or water heating, wind energy or another approved alternative shall be installed on-site. vii) Exterior light shelves. All windows on the southern most facing side of the building shall have external light shelves installed. <p><i>Findings</i></p> <p>The proposed Terminal building proposes among others, the following energy efficient strategies:</p> <ol style="list-style-type: none"> 1. The terminal addition will be primarily oriented on an East/West axis to take advantage of mitigated passive solar gain through southern exposure. 2. Horizontal sun controls louvers will be employed to control

				<p>summer sunlight when higher in the sky. The window and door units will be provided with low emissivity glazing.</p> <p>3. All windows have roof projections for weather protection or sun screening.</p> <p>4. Northern facing glazing is employed to provide views to both the mountains beyond and to the airside operations. Daylight sensors will be utilized to reduce lighting electrical loads during daytime hours. Motion Activated parking area lighting which dims automatically during low-use periods</p> <p>5. High performance, low-E, argon gas filled double pane insulated units will be used for all glazing to maximize visible light transmittance but minimize glare and solar heat transmission.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2j	<p>Gabled coverings, appropriate roof pitch, or snow clips and/or gutters and downspouts shall be provided over all walkways and entries to prevent snow from falling directly onto adjacent sidewalks.</p> <p><i>Findings</i> - No roof pitches are proposed. Roofs are flat.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2k	<p>Downspouts and drains shall be located within landscape areas or other appropriate locations where freezing will not create pedestrian hazards.</p> <p><i>Findings</i> - All proposed roof drainage components will be internally drained within the building and extended to below grade drainage structures. Overflow drain outlets will be located per the building code requirements and in appropriate locations where freezing will not create pedestrian hazards.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2l	<p>Vehicle canopies associated with gas stations, convenience stores or drive-through facilities shall have a minimum roof pitch of 3/12 and be consistent with the colors, material and architectural design used on the principal building(s).</p> <p><i>Findings</i> - N/A</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2m	<p>A master plan for signage is required to ensure the design and location of signs is compatible with the building design and compliance with Article 8.</p> <p><i>Findings</i> - N/A</p>
<p>3. Accessory Structures, Fences and Equipment/Utilities: 6A.8 (A) 3, items (a) thru (i)</p>				
Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and Findings
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3a	Accessory structures shall be designed to be compatible with the principal building(s).

			<i>Findings</i>	- <i>No accessory structures are proposed</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3b	Accessory structures shall be located at the rear of the property.
			<i>Findings</i>	- <i>N/A</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3c	Walls and fences shall be constructed of materials compatible with other materials used on the site.
			<i>Findings</i>	- <i>All new fencing will match existing on the airport site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3d	Walls and fencing shall not dominate the buildings or the landscape. Planting should be integrated with fencing in order to soften the visual impact.
			<i>Findings</i>	- <i>All new fencing will match existing. The approach to the airport has been designed to be more inviting and accommodating, including the fencing.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3e	All roof projections including, roof-mounted mechanical equipment, such as heating and air conditioning units, but excluding solar panels and Wind Energy Systems that have received a Conditional Use Permit, shall be shielded and screened from view from the ground level of on-site parking areas, adjacent public streets and adjacent properties.
			<i>Findings</i>	- <i>All roof mounted mechanical equipment is screened with architectural metal screens with a finish and color compatible with the building.</i> - <i>All roof mounted equipment has been placed in the center of the roof to minimize any visual impacts from the ground.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3f	The hardware associated with alternative energy sources shall be incorporated into the building's design and not detract from the building and its surroundings.
			<i>Findings</i>	- <i>No alternative energy sources are located on the building.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3g	All ground-mounted mechanical equipment, including heating and air conditioning units, and trash receptacle areas shall be adequately screened from surrounding properties and streets by the use of a wall, fence, or landscaping, or shall be enclosed within a building.
			<i>Findings</i>	- <i>All roof mounted mechanical equipment is screened with architectural metal screens with a finish and color compatible with the building.</i> - <i>All roof mounted equipment has been placed in the center of the roof to minimize any visual impacts from the ground.</i> - <i>All mechanical equipment is roof-mounted</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3h	All service lines into the subject property shall be installed underground.
			<i>Findings</i>	- <i>All utility and services lines will be installed underground.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3i	Additional appurtenances shall not be located on existing utility poles.
			<i>Findings</i>	- <i>N/A</i>

4. Landscaping: 6A.8 (A) 4, items (a) thru (n)

Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4a	a. Only drought tolerant plant species and/or xeriscape specific plant materials shall be used, as specified by the Hailey Landscaping Manual or an approved alternative.
			<i>Findings</i>	<ul style="list-style-type: none"> - The landscape plan indicates that plantings will be drought tolerant and proposes native and hardy evergreens, grasses, and deciduous trees. The following is the proposed species and quantities: <ul style="list-style-type: none"> - (29) Ponderosa Pine for street trees - (8) Lodgepole Pine for landscape trees - (15) Swedish Aspens for landscape trees - (350) Ornamental grasses and shrubs <ul style="list-style-type: none"> ▪ Feather Reed Grass ▪ Blue Oat Grass ▪ Ribbon Grass - (22) Evergreen Shrubs <ul style="list-style-type: none"> ▪ Dwarf Mugo Pin ▪ Compressed Scotch Pine ▪ Spreading Yew ▪ Black Arborvitae
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4b	All plant species shall be hardy to the Zone 4 environment.
			<i>Findings</i>	<ul style="list-style-type: none"> - All plant species are native to Idaho and/or hardy to Zone 4 and below.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4c	At a minimum, a temporary irrigation system that fully operates for at least two complete growing seasons is required in order to establish drought tolerant plant species and/or xeriscape specific plant materials. Features that minimize water use, such as moisture sensors, are encouraged.
			<i>Findings</i>	<ul style="list-style-type: none"> - The landscape plan details an irrigation system that incorporates automatic sprinkler system with water conservation technologies. - Water conservation techniques include drip irrigation.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4d	Landscaped areas shall be planned as an integral part of the site with consideration of the urban environment. A combination of trees shrubs, vines, ground covers and ornamental grasses shall be used. New landscaped areas having more than 10 trees, a minimum of 10% of the trees shall be at least 4-inch caliper, 20% shall be at least 3-inch caliper, and 20% shall be at least 2½ inch caliper and a maximum of 20% of any single tree species may be used in any landscape plan (excluding street trees). New planting areas shall be designed to accommodate typical trees at maturity. Buildings within the LI and SCI-I zoning district are excluded from this standard.
			<i>Findings</i>	<ul style="list-style-type: none"> - All trees shown will have a caliper of at least 5 inches with planting height of 8'-10'

				<ul style="list-style-type: none"> - Ground covers are a mix of hardy native grasses and low-lying shrubs. - NOTE: FAA regulations do not allow large deciduous trees within close proximity of aircraft operations for safety considerations.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4e	<p>Seasonal plantings in planter boxes, pots, and/or hanging baskets shall be provided to add color and interest to the outside of buildings in the LI and SCI-I zoning districts.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - Not applicable to this zoning district.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4f	<p>Plantings for pedestrian areas within the B, LB, TN and SCI-O zoning districts shall be designed with attention to the details of color, texture and form. A variety of trees, shrubs, perennials, ground covers and seasonal plantings, with different shapes and distinctive foliage, bark and flowers shall be used in beds, planter boxes, pots, and/or hanging baskets.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - Not applicable to this zoning district.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4g	<p>Storm water runoff should be retained on the site wherever possible and used to irrigate plant materials.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - <i>All storm water will be retained on site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4h	<p>A plan for maintenance of the landscaping areas is required to ensure that the project appears in a well maintained condition (i.e., all weeds and trash removed, dead plant materials removed and replaced).</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - A maintenance plan has been submitted with the landscape plan.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4i	<p>Retaining walls shall be designed to minimize their impact on the site and the appearance of the site.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4j	<p>Retaining walls shall be constructed of materials that are utilized elsewhere on the site, or of natural or decorative materials.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4k	<p>Retaining walls, where visible to the public and/or to residents or employees of the project, shall be no higher than four feet or terraced with a three foot horizontal separation of walls.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4l	<p>Landscaping should be provided within or in front of extensive retaining walls.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4m	<p>Retaining walls over 24" high may require railings or planting buffers for safety.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4n	<p>Low retaining walls may be used for seating if capped with a surface of at least 12 to 16 inches wide.</p>

		<i>Findings</i>	- No retaining walls are proposed
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Design Review Requirements for Non-Residential Buildings located in B, LB, or TN Zoning Districts (6.A.8.B. 1-3)

1. Site Planning: 6A.8.B.1

Compliant			Standards and Findings	
Yes	No	N/A	Guideline	City Standards and <i>Findings</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.1.a	a. The site shall support pedestrian circulation and provide pedestrian amenities. Sidewalks shall be provided along building fronts.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.1.b	b. Wider sidewalks are encouraged to provide additional amenities such as seating areas and bicycle racks underground utilities for new dwelling units.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>

2. Building Design: 6A.8.B.2.a-9

Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and <i>Findings</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.a	a. The main facade shall be oriented to the street. The main entrance(s) to the building shall be located on the street side of the building. If the building is located on a corner, entrances shall be provided on both street frontages. If the design includes a courtyard, the main entrance may be located through the courtyard. Buildings with more than one retail space on the ground floor are encouraged to have separate entrances for each unit.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.b	b. Multi-unit structures shall emphasize the individuality of units or provide visual interest by variations in roof lines or walls or other human scale elements. Breaking the facades and roofs of buildings softens the institutional image which may often accompany large buildings.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.c	c. Building designs shall maximize the human scale of buildings and enhance the small town "sense of place". This can be achieved by utilizing voids and masses, as well as details, textures, and colors on building facades. Human scale can also be achieved by incorporating structural elements such as colonnades and covered walkways, overhangs, canopies, entries, and landscaping. Particular attention should be paid to creating interest at the street level.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.d	d. Buildings that exceed 30 feet in height, the entire roof surface shall not project to the highest point of the roof. The Commission shall review building height relative to the other dimensions of width and depth combined with detailing of parapets, cornices, roof, and other architectural elements.

			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.e	e. Livable outdoor spaces in multi-story buildings that create pleasing elements and reduce the mass of taller buildings are encouraged.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.f	f. Fire department staging areas shall be incorporated into the design elements of the building
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.g	g. New buildings adjacent to residential areas shall be designed to ensure that building massing and scale provide a transition to adjoining residential neighborhoods. Possible mitigation techniques include, but are not limited to the following: i) Locating open space and preserving existing vegetation on the edge of the site to further separate the building from less intensive uses; ii) Stepping down the massing of the building along the site's edge; and iii) Limiting the length of or articulating building facades to reflect adjacent residential patterns
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>

4. Landscaping: 6A.8.B.3.a

Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and <i>Findings</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.3.a	a. When abutting the LR, GR or TN zoning districts, a landscape buffer between the project and the residential property shall be provided. The buffer shall be at least eight foot wide to create a year-round visual screen of at least 6 feet in height. The buffer shall be designed to avoid the appearance of a straight line or wall of uniform plant material and shall be wide enough to accommodate the planted species when mature.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>

Design Review Requirements for Non-Residential Buildings located in LI, SCI, TI or A Zoning Districts (6.A.8.C. 1)

1. Site Planning: 6A.8.C.1

Compliant			Standards and Findings	
Yes	No	N/A	Guideline	City Standards and <i>Findings</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.C.1.a	Adjoining parcels shall be considered when planning building configuration, vehicular circulation and access, parking, and drainage.
			<i>Findings</i>	- <i>Buildings have been designed to eliminate "back" sides of the building to be more appealing to adjoining properties and visitors to the airport.</i>

				<ul style="list-style-type: none"> - <i>Vehicle circulation is primarily accessed along a private driveway that will be lined with evergreens and low-lying landscaping. No access easements are required. No complaints have been received concerning access to the airport.</i> - <i>Parking has been planned to accommodate all parking needs on site without the need to encroach on adjoining properties.</i> - <i>All drainage will be retained on site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.C.1.b	<p>Reciprocal vehicular ingress and egress, circulation, and parking arrangements are encouraged when the adjacent site(s) allows in order to facilitate the ease of vehicular movement between adjoining properties.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - <i>A main access drive and separate egress drive efficiently facilitate circulation through the airport.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.C.1.C	<p>Vehicle circulation, parking and loading shall not block pedestrian access ways.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - <i>With the access and egress drives located off all public streets, pedestrian conflicts have been minimized.</i>

6A.6 Criteria.

A. The Commission or Hearing Examiner shall determine the following before approval is given:

1. **The project does not jeopardize the health, safety or welfare of the public.**
2. **The project conforms to the applicable specifications outlined in the Design Review Guidelines, as set forth herein, applicable requirements of the Zoning Ordinance, and City Standards.**

B. Conditions. The Commission or Hearing Examiner may impose any condition deemed necessary. The Commission or Hearing Examiner may also condition approval of a project with subsequent review and/or approval by the Administrator or Planning Staff. Conditions which may be attached include, but are not limited to those which will:

1. **Ensure compliance with applicable standards and guidelines.**
2. **Require conformity to approved plans and specifications.**
3. **Require security for compliance with the terms of the approval.**
4. **Minimize adverse impact on other development.**
5. **Control the sequence, timing and duration of development.**
6. **Assure that development and landscaping are maintained properly.**
7. **Require more restrictive standards than those generally found in the Zoning Ordinance.**

CONCLUSIONS OF LAW

Based upon the above Findings of Fact, the Commission makes the following Conclusions of Law:

1. Adequate notice, pursuant to Zoning Ordinance No. 532, Section 6A.5, was given.

2. The project is in general conformance with the Hailey Comprehensive Plan.
3. The project does not jeopardize the health, safety, or welfare of the public.
4. Upon compliance with the conditions set forth, the project conforms to the applicable standards of Article 6A, Design Review, other Articles of the Zoning Ordinance and City Standards.

DECISION

The Design Review request submitted by Friedman Memorial Airport Authority for alterations and additions to the current Terminal building including a major expansion, renovation, and site plan alterations to the Friedman Memorial Airport located at the Friedman Memorial Airport, FR Section 10, 15, and 22 (Tax Lot 8151), within the Airport District (A) is hereby approved subject to the following terms and conditions:

1. The project shall be constructed in accordance with the application or as modified by these Findings of Fact, Conclusions of Law and Decision.
2. Any change in use or occupancy type from that approved at time of issuance of Building Permit may require additional improvements and/or approvals. Additional parking may also be required upon subsequent change in use, in conformance with Hailey's Zoning Ordinance at the time of the new use.
3. All City infrastructure requirements shall be met. Detailed plans for all infrastructure to be installed or improved at or adjacent to the site shall be submitted for Department Head approval and shall meet City Standards where required
4. A sidewalk and drainage improvements, running the length of the property line adjacent to the public right of way is required. An In-lieu contribution to the City of Hailey for sidewalk improvements will be acceptable, according to 6A.7.6 of the Zoning Ordinance.
5. Building service areas shall be located off alley and away from public view or building frontage areas.
6. Blue board insulating material is required for water mains and individual water service lines less than 6 feet deep.
7. All new and existing exterior lighting shall comply with the Outdoor Lighting Ordinance.
8. This Design Review approval shall expire one (1) year from the date these Findings of Fact are signed, unless a building permit application has been submitted to the Building Department.
9. This project is subject to Development Impact Fees pursuant to Municipal Code Chapter 15.16. The estimated fee is determined at the time of Building Permit application.
10. Except as otherwise provided, all the required improvements shall be constructed and completed, or sufficient security provided as approved by the City Attorney, before a Certificate of Occupancy can be issued.

11. Except as otherwise provided, all the required building, landscaping, site, infrastructure improvements and all other conditions of approval shall be constructed and completed, or security in the amount of 150% of the estimated cost as approved by the City, before a Certificate of Occupancy will be issued.
12. This Design Review approval is subject to the following conditions:
 - a) All applicable Fire Department and Building Department requirements shall be met, including but not limited to:
 - a. Prior to installation of the new fence, a fence permit shall be obtained from the Community Development Department and the plans for the fence shall meet the standards of Section 8.1, Hailey Zoning Ordinance.
 - b. No auxiliary apparatus (e.g. utility meters, fire suppression equipment) may extend into any public right-of-way.
 - c. Any change in use or occupancy type from that approved at time of issuance of Building Permit may require additional parking, improvements and/or approvals.
13. Subject to all restrictions listed on the recorded plat for this lot and, if applicable, subdivisions.

A party aggrieved by a final decision of the Commission may appeal in writing any final decision by filing a Notice of Appeal with the Hailey City Clerk within fifteen (15) days from the date of the decision.

Signed this ____ day of _____, 2014.

Jay Cone, Chair

Attest:

Kristine Hilt, Community Development Coordinator

FINDINGS OF FACT, CONCLUSIONS OF LAW AND DECISION

On June 9, 2014 the Hailey Planning & Zoning Commission considered an application submitted by Friedman Memorial Airport Authority for Design Review of a new Airport Operations Building and accompanying site plan alterations to the Friedman Memorial Airport located at Tax Lot 8151 within the Airport District (A). The Commission, having been presented with all information and testimony in favor and in opposition to the proposal, hereby makes the following Findings of Fact, Conclusions of Law and Decision.

FINDINGS OF FACT

Applicant: Friedman Memorial Airport Authority

Request: Design Review of new construction of the Airport Operation Building

Location: Friedman Memorial Airport, FR Section 10, 15, and 22 (Tax Lot 8151)

Zoning: Airport (A)

Notice:

Notice of the public hearing on June 9, 2014 was published in the Idaho Mountain Express on Wednesday, May 21, 2014. Notice was mailed to all adjoining property owners on Wednesday, May 21, 2014.

Application:

The Friedman Memorial Airport Authority (FMAA) request design review approval of a new Airport Operation Building that would house the administrative offices for the airport as well as the Aircraft Rescue, Recovery, and Firefighting (ARFF) services for the airport. The Operations Building comprises a total of 14,146 square feet with 13,537 square feet on the main floor and 609 square foot mezzanine above the main floor. The total footprint of the proposed building will be 13,515 square feet. The building has been designed with architectural features to minimize the scale of the building. The north and west sides are landscaped with the east and south sides designed to meet the needs of daily and emergency operations at the airport. The height of the building is 24' at its maximum with a 14' height over the administrative portions of the building. All mechanical equipment as proposes is screened and placed out of public view. Awnings, varied wall materials, contrasting/complimentary colors, landscaping, and a broken roof line all contribute to a structure that serves a practical and necessary function. This proposed building would replace the current administrative building and the ARFF building, both of which would be demolished to make space for a new taxiway bypass.

Procedural History

On June 9, 2014, the Hailey Planning and Zoning Commission held a public hearing to consider the request from the Friedman Memorial Airport Authority for design review of a new Airport Operations Building to be located at the airport. After holding a public hearing and deliberation among the Commissioners, the Planning and Zoning Commission voted unanimously to approve the request without conditions.

General Requirements for all Design Review Applications				
Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.5 (B)	Complete Application
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Department and Boards/Commissions Comments	<p>Life/Safety:</p> <ul style="list-style-type: none"> - Police Department <ul style="list-style-type: none"> o No concerns with presented design review request. - Fire Department <ul style="list-style-type: none"> o With proposed plan, a dry suppression system is required. o Lower access drive to the lower parking lot should be a minimum of 20' wide.
				<p>Water</p> <ul style="list-style-type: none"> - The City of Water Department requires an EZ Valve installed North of our pressure reducing station (PRV). This would allow the Airport to relocate our water main at the airport terminal without having to bypass the PRV, and increasing water pressures in south Woodside over 100 psi. - The conditions the Water Department are requiring, are: <ol style="list-style-type: none"> 1. They must dig up the water main at a location convenient for the Airport and the water dept. to confirm Water Main size and to install EZ Valve 2. Trench must be adequate for EZ valve installation 3. Must hire EZ valve contractor to install the valve (i.e. Ferguson Waterworks) 4. Once valve is installed a valve box must be supplied and installed by the airport following City standards. Also all bedding and back fill must follow City standards and is the responsibility of the airport. The water department must inspect bedding and valve box placement over the valve to ensure we can turn valve on and off.
				<p>Sewer</p> <ul style="list-style-type: none"> - Manholes should be offset from the center of the access drives to match City Standards

			<ul style="list-style-type: none"> - The private lift station and accompanying private pressure line should be abandoned. No concerns with closing the lift station. - A 20' PUE may be necessary to obtain by the developer if they decide to hook into the city sewer main through adjacent private property. - The service line shown on the plans running west from the new terminal building will need to gravity feed across a parking lot to a gravity sewer line. Currently, it is shown connecting into the pressure sewer line, which will be abandoned with the lift station. <p>Building:</p> <ul style="list-style-type: none"> - More detail is needed to ensure that proper safety precautions are taken during construction such as adequate separation between the public and the construction areas. - Plans are not adequate to show that safety issues have been addressed. This will not hold up Design Review, but the applicant is advised that more precautions are necessary to obtain a building permit - <p>Streets:</p> <ul style="list-style-type: none"> - No Concerns. No city streets are included or altered in this design review request. <p><u>Boards and Commissions</u></p> <p>Tree Committee:</p> <ul style="list-style-type: none"> - N/A <p>Parks and Lands Board</p> <ul style="list-style-type: none"> - N/A <p>Hailey Arts and Historic Preservation Commission:</p> <ul style="list-style-type: none"> - N/A
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>8.2 Signs</p> <p>8.2 Signs: The applicant is hereby advised that a sign permit is required for any signage exceeding four square feet in sign area. Approval of signage areas or signage plan in Design Review does not constitute approval of a sign permit.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - Any signage will be subject to the sign regulations in Section 8.2. - No signage is proposed with this application. - The applicant has indicated that signage will be submitted to the City for approval under a separate signage permit application.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>9.4 On-site Parking Req.</p> <p>See Section 9.4 for applicable code.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - The administrative and operations building falls under the parking regulations of 9.4.2 which requires 1 space for 1,000 gross square feet of building.

				<ul style="list-style-type: none"> - <i>Per the City of Hailey Parking Ordinance – Title 9 and calculated occupancy loads for the Airport Operations Building, 14 parking spaces are required for the project (14,146 square feet of building area).</i> - <i>Seventeen (17) parking spaces have been provided on-site to accommodate employees and visitors. One (1) accessible van space has been provided and located on the shortest route of travel to the main building entrance in compliance with the International Building Code. Refer to Sheet 1.1 Site Plan.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	9.4.7 (A) Bicycle Parking Req.	<p>All multi-family residential and commercial development including new construction and additions, must provide at least three (3) bicycle parking spaces or bicycle spaces equivalent to twenty five (25%) of the required number of vehicle parking spaces, whichever is greater</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Four bike parking spaces have been provided on the north west side of the building and adjacent to the sidewalk.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	8B.4.1 Outdoor Lighting Standards	<p>8B.4.1 General Standards</p> <ul style="list-style-type: none"> a. All exterior lighting shall be designed, located and lamped in order to prevent: <ol style="list-style-type: none"> 1. Overlighting; 2. Energy waste; 3. Glare; 4. Light Trespass; 5. Skyglow. b. All non-essential exterior commercial and residential lighting is encouraged to be turned off after business hours and/or when not in use. Lights on a timer are encouraged. Sensor activated lights are encouraged to replace existing lighting that is desired for security purposes. c. Canopy lights, such as service station lighting shall be fully recessed or fully shielded so as to ensure that no light source is visible from or causes glare on public rights of way or adjacent properties. d. Area lights. All area lights are encouraged to be eighty-five (85) degree full cut-off type luminaires. e. Idaho Power shall not install any luminaires after the effective date of this Article that lights the public right of way without first receiving approval for any such application by the Lighting Administrator.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>All parking lot lights are proposed at a maximum height of 17', which is compliant with Section 8B.4.3(a).</i> - <i>All exterior lighting proposed meets the requirements of Article 8B.</i> - <i>The Federal Aviation Administration (FAA) requires dark-sky compliant light fixtures, which complement the City of Hailey's night sky ordinance.</i> - <i>Refer to Sheets 1.6, 1.6A, and 1.6B for overall site exterior lighting and Sheets 3.0, 3.1, 3.4 and 3.5 for exterior lighting specific to the Airport Operations Building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.7 (A) Required Street	<p>Sidewalks and drainage improvements are required in all zoning districts, except as otherwise provided herein.</p>

			Improvements Required	
			Findings	<ul style="list-style-type: none"> - <i>This requirement is applicable only for projects fronting public streets.</i> - <i>All improvements will be located on property owned by the FMAA and do not involve encroachments or improvements to public rights-of-way.</i> - <i>The applicant is proposing a sidewalk on the north side of the project to provide safe pedestrian access from the parking area to the main building entrance and to the proposed bike parking area.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.7 (B) Required Water System Improvements	<p>In the Townsite Overlay District, any proposal for new construction or addition of a garage accessing from the alley, where water main lines within the alley are less than six (6) feet deep, the developer shall install insulating material (blue board insulation or similar material) for each and every individual water service line and main line between and including the subject property and the nearest public street, as recommended by the City Engineer.</p>
			Findings	<ul style="list-style-type: none"> - N/A
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Bulk Req.	4.13.6 Bulk Requirements.
			Findings	<ul style="list-style-type: none"> - <i>According to Section 5.4 of the Zoning Ordinance, all bulk requirements in the Airport Zone are, "Subject to FAA regulations and 14 CFR, Chapter 1, Subchapter E, Part 77, Objects Affecting Navigable Airspace, as amended."</i> - <i>The applicant has submitted the following in regards to meeting FAA bulk requirements</i> <p><i>Friedman Memorial Airport, Facility Siting Requirements</i></p> <p><i>The Terminal and AOB projects proposed have been developed to be in compliance with applicable FAA criteria for building heights and setbacks as defined in FAA AC 150/5300-13A, and 14 CFR Part 77 SAFE, EFFICIENT USE, AND PRESERVATION OF THE NAVIGABLE AIRSPACE.</i></p> <p><i>FAA AC 150/5300-13A defines the safety areas for Friedman Memorial Airport in relation to design aircraft. Based on this criteria, maximum heights and setbacks are established to protect the airspace around the airfield as defined in 14 CFR Part 77, with only items fixed by function desired to penetrate the surfaces without lighting or other adjustments to airspace. Examples of items fixed by function include control towers and runway lighting.</i></p>

				<ul style="list-style-type: none"> - <i>Based on proximity to the runway, the terminal is the most critical structure, and has been designed to accommodate relevant height restrictions. The AOB is well below typical surface requirements based on distance from runway. Necessary documentation with the FAA will be completed prior to construction to include FAA Form 7460 Notice of Proposed Construction for concurrence on our interpretation of the applicable criteria.</i>
Title 18: Mobility Design Ordinance Requirements				
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.010 Street Classifications, Types, and Designations	
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The requirements of Title 18 are not applicable to this application because this does not involve an "Infrastructure Project" as defined by 18.04.015.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.012 Street Design and Guideline Standards	
			<i>Findings</i>	<ul style="list-style-type: none"> - N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.016 Traffic Calming	
			<i>Findings</i>	<ul style="list-style-type: none"> - N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.022 Pedestrian Facility Design Guidelines and Standards	
			<i>Findings</i>	<ul style="list-style-type: none"> - N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.024 Bicycle Facility Design Guidelines and Standards	
			<i>Findings</i>	<ul style="list-style-type: none"> - N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.26 Street Tree Guidelines and Standards	
			<i>Findings</i>	<ul style="list-style-type: none"> - N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.028 Streetscape Elements Guidelines and Standards	
			<i>Findings</i>	<ul style="list-style-type: none"> - N/A

Design Review Requirements for Non-Residential, Multifamily, and/or Mixed Use Buildings within the City of Hailey

1. Site Planning: 6A.8 (A) 1, items (a) thru (n)					
Compliant			Standards and Findings		
Yes	No	N/A	City Code	City Standards and Findings	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1a	<p>The location, orientation and surface of buildings shall maximize, to the greatest extent possible sun exposure in exterior spaces to create spaces around buildings that are usable by the residents and allow for safe access to buildings</p>	
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The proposed Airport Operations Building is situated roughly north-south with windows and doors breaking all sides to serve the functional purpose of the building.</i> - <i>The building has been oriented and positioned on the site to accomplish the following goals that address functionality, accessibility, and interest:</i> <ul style="list-style-type: none"> o <i>Orient the building to take advantage of solar exposure to the greatest extent possible for the large equipment bay doors and adjacent aprons.</i> o <i>Provide reasonable maneuvering clearances adjacent to existing buildings for airport operations, A.R.F.F. apparatus, snow removal equipment, and other vehicles.</i> o <i>Position the building to provide direct A.R.F.F. apparatus access to the runway areas.</i> o <i>Provide adequate circulation for fuel equipment operations.</i> o <i>Position the structure to provide the administrative staff with as much visible access as possible to the runway and Terminal parking areas.</i> o <i>Maintain the existing Storage building and open air canopy storage structure.</i> o <i>Position the building to accommodate potential expansion of aircraft operation and parking to the south of the terminal.</i> o <i>Provide public access and parking to the administration portions of the building.</i> 	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1b	<p>All existing plant material shall be inventoried and delineated, to scale, and noted whether it is to be preserved, relocated or removed. Removal of trees larger than 6 inch caliper proposed to be removed require an arborist review. Any tree destroyed or mortally injured after previously being identified to be preserved, or removed without authorization, shall be replaced with a species of tree found in the Tree Guide and shall be a minimum of 4 inch caliper.</p>	
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>No existing plant material is located on the current site, which is dominated by asphalt and gravel.</i> - <i>A new "site" will be created to accommodate this building.</i> 	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1c	<p>Site circulation shall be designed so pedestrians have safe access to and through the site and to building.</p>	

			Findings	<ul style="list-style-type: none"> - A designated, delineated employee and visitor vehicular parking area is proposed for the Airport Operations Building project. - Safe pedestrian access to the building from the parking is provided by a concrete sidewalk that runs along the entire length of the main building entrance. - For air side circulation, employee access to the Administration and ARFF functions is located on the northeast side of the building away from the large vehicle and equipment bays. - Employee access to the Snow Removal functions is located on the northeast side of the building for direct access to large vehicle and equipment bays for servicing and on the southeast side of the building for safe egress and circulation away from the large service bays. Refer to Sheet 1.1 Site Plan and 3.0 Floor Plan. - Compared to the current configuration for pedestrian access to the administration offices, this proposed plan offers much more provisions for safety.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1d	<p>Building services including loading areas, trash storage/pickup areas and utility boxes shall be located at the rear of a building; the side of the building adjacent to an internal lot line may be considered as an alternate location. These areas shall be designed in a manner to minimize conflict among uses and shall not interfere with other uses, such as snow storage. These areas shall be screened with landscaping, enclosures, fencing or by the principal building.</p>
			Findings	<ul style="list-style-type: none"> - The electrical and gas meters for the Airport Operations Building are located on the southwest side of the building, screened with proposed landscaping, and do not affect proposed snow storage areas. Refer to Sheet 1.5 Landscape Plan and Sheet 3.0 Floor Plan.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)1e	<p>Where alleys exist, or are planned, they shall be utilized for building services.</p>
			Findings	<ul style="list-style-type: none"> - No alleys exist on site or adjacent to this project.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)1f	<p>Vending machines located on the exterior of a building shall not be visible from any street.</p>
			Findings	<ul style="list-style-type: none"> - No vending machines are proposed.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1g	<p>On-site parking areas shall be located at the rear of the building and screened from the street. Parking and access shall not be obstructed by snow accumulation. (NOTE: If project is located in Airport West Subdivision, certain standards may apply that are not listed here. See code for details.)</p>
			Findings	<ul style="list-style-type: none"> - With no street adjacent to this structure, this particular standard does not apply. Regardless, the applicant has proposed a parking area with landscape screening and adequate circulation for pedestrian traffic that provides space for snow storage.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)1h	<p>Access to on-site parking shall be from the alley or, if the site is not serviced by an alley, from a single approach to the street to confine vehicular/pedestrian conflict to limited locations, allow more buffering of the parking area and preserve the street frontage for pedestrian traffic.</p>

			<i>Findings</i>	<ul style="list-style-type: none"> - <i>No alleys exist adjacent to this project.</i> - <i>Access to the proposed onsite parking designated for the Airport Operations Building is serviced from a single approach to the larger parking lot area, which is also serviced by a single approach at the main visitor entrance. The proposed, relocated airfield operations service road is configured in a manner to delineate and limit intermittent circulation conflicts with the designated Airport Operations Building and Terminal Parking parking circulation.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1i	<p>Snow storage areas shall be provided on-site where practical and sited in a manner that is accessible to all types of snow removal vehicles of a size that can accommodate moderate areas of snow.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The airport has ten total areas designated for snow storage with a total storage capacity of 37,470 square feet.</i> - <i>The terminal parking area requires 31,164 square feet and 34,886 square feet has been provided.</i> - <i>The Airport Operations Building parking area requires 2,138 square feet and 2,584 square feet has been provided with Area “J” located directly adjacent to the AOB parking area.</i> - <i>All snow storage areas are practically sited and are accessible to all types of snow removal equipment.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1j	<p>Snow storage areas shall not be less than 25% of the improved parking and vehicle and pedestrian circulation areas.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The airport has a total of 133,204 square feet of parking areas. To meet the snow storage needs of these parking area, ten total areas designated for snow storage with a total storage capacity of 37,470 square feet have been provided. The following is breakdowns of requirements for each parking area:</i> <ul style="list-style-type: none"> o <i>The terminal parking area comprises 124,654 square feet, resulting in 31,164 square feet required for snow storage. 34,886 square feet has been provided utilizing Areas “A-I” as illustrated on Sheet 1.1.</i> o <i>The Airport Operations Building parking area comprises 8,550 square feet and requires 2,138 square feet. 2,584 square feet has been provided with Area “J” located directly adjacent to the AOB parking area, as illustrated on Sheet 1.1.</i> -
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1k	<p>A designated snow storage area shall not have any dimension less than 10 feet.</p>
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Adequate snow storage has been provided.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1l	<p>Hauling of snow from downtown areas is permissible where other options are not practical.</p>

			<i>Findings</i>	- <i>All snow will be retained on-site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1m	Snow storage areas shall not impede parking spaces, vehicular and pedestrian circulation or line of sight, loading areas, trash storage/pickup areas, service areas or utilities.
			<i>Findings</i>	- <i>All snow storage areas have been planned to accommodate access, safety, trash pickup services, and utility services.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1n	Snow storage areas shall be landscaped with vegetation that is salt-tolerant and resilient to heavy snow.
			<i>Findings</i>	- <i>The proposed snow storage areas are proposed for natural grass species seeding that are hardy, moderately salt tolerant, and resilient to heavy snow.</i>

2. Building Design: 6A.8 (A) 2, items (a) thru (m)

Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and <i>Findings</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2a	The proportion, size, shape and rooflines of new buildings shall be compatible with surrounding buildings.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The surrounding buildings are mainly aircraft hangars, the airport terminal, storage facilities, and airport operations buildings. Adjacent to this site are a variety of industrial and commercial uses.</i> - <i>The proportion, size, shape, and rooflines of the proposed structure are compatible with surrounding buildings. The proposed Airport Operations Building is comprised of a two main flat roof massing components.</i> - <i>The Administration portion of the facility is covered by a lower height flat roof while the ARFF and Snow Removal functions are covered with a relatively higher flat roof (24’).</i> - <i>The variation of the building heights is intended to reduce the bulk massing of the building and provide visual interest. The proportion, shape, and size of the rooflines are consistent with existing aircraft hangars and service buildings on the airport site, and also consistent with buildings located in the surrounding area within the Airport Way Subdivision.</i> - <i>The choice of materials for the façade of the building matches the surrounding buildings in Airport West and within the airport campus.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2b	Standardized corporate building designs are prohibited.
			<i>Findings</i>	- <i>The design is not a standard corporate design.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2c	At ground level, building design shall emphasize human scale, be pedestrian oriented and encourage human activity and interaction.

			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The design breaks up the scale of the building with a varied roofline, awnings, materials, and other aspects that reduce the overall scale.</i> - <i>At the ground level pedestrian/visitor side of the building the project proposes a relatively lower building mass, pedestrian circulation amenities, landscape features and projecting roof elements that provide shade and protection from weather, the main building entrance, and significant fenestration.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2d	The front façade of buildings shall face the street and may include design features such as windows, pedestrian entrances, building off-sets, projections, architectural detailing, courtyards and change in materials or similar features to create human scale and break up large building surfaces and volumes.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Not applicable. No street is adjacent to this site.</i> -
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2e	Any addition onto or renovation of an existing building shall be designed to create a cohesive whole.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The project is for new construction.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2f	All exterior walls of a building shall incorporate the use of varying materials, textures and colors.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The proposed design succeeds by incorporating varying materials, textures and colors to create a cohesive design that is appealing and scaled down.</i> - <i>The exterior walls of the proposed Airport Operations Building incorporate the use of varying materials, textures, and colors.</i> - <i>The exterior wall cladding proposed for the project is a mix of metal wall panels with varying color and profiles proportional to the scale and different massing components of the Administration and ARFF and Snow Removal area.</i> - <i>The window colors are of a dark bronze color that is compatible with both wall color finish selections and the exterior architectural sun shade devices are of a varying color compatible with the adjacent wall finish material.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2g	Exterior buildings colors and materials shall be integrated appropriately into the architecture of the building and be harmonious within the project and with surrounding buildings.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>The proposed building colors and facades are compatible with existing buildings at the airport and within the Airport West subdivision.</i> - <i>The applicant has expressed that the design of the building is intended to make for a more appealing entrance to the Friedman Memorial Airport.</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2h	Flat-roofed buildings over two stories in height shall incorporate roof elements such as parapets, upper decks, balconies or other design elements.
			<i>Findings</i>	- Two roofs are proposed for the administrative and ARFF function at 14’ and 24’ respectively. The higher flat roof building portion of the ARFF and Snow Removal is proposed with cantilevered architectural sun shade devices. Varying materials help in breaking up the roof lines and scaling down the building.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2i	<p>All buildings shall minimize energy consumption by utilizing alternative energy sources and/or passive solar techniques. At least three (3) of the following techniques, or an approved alternative, shall be used to improve energy cost savings and provide a more comfortable and healthy living space:</p> <ul style="list-style-type: none"> i) Solar Orientation. If there is a longer wall plane, it shall be placed on an east-west axis. A building’s wall plane shall be oriented within 30 degrees of true south. ii) South facing windows with eave coverage. At least 40% of the building’s total glazing surface shall be oriented to the south, with roof overhang or awning coverage at the south. iii) Double glazed windows. iv) Windows with Low Emissivity glazing. v) Earth berming against exterior walls vi) Alternative energy. Solar energy for electricity or water heating, wind energy or another approved alternative shall be installed on-site. vii) Exterior light shelves. All windows on the southern most facing side of the building shall have external light shelves installed.
			<i>Findings</i>	<p>The proposed Airport Operations Building proposes among others, the following energy efficient strategies:</p> <ol style="list-style-type: none"> 1. The long axis building wall plane solar orientation is very close to 30 degrees of true south. 2. The project will incorporate metal thermal break insulated double glazing window units. 3. The window and door units will be provided with low emissivity glazing. 4. All windows have roof projections for weather protection or sun screening. 5. High clerestory windows and integrated overhead door glazing provide significant daylighting to reduce the overall lighting demand within the interior spaces. 6. Motion Activated parking area lighting which dims automatically during low-use periods
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2j	Gabled coverings, appropriate roof pitch, or snow clips and/or gutters and downspouts shall be provided over all walkways and entries to prevent snow from falling directly onto adjacent sidewalks.

			<i>Findings</i>	- <i>No roof pitches are proposed. Roofs are flat.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2k	Downspouts and drains shall be located within landscape areas or other appropriate locations where freezing will not create pedestrian hazards.
			<i>Findings</i>	- <i>All proposed roof drainage components will be internally drained within the building and extended to below grade drainage structures. Overflow drain outlets will be located per the building code requirements and in appropriate locations where freezing will not create pedestrian hazards.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2l	Vehicle canopies associated with gas stations, convenience stores or drive-through facilities shall have a minimum roof pitch of 3/12 and be consistent with the colors, material and architectural design used on the principal building(s).
			<i>Findings</i>	- <i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2m	A master plan for signage is required to ensure the design and location of signs is compatible with the building design and compliance with Article 8.
			<i>Findings</i>	- <i>N/A</i>

3. Accessory Structures, Fences and Equipment/Utilities: 6A.8 (A) 3, items (a) thru (i)

Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and <i>Findings</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3a	Accessory structures shall be designed to be compatible with the principal building(s).
			<i>Findings</i>	- <i>No accessory structures are proposed</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3b	Accessory structures shall be located at the rear of the property.
			<i>Findings</i>	- <i>N/A</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3c	Walls and fences shall be constructed of materials compatible with other materials used on the site.
			<i>Findings</i>	- <i>All new fencing will match existing on the airport site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3d	Walls and fencing shall not dominate the buildings or the landscape. Planting should be integrated with fencing in order to soften the visual impact.
			<i>Findings</i>	- <i>All new fencing will match existing. The approach to the airport has been designed to be more inviting and accommodating, including the fencing.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3e	All roof projections including, roof-mounted mechanical equipment, such as heating and air conditioning units, but excluding solar panels and Wind Energy Systems that have received a Conditional Use Permit, shall be shielded and screened from view from the ground level of on-site parking areas, adjacent public streets and adjacent properties.
			<i>Findings</i>	- <i>All roof mounted mechanical equipment is screened with</i>

				<p><i>architectural metal screens with a finish and color compatible with the building.</i></p> <ul style="list-style-type: none"> - <i>All roof mounted equipment has been placed in the center of the roof to minimize any visual impacts from the ground.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3f	<p>The hardware associated with alternative energy sources shall be incorporated into the building's design and not detract from the building and its surroundings.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - <i>No alternative energy sources are located on the building.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3g	<p>All ground-mounted mechanical equipment, including heating and air conditioning units, and trash receptacle areas shall be adequately screened from surrounding properties and streets by the use of a wall, fence, or landscaping, or shall be enclosed within a building.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - <i>All mechanical equipment is roof-mounted</i> - <i>All roof mounted mechanical equipment is screened with architectural metal screens with a finish and color compatible with the building.</i> - <i>All roof mounted equipment has been placed in the center of the roof to minimize any visual impacts from the ground.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3h	<p>All service lines into the subject property shall be installed underground.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - <i>All utility and services lines will be installed underground.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3i	<p>Additional appurtenances shall not be located on existing utility poles.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - <i>N/A</i>

4. Landscaping: 6A.8 (A) 4, items (a) thru (n)

Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4a	<p>Only drought tolerant plant species and/or xeriscape specific plant materials shall be used, as specified by the Hailey Landscaping Manual or an approved alternative.</p> <p><i>Findings</i></p> <ul style="list-style-type: none"> - The landscape plan indicates that plantings will be drought tolerant and proposes native and hardy evergreens, grasses, and deciduous trees. The following is the proposed species and quantities: <ul style="list-style-type: none"> - (29) Ponderosa Pine for street trees - (8) Lodgepole Pine for landscape trees - (15) Swedish Aspens for landscape trees - (350) Ornamental grasses and shrubs <ul style="list-style-type: none"> ▪ Feather Reed Grass ▪ Blue Oat Grass ▪ Ribbon Grass - (22) Evergreen Shrubs <ul style="list-style-type: none"> ▪ Dwarf Mugo Pin

				<ul style="list-style-type: none"> ▪ Compressed Scotch Pine ▪ Spreading Yew ▪ Black Arborvitae
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4b	All plant species shall be hardy to the Zone 4 environment.
			<i>Findings</i>	<ul style="list-style-type: none"> - All plant species are native to Idaho and/or hardy to Zone 4 and below.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4c	At a minimum, a temporary irrigation system that fully operates for at least two complete growing seasons is required in order to establish drought tolerant plant species and/or xeriscape specific plant materials. Features that minimize water use, such as moisture sensors, are encouraged.
			<i>Findings</i>	<ul style="list-style-type: none"> - The landscape plan details an irrigation system that incorporates automatic sprinkler system with water conservation technologies. - Water conservation techniques include drip irrigation.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4d	Landscaped areas shall be planned as an integral part of the site with consideration of the urban environment. A combination of trees shrubs, vines, ground covers and ornamental grasses shall be used. New landscaped areas having more than 10 trees, a minimum of 10% of the trees shall be at least 4-inch caliper, 20% shall be at least 3-inch caliper, and 20% shall be at least 2½ inch caliper and a maximum of 20% of any single tree species may be used in any landscape plan (excluding street trees). New planting areas shall be designed to accommodate typical trees at maturity. Buildings within the LI and SCI-I zoning district are excluded from this standard.
			<i>Findings</i>	<ul style="list-style-type: none"> - All trees shown will have a caliper of at least 5 inches with planting height of 8'-10' - Ground covers are a mix of hardy native grasses and low-lying shrubs. - NOTE: FAA regulations do not allow large deciduous trees within close proximity of aircraft operations for safety considerations.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4e	Seasonal plantings in planter boxes, pots, and/or hanging baskets shall be provided to add color and interest to the outside of buildings in the LI and SCI-I zoning districts.
			<i>Findings</i>	<ul style="list-style-type: none"> - Not applicable to this zoning district.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4f	Plantings for pedestrian areas within the B, LB, TN and SCI-O zoning districts shall be designed with attention to the details of color, texture and form. A variety of trees, shrubs, perennials, ground covers and seasonal plantings, with different shapes and distinctive foliage, bark and flowers shall be used in beds, planter boxes, pots, and/or hanging baskets.
			<i>Findings</i>	<ul style="list-style-type: none"> - Not applicable to this zoning district.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4g	Storm water runoff should be retained on the site wherever possible and used to irrigate plant materials.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>All storm water will be retained on site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4h	A plan for maintenance of the landscaping areas is required to ensure that the project appears in a well maintained condition (i.e., all weeds and trash removed, dead plant

				materials removed and replaced).
			<i>Findings</i>	- A maintenance plan has been submitted with the landscape plan.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4i	Retaining walls shall be designed to minimize their impact on the site and the appearance of the site.
			<i>Findings</i>	- No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4j	Retaining walls shall be constructed of materials that are utilized elsewhere on the site, or of natural or decorative materials.
			<i>Findings</i>	- No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4k	Retaining walls, where visible to the public and/or to residents or employees of the project, shall be no higher than four feet or terraced with a three foot horizontal separation of walls.
			<i>Findings</i>	- No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4l	Landscaping should be provided within or in front of extensive retaining walls.
			<i>Findings</i>	- No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4m	Retaining walls over 24" high may require railings or planting buffers for safety.
			<i>Findings</i>	- No retaining walls are proposed
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4n	Low retaining walls may be used for seating if capped with a surface of at least 12 to 16 inches wide.
			<i>Findings</i>	- No retaining walls are proposed

Design Review Requirements for Non-Residential Buildings located in B, LB, or TN Zoning Districts (6.A.8.B. 1-3)				
1. Site Planning: 6A.8.B.1				
Compliant			Standards and Findings	
Yes	No	N/A	Guideline	City Standards and Findings
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.1.a	a. The site shall support pedestrian circulation and provide pedestrian amenities. Sidewalks shall be provided along building fronts.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.1.b	b. Wider sidewalks are encouraged to provide additional amenities such as seating areas and bicycle racks underground utilities for new dwelling units.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
2. Building Design: 6A.8.B.2.a-9				
Compliant			Standards and Findings	
Yes	No	N/A	City Code	City Standards and Findings

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.a	a. The main facade shall be oriented to the street. The main entrance(s) to the building shall be located on the street side of the building. If the building is located on a corner, entrances shall be provided on both street frontages. If the design includes a courtyard, the main entrance may be located through the courtyard. Buildings with more than one retail space on the ground floor are encouraged to have separate entrances for each unit.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.b	b. Multi-unit structures shall emphasize the individuality of units or provide visual interest by variations in roof lines or walls or other human scale elements. Breaking the facades and roofs of buildings softens the institutional image which may often accompany large buildings.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.c	c. Building designs shall maximize the human scale of buildings and enhance the small town “sense of place”. This can be achieved by utilizing voids and masses, as well as details, textures, and colors on building facades. Human scale can also be achieved by incorporating structural elements such as colonnades and covered walkways, overhangs, canopies, entries, and landscaping. Particular attention should be paid to creating interest at the street level.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.d	d. Buildings that exceed 30 feet in height, the entire roof surface shall not project to the highest point of the roof. The Commission shall review building height relative to the other dimensions of width and depth combined with detailing of parapets, cornices, roof, and other architectural elements.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.e	e. Livable outdoor spaces in multi-story buildings that create pleasing elements and reduce the mass of taller buildings are encouraged.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.f	f. Fire department staging areas shall be incorporated into the design elements of the building
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.g	g. New buildings adjacent to residential areas shall be designed to ensure that building massing and scale provide a transition to adjoining residential neighborhoods. Possible mitigation techniques include, but are not limited to the following: i) Locating open space and preserving existing vegetation on the edge of the site to further separate the building from less intensive uses; ii) Stepping down the massing of the building along the site’s edge; and iii) Limiting the length of or articulating building facades to reflect adjacent residential patterns
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>

4. Landscaping: 6A.8.B.3.a

Compliant	Standards and Findings
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Yes	No	N/A	City Code	City Standards and Findings
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.3.a	a. When abutting the LR, GR or TN zoning districts, a landscape buffer between the project and the residential property shall be provided. The buffer shall be at least eight foot wide to create a year-round visual screen of at least 6 feet in height. The buffer shall be designed to avoid the appearance of a straight line or wall of uniform plant material and shall be wide enough to accommodate the planted species when mature.
			<i>Findings</i>	- <i>Not applicable to this zoning district.</i>

Design Review Requirements for Non-Residential Buildings located in LI, SCI, TI or A Zoning Districts (6.A.8.C. 1)				
1. Site Planning: 6A.8.C.1				
Compliant			Standards and Findings	
Yes	No	N/A	Guideline	City Standards and Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.C.1.a	Adjoining parcels shall be considered when planning building configuration, vehicular circulation and access, parking, and drainage.
			<i>Findings</i>	<ul style="list-style-type: none"> - <i>Buildings have been designed to eliminate “back” sides of the building to be more appealing to adjoining properties and visitors to the airport.</i> - <i>Vehicle circulation is primarily accessed along a private driveway that will be lined with evergreens and low-lying landscaping. No access easements are required. No complaints have been received concerning access to the airport.</i> - <i>Parking has been planned to accommodate all parking needs on site without the need to encroach on adjoining properties.</i> - <i>All drainage will be retained on site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.C.1.b	Reciprocal vehicular ingress and egress, circulation, and parking arrangements are encouraged when the adjacent site(s) allows in order to facilitate the ease of vehicular movement between adjoining properties.
			<i>Findings</i>	- <i>A main access drive and separate egress drive efficiently facilitate circulation through the airport.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.C.1.C	Vehicle circulation, parking and loading shall not block pedestrian access ways.
			<i>Findings</i>	- <i>With the access and egress drives located off all public streets, pedestrian conflicts have been minimized.</i>

6A.6 Criteria.

- A. The Commission or Hearing Examiner shall determine the following before approval is given:**
- 1. The project does not jeopardize the health, safety or welfare of the public.**

2. **The project conforms to the applicable specifications outlined in the Design Review Guidelines, as set forth herein, applicable requirements of the Zoning Ordinance, and City Standards.**

B. Conditions. The Commission or Hearing Examiner may impose any condition deemed necessary. The Commission or Hearing Examiner may also condition approval of a project with subsequent review and/or approval by the Administrator or Planning Staff. Conditions which may be attached include, but are not limited to those which will:

1. **Ensure compliance with applicable standards and guidelines.**
2. **Require conformity to approved plans and specifications.**
3. **Require security for compliance with the terms of the approval.**
4. **Minimize adverse impact on other development.**
5. **Control the sequence, timing and duration of development.**
6. **Assure that development and landscaping are maintained properly.**
7. **Require more restrictive standards than those generally found in the Zoning Ordinance.**

CONCLUSIONS OF LAW

Based upon the above Findings of Fact, the Commission makes the following Conclusions of Law:

1. Adequate notice, pursuant to Zoning Ordinance No. 532, Section 6A.5, was given.
2. The project is in general conformance with the Hailey Comprehensive Plan.
3. The project does not jeopardize the health, safety, or welfare of the public.
4. Upon compliance with the conditions set forth, the project conforms to the applicable standards of Article 6A, Design Review, other Articles of the Zoning Ordinance and City Standards.

DECISION

The Design Review request submitted by Friedman Memorial Airport Authority for Design Review of a new Airport Operations Building and accompanying site plan alterations to the Friedman Memorial Airport located at Tax Lot 8151 within the Airport District (A) is hereby approved subject to the following terms and conditions:

1. The project shall be constructed in accordance with the application or as modified by these Findings of Fact, Conclusions of Law and Decision.
2. Any change in use or occupancy type from that approved at time of issuance of Building Permit may require additional improvements and/or approvals. Additional parking may also

- be required upon subsequent change in use, in conformance with Hailey's Zoning Ordinance at the time of the new use.
3. All City infrastructure requirements shall be met. Detailed plans for all infrastructure to be installed or improved at or adjacent to the site shall be submitted for Department Head approval and shall meet City Standards where required
 4. A sidewalk and drainage improvements, running the length of the property line adjacent to the public right of way is required. An In-lieu contribution to the City of Hailey for sidewalk improvements will be acceptable, according to 6A.7.6 of the Zoning Ordinance.
 5. Building service areas shall be located off alley and away from public view or building frontage areas.
 6. Blue board insulating material is required for water mains and individual water service lines less than 6 feet deep.
 7. All new and existing exterior lighting shall comply with the Outdoor Lighting Ordinance.
 8. This Design Review approval shall expire one (1) year from the date these Findings of Fact are signed, unless a building permit application has been submitted to the Building Department.
 9. This project is subject to Development Impact Fees pursuant to Municipal Code Chapter 15.16. The estimated fee is determined at the time of Building Permit application.
 10. Except as otherwise provided, all the required improvements shall be constructed and completed, or sufficient security provided as approved by the City Attorney, before a Certificate of Occupancy can be issued.
 11. Except as otherwise provided, all the required building, landscaping, site, infrastructure improvements and all other conditions of approval shall be constructed and completed, or security in the amount of 150% of the estimated cost as approved by the City, before a Certificate of Occupancy will be issued.
 12. This Design Review approval is subject to the following conditions:
 - a) All applicable Fire Department and Building Department requirements shall be met, including but not limited to:
 - a. Prior to installation of the new fence, a fence permit shall be obtained from the Community Development Department and the plans for the fence shall meet the standards of Section 8.1, Hailey Zoning Ordinance.
 - b. No auxiliary apparatus (e.g. utility meters, fire suppression equipment) may extend into any public right-of-way.
 - c. Any change in use or occupancy type from that approved at time of issuance of Building Permit may require additional parking, improvements and/or approvals.
 13. Subject to all restrictions listed on the recorded plat for this lot and, if applicable, subdivisions.

A party aggrieved by a final decision of the Commission may appeal in writing any final decision by filing a Notice of Appeal with the Hailey City Clerk within fifteen (15) days from the date of the decision.

Signed this ____ day of _____, 2014.

Jay Cone, Chair

Attest:

Kristine Hilt, Community Development Coordinator

FINDINGS OF FACT, CONCLUSIONS OF LAW AND RECOMMENDATION

On June 9, 2014, the Hailey Planning and Zoning Commission considered an application submitted by Judy A. Castle, represented by Gary D. Slette, for an amendment to the Hailey Zoning Map for Lots 11-15, Block 41, Woodside Subdivision No. 9. The proposed amendment would change the current zoning from Business (B) to General Residential (GR) to allow for the construction of single family homes on lots 11-15, Block 41, Woodside Subdivision No. 9. The Commission, having been presented with all information and testimony in favor and in opposition to the proposal, hereby makes the following Findings of Fact, Conclusions of Law and Recommendation.

FINDINGS OF FACT

Applicant: Judy A. Castle, represented by Gary D. Slette
Request: Amendment to Zoning District Map by changing the zoning for Lots 11-15, Block 41, Woodside Subdivision No. 9 from Business (B) to General Residential (GR)
Location: Lots 11-15, Block 41, Woodside Subdivision No. 9
Current Zoning: Business (B)
Proposed Zoning: General Residential (GR)

Notice

Notice for the public hearing scheduled for June 9, 2014 was published in the Idaho Mountain Express on May 21, 2014 and mailed to property owners within 300 feet on and to public agencies and area media on May 21, 2014. Notice was posted on all external boundaries of the property on May 28, 2014.

Application

The applicant, Judy A. Castle, initiated action for a zone change from Business (B) to General Residential (GR) to allow for the construction of single family homes on lots 11-15, Block 41, Woodside Subdivision No. 9. The applicant's proposal is to build five single family homes, one on each lot, and has no intention of opening a business on the lots. The current zoning designation, Business (B), does not allow for single family dwellings. The lots range from .138 acre to .165 acre, which is large enough to accommodate one dwelling unit within the GR zone. As required by GR zoning, the lots would not be large enough to subdivide individually into a higher density development.

Map of Area Proposed for Zone Change and Adjacent Parcels



Procedural History

The Planning and Zoning Commission held a public hearing on the proposed amendments on June 9, 2014 at 5:30 pm in the Council Chambers. After deliberation among the Commissioners, the Planning and Zoning Commission voted to recommend denial of the zone change request to the Hailey City Council. The Planning and Zoning Commission found the application for zone change to be inconsistent with the surrounding neighborhood and recommended the zone remain Business (B).

Analysis and Discussion

All lots are currently vacant and have not been used for any purpose. Current adjacent uses to the subject properties include self-storage facilities and professional offices to the south and east with Power Engineers comprising the entire Block 84 and several lots within Block 39. Directly south of Lot 15 is a self-storage business owned by Sunbarg Associates and Fainbarg Ventures. Directly north of the properties and adjacent and across Glenbrook Drive from Lot 11 are single-family homes. Further south of the properties and across Woodside Boulevard is the Woodside Light Industrial area where many of the city's industrial businesses are located.

The purpose of the General Residential (GR) District is to provide areas for a variety of residential uses and a limited number of other uses compatible with this type of residential development. The intent of

the GR zone is to preserve the favorable amenities associated with a residential neighborhood while providing for a variety of housing opportunities. Uses are limited to those that generate relatively little traffic. The applicant's intent is to keep the residential nature of the neighborhood and street by building single family homes, which is consistent with GR zoning. The lots have never been developed and have been vacant for many years.

Within the City of Hailey, buildable lots can be found in all neighborhoods; however the vast majority of vacant lots are located in new subdivisions or developing subdivisions, such as Old Cutters and Northridge. These subdivisions typically have a price point beyond many prospective buyers. The numbers of vacant lots are limited in the Woodside Subdivision, which has traditionally had lower real estate prices than other neighborhoods in the city. These five additional residential lots could provide individuals and families the opportunity of home ownership who otherwise would not have that opportunity in the City of Hailey.

Commercial lots are also very limited in Hailey and removing five lots from the commercial real estate stock may negatively impact the ability for five or more businesses to locate in Hailey on affordable lots with the appropriate zoning, thereby eliminating the opportunity for new jobs and investment in the local economy. In general, staff does not recommend downzoning from commercial to residential lots when the lots are clearly located in a business corridor or near the central business district. In this case, the lots were originally platted for single-family dwellings and the rezone to Business from General Residential has not produced the desired commercial developments. In addition, the lots are miles from the downtown area and central business district.

Below is a comparison of permitted, conditional and accessory uses in General Residential (GR), General Residential (GR), and Business (B) zoning districts. The change from B to GR would significantly remove the number of commercial uses allowed on the properties, however it also adds single-family homes as a permitted use.

Category	Description (Excerpt)	GR	B
Residential	Single Family Dwellings	P	N
	Manufactured Home	P	N
	Multi-Family Dwellings	P	P
	Dwelling Units Within Mixed Use Buildings	N	P
Public or Semi-Public	Churches	P	C
	Colleges, vocational and technical trade schools.	N	P
	Government offices and public administration, except correctional institutions.	N	P
	Health care and social assistance.	N	P
	Municipal Uses limited to water storage and well facilities	N	N
	Non-motorized recreational pathways	P	P
	Non-profit recreation center	N	N
	Parks	P	P
	Public recreational or cultural areas	N	N
	Public Service, Public Use and Public Utility Facilities.	C	C
	Semi-Public Uses.	C	P
	Schools and other educational services.	P	P
	PWSF's and WCF's, mounted on any proposed freestanding tower, upon the issuance of Wireless Permit in accordance with the provision of Article VIII A of this Ordinance. (Lattice towers are prohibited.)	N	C
	PWSF's or WCF's, attached to street poles or mounted on existing buildings or structures, upon the issuance of a Wireless Permit in accordance with the provisions of Article VIII A of this Ordinance. (Freestanding and lattice towers are prohibited.)	C	P
Commercial	Administrative and support services.	N	P
	Airport	N	N
	Artist studios, which have no associated gallery.	N	N
	Arts, entertainment and recreation uses (indoor and outdoor).	N	P
	Arts, entertainment and recreation uses, except outdoor arenas and amusement parks .	N	P
	Auto dealerships.	N	C
	Automobile rental companies	N	N
	Automobile towing.	N	N
	Automotive Repair and Maintenance.	N	C
	Bed and Breakfast Inn.	C	P
	Boarding and Rooming Houses.	C	P
	Broadcasting firms, media offices, and related uses.	N	P
	Business parks	N	N
	Cable television firms.	N	N

Category	Description (Excerpt)		
		GR	B
	Car rental companies.	N	N
	Catering Services.	N	P
	Commercial brewery, bakery, or food catering where no retail sales are conducted.	N	N
	Computer software development, manufacture and service firms.	N	N
	Construction and building material sales (except hardware stores).	N	N
	Construction contractors.	N	N
	Construction equipment and materials rental, storage, sales and service, excluding hardware stores.	N	N
	Construction equipment rental, storage, sales and service.	N	N
	Construction trade contractors' offices with no exterior storage.	N	N
	Construction trade contractors, excluding excavation and landscaping companies.	N	N
	Construction trade contractors, including excavation companies.	N	N
	Control Tower (Air Traffic)	N	N
	Convenience stores, in conjunction with Gasoline Stations that have no more than 1800 square feet of gross floor area. Drive-through service windows are not allowed.	N	N
	Convenience Stores.	N	N
	Dance and martial art studios, fitness facilities.	N	N
	Day Care Centers provided no more than eighteen (18) children will be cared for at any one time.	C	P
	Day Care Centers (13+ children).	C	P
	Day Care Facilities (up to 12 children)	P	P
	Day Care Homes (6 or less children)	P	P
	Employee housing for golf courses or recreational facilities	N	N
	Fabrication and repair of building materials and components, including log homes.	N	N
	Farm supply and equestrian tack and feed stores.	N	N
	Fencing supplies and installation.	N	N
	Finance and insurance firms.	N	P
	Firewood production and storage.	N	N
	Flight schools, provided regularly scheduled commercial passenger aircraft services are operated at the Airport.	N	N
	Floor covering stores.	N	N
	Gasoline Stations and Automotive Repair and Maintenance.	N	N
	Gasoline Stations, including card-lock stations.	N	N
	Gasoline Stations.	N	P
	Gift shops within terminal	N	N
	Golf course, public	N	N
	Guides and outfitters with no more than 20% of the floor area dedicated to retail sales.	N	N
	Helicopter areas, provided regularly scheduled commercial passenger aircraft services are operated at the Airport.	N	N
	Home Occupations.	P	P
	Hotels or motels with or without attached restaurants.	N	P
	Hybrid Production Facilities.	N	C
	Indoor recreational facilities primarily for instruction.	N	N
	Industrial laundry/dry cleaning service and distribution establishments.	N	N
	Interior decorating and design that have no more than 20% of the gross floor area dedicated to on-site retail sales.	N	N
	Investigation and Security Services.	N	N
	Landscape design and installation firms, and landscape nurseries.	N	N
	Landscape design, installation and maintenance firms.	N	N
	Laundromat, dry cleaning, and laundry.	N	P
	Light Manufacturing.	N	N
	Medical and personal care stores.	N	N
	Mercantile (wholesale and retail).	N	P

Category	Description (Excerpt)	GR	B
	Mixed Use Buildings.	N	P
	Motor vehicles and parts dealers, service, rental and leasing.	N	N
	Nurseries, greenhouse and floriculture production and sales.	N	N
	Outpatient Animal Services.	N	C
	Parcel delivery and shipping services.	N	
	Parking facilities and structures.	N	C
	Personal Services where retail sales are clearly incidental to the principal use and no outside storage yard or facility is required.	N	P
	Photography studios and photo processing.	N	N
	Physical fitness facilities.	N	N
	Printing and publishing firms.	N	N
	Processing and sales of firewood.	N	N
	Professional and general offices.	N	P
	Radio and television recording studios and stations.	N	N
	Real estate and property management companies.	N	P
	Recording studios (audio or video) and broadcasting studios.	N	N
	Research and development facilities.	N	N
	Residential Care Facility	P	P
	Restaurants which may or may not include the sale of alcoholic beverages.	N	N
	Restaurants within terminal	N	N
	Restaurants and bars.	N	P
	Restaurants. Drive-through service windows are not allowed.	N	N
	Retail trade limited to the following: floor covering and window treatment, household appliances, woodstoves/fireplaces, spa/hot tub, building material and garden equipment and supplies (excluding hardware stores), farm and equestrian, and non-store retail (mail-order and vending machines).	N	N
	Sales, rental, and servicing of trailers, mobile homes, farm implements and heavy equipment.	N	N
	Services to buildings (janitorial/maintenance) and property management companies.	N	N
	Sign studios and manufacturers.	N	N
	Snow removal contractors.	N	N
	Structures and/or buildings integral to a golf course such as club houses, maintenance, buildings, and rest rooms	N	N
	Truck transportation, bus, taxi and limousine services, and couriers.	N	N
	Veterinarians, pet grooming, and training with no outdoor kenneling.	N	C
	Warehouse and storage facilities, including self-storage facilities and exterior storage facilities.	N	N
	Warehouse and storage facilities.	N	N
	Wholesale distributors or Wholesale distributors with incidental and subordinate retail sales.	N	N
	Wholesale distributors.	N	N
	Wholesale trade.	N	N
Agriculture	Horses, a maximum of two (2) horses per acre on lots of one (1) acre minimum size	C	N
	Urban Agriculture	P	N
Above Ground Fuel Tanks	Above ground flammable and combustible liquid tanks utilized by a public use	C	A
	Above ground fuel tank for private or commercial use	N	P
Accessory Dwelling Unit (ADU)	One (1) Accessory Dwelling Unit on lots 7,000 square feet or larger, accessory to a single family dwelling unit or to a non-residential Principal Building. Primary vehicular access to any ADU shall be from a City street or alley. All Accessory Dwelling Units shall have adequate water and sewer services installed to meet City Standards.	A	A
Alternative Energy Systems	Freestanding Solar Panels, subject to the maximum building height for the applicable district.	C	C
	Roof Mounted Solar Panels, subject to the maximum building height for the applicable district	A	A
	Roof Mounted and Freestanding Small Scale Wind Energy System	N	C

Category	Description (Excerpt)		
		GR	B
Garages		A	A
Greenhouse/ Private	A greenhouse for private use	A	N
Storage	Storage Buildings with a gross floor area of greater than 120 square feet	A	A
	Storage Buildings with a gross floor area less than 120 square feet	A	A
	Shipping Containers utilized for storage, must be shielded from view with fencing and/or landscaping and shall require a building permit	N	N
Swimming Pool		A	N
Temporary Structures	Temporary Structures for use of no more than 12 months	C	C
Lot Dimensions	Minimum Lot Size (square feet)	6000 ⁵	0 ⁹
	Minimum Lot Width (feet)	50 ⁶	0 ⁶
Building Height	Maximum Building Height (feet)	35	35
Setbacks	Minimum Front Yard Setback (feet)	20	0 ⁷
	Minimum Side Yard Setback (feet)	10 ⁷	0 ⁷
	Minimum Rear Yard Setback (feet)	10 ⁷	0 ⁷
	Riparian	100 ⁴	100 ⁴
Multi-Family and Mixed Use Density	Mixed Use Residential Density: Maximum units per acre	--	20
	Multi-Family Residential Density: Maximum units per acre	10	20
Total Lot Coverage	Total maximum coverage by all buildings, which includes one accessory dwelling unit (percentage)	40%	--
Maximum Floor Area	Aggregate gross floor area for Individual Retail/Wholesale Trade (000's square feet)	--	36
	Aggregate gross floor area for Grouped Retail/Wholesale (000's square feet)	--	50

Summary of Use Comparison

The applicant is proposing single-family homes for all five lots, which is allowed in the General Residential but not the Business District. While the GR district is primarily established residential uses, not all commercial uses are prohibited. A variety of low-impact business are permitted or conditionally approved in General Residential, such as a Bed and Breakfast, day care facilities, Home Occupations, and several others. While the change from Business (B) to General Residential (GR) is a significant down zone, it will provide the applicant with a path forward to develop the property and provide potential housing.

Summary of Bulk Comparison

The primary difference between the bulk requirements of General Residential and Business zoning is the setbacks. In GR, setbacks are traditional residential setbacks of 20/10/10 (front/side/rear) while the business zone allows for zero setbacks. To accommodate an appropriate separation between the uses, staff recommends a mandatory landscape and fencing buffer between the self-storage units and Lot 15 of at least 8 feet wide and 6 feet in height along the length of the property that abuts Lot 16A. This requirement is in compliance with Section 6A.8.B.3 of the Zoning Ordinance, which address design review requirements when commercial developments are located adjacent to residential zones and uses. If the Commission and Council choose to require this mandatory buffer, staff recommends the developer complete the buffer prior to issuance of any building permits for these five lots.

Standards of Evaluation

14.6 When evaluating any proposed amendment under this Article, the Commission shall make findings of fact (see regular print) on the following criteria (see bold print):

a. The proposed amendment is in accordance with the Comprehensive Plan;

The Comprehensive Plan Land Use Map reflects suitable projected land uses for the City. It considers existing conditions, trends, and desirable future situations, the objective being a balanced mix of land uses for the community. The Land Use Map establishes a basis and direction for the expansion and/or location of business, residential, industrial, institutional and green space areas within and adjacent to the City. The Land Use Map depicts the area proposed for rezone as Traditional Residential but on the border between a Light Industrial/Business Park and Traditional Residential. Goal 5.1 of the Comprehensive Plan describes Traditional Residential areas as, “Density varies depending on the qualities of different neighborhoods, and generally density is higher within a ¼ mile of downtown, community Activity areas or Neighborhood Service Centers and connected by transit service.” The proposed use is compatible with this description, however the proximity to the designated commercial areas must be acknowledged and addressed when considering this zone change request and the Comprehensive Plan supports this zone change.

Based on the foregoing, the proposed amendment is in accordance with Hailey’s Comprehensive Plan.

	COMP PLAN DESIGNATION	ZONING DESIGNATION	LAND USE
Existing	Traditional Residential	Business (B)	Vacant/Building
Proposed	No Change	General Residential (GR)	Single-Family Homes
North of site	Traditional Residential	General Residential (GR)	Single Family Homes
South of site	Light Industrial/Business Park	Business (B)	Commercial businesses and activities
East of site	Traditional Residential	Business (B)	Commercial businesses and activities (Power Engineers)

b. Essential public facilities and services are available to support the full range of proposed uses without creating excessive additional requirements at public cost for the public facilities and services;

It is anticipated that public facilities and services are available and sufficient to support the full range of uses permitted by the GR district.

c. The proposed uses are compatible with the surrounding area; and

Based on the testimony provided on June 9, 2014, the Planning and Zoning Commission found the proposed zone change and accompanying uses are not incompatible with the surrounding area.

d. The proposed amendment will promote the public health, safety and general welfare.

The proposed zone change would be consistent with the Comprehensive Plan’s description of Traditional Residential areas and this area complies with the Land Use Map. The general welfare of neighboring properties will unlikely be negatively impacted, based on the nature of the uses currently in the vicinity and with the allowed used of adjacent zones.

Based on the foregoing, the proposed amendment will promote the public health, safety and general welfare.

14.6.1 When evaluating any proposed Zoning Ordinance Map Amendment to rezone property to Business (B) Zoning District, Limited Business (LB) Zoning District, or Transitional (TN) Zoning District, the Commission and Council shall consider the following:

- a. **Vacancy rates of existing buildings and land within the existing Business (B), Limited Business (LB) or Transitional (TN) Zoning Districts. A lower vacancy rate will favor a rezone, while a higher vacancy rate will not favor a rezone.**

The General Residential (GR) zoning is proposed for the parcels and the properties would be used for single-family homes; therefore, this requirement does not apply.

- b. **The distance of the parcel proposed for rezone from the Central Core Overlay District boundary. A shorter distance from the Central Core Overlay District boundary will favor a rezone, while a longer distance from the Central Core Overlay District boundary will not favor a rezone.**

The General Residential (GR) zoning is proposed for the parcels and the properties would be used for single-family homes; therefore, this requirement does not apply.

CONCLUSIONS OF LAW

Based upon the above Findings of Fact, the Commission makes the following Conclusions of Law:

1. Adequate notice, pursuant to Section 14.4.1 of the Hailey Zoning Ordinance No. 532 and Idaho Code, Section 67-6511, was provided.
2. The applicant has met the burden of establishing the criteria for review set forth in §§14.6(a), (b), and (d).
3. The applicant has failed to meet the burden of establishing the criteria review set forth in §14.6(c).

RECOMMENDATION

The Zoning Map amendment request submitted by Judy A. Castle, represented by Gary D. Slette, for an amendment to the Hailey Zoning Map for Lots 11-15, Block 41, Woodside Subdivision No. 9 to change the current zoning from Business (B) to General Residential (GR) to allow for the construction of single family homes on lots 11-15, Block 41, Woodside Subdivision No. 9 is hereby recommended for denial for the following reason:

1. The Planning and Zoning Commission finds the request to be incompatible with the surrounding neighborhood and uses.

City of Hailey

COMMUNITY DEVELOPMENT DEPARTMENT

115 MAIN STREET SOUTH
HAILEY, IDAHO 83333

Zoning, Subdivision, Building and Business Permitting and Community Planning Services

(208) 788-9815
Fax: (208) 788-2924

July 14, 2014

Chip Maguire
p. 208-720-7996
chip@mode-llc.com
mode-llc.com

Re: Extension of Design Review approval for construction of a new single family residence at 201 N. 3rd Avenue (Lot 11A, Block 47, of the Hailey Townsite)

Dear Mr. Maguire,

On August 12, 2013, you received Design Review approval for construction of a new single family dwelling to be located at 201 N. 3rd Avenue (Lot 11A, Block 47, Hailey Townsite). It is my understanding that you have not commenced construction on this project, however Section 6A.4.F of the Zoning Ordinance allows for one extension of the Design Review approval for a period of six months. This extension can be granted upon receipt of a written request from the applicant.

On June 27, 2014, you submitted a letter requesting extension of your Design Review approval. This letter is attached as Exhibit A. In your letter, you state that an additional six months may be necessary to make all necessary plans to begin construction.

On July 14, 2014, the Planning and Zoning Commission approved your request for extension. Please note that the new expiration date for your Design Review approval is February 8, 2015. The Zoning Ordinance allows for only one extension of a Design Review approval, however if you file a building permit prior to the expiration date, the Design Review approval will be valid through the construction process.

Please let me know if you have any questions.

Micah



Micah Austin, AICP
Community Development Director
City of Hailey
Micah.austin@haileycityhall.org
208-788-9815 ext. 13

FINDINGS OF FACT, CONCLUSIONS OF LAW AND DECISION

On July 14, 2014 the Hailey Planning & Zoning Commission considered a recommendation by the Administrator to exempt from design review an application submitted by the Wood River Valley Hangar Association, represented by Devon Jolley, for minor improvements to the doors and windows of the aircraft located at Friedman Memorial Airport, 2010 Airport Cr. The parcel is within the Airport District (A). The Commission, having been presented with all information and testimony in favor and in opposition to the proposal, hereby makes the following Findings of Fact, Conclusions of Law and Decision.

FINDINGS OF FACT

Summary of Project

The applicant is proposing to add two existing windows and enlarge the existing overhead door. The proposes windows are 36" X 36" for total of 18 square feet of new windows added to the building. The existing door is 10' X 10' and proposed new door will be 14' X 10', adding 40 square feet to the door area. In total, the project involves altering 38 square feet of the building façade. The proposed door will match existing colors and materials of the doors and walls present on site.

Standards of Evaluation

Articles IV and VIA of the Hailey Zoning Ordinance establish the criteria for applications for Zoning and Design Review. For each applicable standard (in bold print), the Commission makes the following Findings of Fact:

6A.1(A). No person shall build, develop, or substantially remodel or alter the exterior of the following Buildings without receiving design review approval pursuant to this Article:

- 1. A building for a non-residential use within any zoning district.**
- 2. A building for a Public or Semi-Public Use within any zoning district.**
- 3. A Multi-Family Dwelling of three or more units within any zoning district.**
- 4. A Single Family Dwelling, Duplex or Accessory Structure within the Townsite Overlay (TO) District.**
- 5. A Historic Structure.**

The hangar unit owned by Wood River Valley Hangar Association is considered a non-residential building. Therefore, Design Review approval is required according to 6A.1(A).

6A.2 Authority of the Administrator.

A. The Administrator has the authority to review and make, or recommend, decisions as follows:

1. The Administrator has the authority to recommend for approval or denial certain applications for Design Review that the Administrator determines to have no substantial impact on adjacent properties or on the community at large, subject to final approval or denial by the Commission on its consent agenda. Such recommendation for approval or denial shall specify the standards used in evaluating the application; the reasons for the approval or denial; and conditions of approval, if any. Applications that have no substantial impact may include, but are not limited to: additions under five-hundred (500) square feet or which are not prominently visible from a public street, façade changes and alterations to parking or other site elements.

2. Those applications for projects of an emergency nature, necessary to guard against imminent peril, regardless of zoning district, shall receive administrative review and approval, denial, or conditional approval, subject to criteria set forth in subsection 6A.7.B of this Article. The Administrator may, upon the request of the applicant or the direction of any City official, forward the application to the Commission for review

3. The Administrator has the authority to recommend exemption of certain projects from the design review requirements, upon finding; the project is minor, will not conflict with the design review standards of this Chapter and will not adversely impact any adjacent properties. Examples include, but are not limited to minor deck additions, changes to siding materials, changes to an existing window or door, an addition of a window or door, and minor landscape changes. Such recommendation for exemption shall be drafted in the form of Findings of Fact and Conclusions of Law, subject to final decision by the Commission on its consent agenda prior to issuance of a building permit. Should the Commission deny the Administrator's recommendation or should the Administrator determine that the proposal does not meet all of the above evaluation criteria, the project shall be subject to the provisions of this chapter prior to issuance of a building permit.

4. The Administrator has the authority to approve minor modifications to projects that have received design review approval by the Commission prior to, and for the duration of a valid Building Permit. The Administrator shall make the determination as to what constitutes minor modifications and may include, but are not limited to changes to approved colors and/or siding materials, changes to site plans that do not significantly increase building footprints or significantly change driveway or road alignment, changes to landscape plans that do not decrease the amount of landscaping, changes to dumpster enclosures, changes to exterior lighting fixtures and location, or changes to windows that do not significantly affect project design, appearance or function. All approved modifications must be documented in a memo to the project file and on the approved set of plans on file with the city. For modifications to design review approval that are

determined by the Administrator not to be minor, the Administrator has the authority to recommend approval or denial of such modifications, subject to final decision by the Commission on its consent agenda. Such recommendation for approval or denial shall be drafted in the form of Findings of Fact and Conclusions of Law.

The Administrator recommends exemption of this project from design review requirements according to 6A.2.A.3, as printed above. This project involves *“changes to an existing window or door, an addition of a window or door.”* In particular, the project does not change the overall architectural theme of the building and will not be prominently visible to the public. The modification involves changing approximately 38 square feet of the building façade.

After reviewing the complete application and proposal, the Administrator finds the proposed alteration:

1. is minor;
2. will not conflict with the design review standards of this Chapter; and
3. will not adversely impact any adjacent properties.

Application

Wood River Valley Hangar Association, represented by Devon Jolley, submitted an application on June 26, 2014 for exemption from the full Design Review process for a minor modification to an existing building.

Procedural History

The application was considered before the P&Z Commission on July 14, 2014 as a Design Review Exemption.

CONCLUSIONS OF LAW AND DECISION

Based upon the above Findings of Fact, the Commission makes the following Conclusions of Law:

1. Adequate notice, pursuant to Zoning Ordinance No. 532, Section 6A.5, was given.
2. The project is in general conformance with the Hailey Comprehensive Plan.
3. The project does not jeopardize the health, safety, or welfare of the public.
4. Given nature of this project, this constitutes a minor project, will not conflict with the design review standards of this Chapter and will not adversely impact any adjacent properties.
5. The addition of the proposed improvements is approved and hereby documented.

Signed this ____ day of _____, 2014.

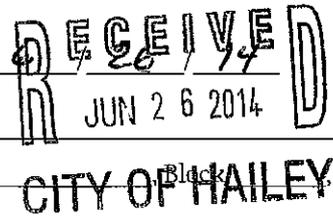
Jay Cone, Chair

Attest:

Kristine Hilt, Community Development Coordinator

City of Hailey - Design Review Application

Submittal Date:



Project Name: West River Valley Hangar Assoc.

Parcel No. RPH _____

Legal Description of Property: Subdivision Airport Hangar E-18

Lot(s) _____

Street Address of Property: 2010 Airport Cr. Hailey, ID 83333

Current Zoning of Property: ~~SPD1200~~ Airport

Year of original construction: _____

(Only applicable if property is within the Townsite Overlay)

Existing building gross sq. ft. (if applicable) _____

Proposed addition or new construction sq. ft. _____

Name of Owner of the Property: Dangel Nest LLC

Mailing Address: PO Box 3269

City: Hailey

State: ID Zip: 83333

Phone: () _____

Fax: () _____

Cell: (208) 720-4981

Email Address: _____

Property Owner Consent:

By signature hereon, the property owner acknowledges that City officials and/or employees may, in the performance of their functions, enter upon the property to inspect, post legal notices, and/or other standard activities in the course of processing this application, pursuant to Idaho Code §67-6507. The property owner is also hereby notified that members of the Planning and Zoning Commission and City Council are required to generally disclose the content of any *ex parte* discussion (outside the hearing) with any person, including the property owner or representative, regarding this application.

Property Owner's Signature:

Date: 06 / 26 / 14

Name of individual to contact on behalf of Trust or LLC (if applicable): Devan Tolley

Mailing Address: PO Box 3269

City: Hailey

State: ID Zip: 83333

Phone: () _____

Fax: () _____

Cell: (208) 720-4981

Email Address: DETOL614@gmail.com

Application Contact (if different than above): Same as above

**Application Contact will be the Planning Department's primary point of contact for questions related to the application.

Mailing Address: _____

City: _____

State: _____

Zip: _____

Phone: () _____

Fax: () _____

Cell: () _____

Email Address: _____

Signature: _____

Date: _____ / _____ / _____

See attached checklist for items that must be submitted with this application in order for application to be considered complete. See the applicable Design Review Guidelines, including Townsite Overlay Guidelines if applicable, in Section 6A of the Hailey Zoning Ordinance.

Appeals: Any interested party may appeal in writing any final decision of the Planning and Zoning Administrator, Hearing Examiner, or Commission to the City Council by filing an appeal with the Hailey City Clerk within fifteen (15) days form the date of the decision. The appeal shall specifically state the decision appealed and reasons for the appeal. If no appeal is filed within the fifteen (15) day period, the decision shall be deemed final.

FOR CITY USE ONLY Fees: Cost of additional noticing, recording fees, and other direct costs will also be assessed.

- Commercial, Mixed-Use or Multi-Family..... \$ 450.00
- plus \$25 / 1,000 gross square feet..... \$ _____
- OR Single-Family Dwelling, Duplex or Accessory Structure in TO..... \$ 250.00
- OR No Substantial Impact..... (Mailing only)..... \$ 75.00
- OR Modification to DR Approval... (No publication or mailing)..... \$ 50.00
- OR DR Exemption... (No publication or mailing)..... \$ 30.00
- Publication cost..... \$ 40.00
- Mailing (# of addresses _____) x (. _____ postage + .15 paper, envelope & label) \$ _____
- DO NOT COUNTY DUPLICATES OR CITY OF HAILEY**
- Total Due..... \$ _____



Add Windows

**Widen Door
(4')**

AMENDED
FINDINGS OF FACT, CONCLUSIONS OF LAW AND DECISION

On February 25, 2014, the Administrator of the Hailey Subdivision Ordinance considered the application for a Lot Line Adjustment submitted by Capstone Development, LLC, represented by Greg Bloomfield, to reconfigure Lots 1-4 within Block 4 in the Hailey Croy Addition (206 West Croy Street), thereby creating Lot 1A, Lot 2A, Lot 3A and Lot 4A comprising 4,884 square feet each. The zoning of the adjusted lots would remain General Residential (GR) and located with the Townsite Overlay (TO). On July 14, 2014, at recommendation by The Administrator, the Hailey Planning and Zoning Commission approved the Amended Preliminary Plat as presented by the applicant. The Final Plat remains unchanged by this amendment. The Administrator, having been presented with all information and testimony in favor and in opposition to the proposal, hereby makes the following Findings of Fact, Conclusions of Law, and Decision.

FINDINGS OF FACT

Notice

Notice of the application was published in the Idaho Mountain Express on February 12, 2014. Notice was mailed to property owners within 300 feet and public agencies on February 12, 2014. The notice informed adjacent property owners they may comment on the application during a period of fifteen (15) days after the post mark of the notice. During this period, no comments were submitted on the lot line adjustment.

Application

Application for Preliminary Plat for a Lot Line Adjustment was submitted by Capstone Development, LLC, represented by Greg Bloomfield, to reconfigure Lots 1-4 within Block 4 in the Hailey Croy Addition (206 West Croy Street), thereby creating Lot 1A, Lot 2A, Lot 3A and Lot 4A comprising 4,884 square feet each on February 6, 2014 and certified complete on February 6, 2014. The properties are located within the General Residential District (GR) and within the Townsite Overlay (TO).

On February 25, 2014, the preliminary plat was approved with conditions and the applicant was informed that a Final Plat could be submitted for signatures and approval.

Procedure

Following expiration of the comment period, and upon a finding by the Administrator that the plat is in compliance with the provisions of the Subdivision Ordinance, the Administrator may approve, or conditionally approve, the Preliminary Plat.

Standards

4.2.5 Bulk Requirements. For other supplementary location and bulk regulations, see Article VII.

a. Minimum Lot Size. GR in Townsite Overlay – four thousand, five hundred (4,500) square feet.

- All four proposed lots are larger than 4,500 square feet as follows:
 - Lot 1A, comprising 4,884 square feet
 - Lot 2A, comprising 4,884 square feet
 - Lot 3A, comprising 4,884 square feet
 - Lot 4A, comprising 4,884 square feet

- b. Minimum Lot Width within GR and TO – 37.5 feet.**
- All four proposed lots have minimum lot widths of 50 feet or more as follows:
 - Lot 1A, with a minimum width of 64.99 feet
 - Lot 2A, with a minimum width of 64.99 feet
 - Lot 3A, with a minimum width of 64.99 feet
 - Lot 4A, with a minimum width of 64.99 feet

CONCLUSIONS OF LAW

Based upon the above Findings of Fact, the Administrator makes the following Conclusions of Law and Decision:

1. Adequate notice for Preliminary Plat of a Lot Line Adjustment, pursuant to Section 6 of the Hailey Subdivision Ordinance, was given.
2. The application conforms to the definition of a Lot Line Adjustment.
3. Upon compliance with the conditions noted below, the application substantially meets the standards of approval set forth in the Hailey Subdivision Ordinance.

DECISION

The application for a lot line adjustment submitted by Capstone Development, LLC, represented by Greg Bloomfield, to reconfigure Lots 1-4 within Block 4 in the Hailey Croy Addition (206 West Croy Street) for approval of a Preliminary Plat to create Lots 1A, 2A, 3A, and 4A of Block 4 of the Hailey Croy Addition is approved by the Administrator with the following conditions:

1. Upon any further development of the new single lot, any applicable provisions of the Hailey Zoning Ordinance or Hailey Subdivision Ordinance in effect on the date of the application for Development shall apply. City infrastructure requirements as outlined in Section 5 of the Hailey Subdivision Ordinance shall be met. Detailed plans for any required infrastructure to be installed or improved at or adjacent to the site shall meet City Standards and shall be submitted for City approval.
2. Issuance of permits for the construction of buildings within the proposed single lot shall be subject to Section 2.9 of the Subdivision Ordinance.
3. The final plat must be submitted within one (1) calendar year from the date of approval of the preliminary plat, unless otherwise allowed for within a phasing agreement.
4. The following plat notes shall be recorded on the final plat:
 1. Lots 1A and 2A shall access from Croy Street only
 2. A 16 foot wide driveway and utility easement to benefit and be maintained by lots 3A and 4A shall be dedicated as shown on the plat.
 3. Water meter vaults for Lots 3A and 4A shall be located adjacent to and within the Croy Street Public Right of Way. Lot owners shall be responsible for the service lines extending behind said vaults.
 4. For all setbacks and building envelopes, the currently adopted City of Hailey zoning ordinance shall govern at the time of building permit submittal.
 5. Owners of Lots 1A, 2A, 3A, and 4A are hereby advised that portions of these lots are located in part, or in their entirety, within a 500-Year FEMA designated floodplain.

Findings approved this 25 day of February, 2014

Micah Austin, Community Development Director

Attest:

Kristine Hilt, Community Development Coordinator

STAFF REPORT

TO: Hailey Planning and Zoning Commission

FROM: Micah Austin, Community Development Director

RE: Consideration of an application for a Text Amendment to the Hailey Zoning Ordinance submitted by Syringa Mountain School to amend Section 5.4 of the Hailey Zoning Ordinance 532, as amended, to allow for a reasonable number of livestock on private and public school properties within Hailey City Limits for educational purposes only, and by amending Section 2.2 of the Hailey Zoning Ordinance 532, as amended, to revised the definition of Urban Agriculture.

HEARING: July 14, 2014

Notice

Notice for the public hearing was published in the Idaho Mountain Express on June 25th, 2014 and mailed to public agencies and area media on June 20th, 2014.

Proposal

The text amendment application was submitted by the Syringa Mountain School (SMS) to allow for a reasonable number of farm animal for educational purposes. Currently, the Zoning Ordinance only permits chickens and bees. Horses are conditionally approved, based on the size of property. The Syringa Mountain Schools proposes to amend Section 5.4 and 7.1 of the Zoning Ordinance to allow for a “reasonable number of livestock.” Specifically, SMS proposes the following language: “Educational institutions located within the Hailey City limits shall be allowed to house a reasonable number of livestock on their properties when used for educational purposes.” The text amendment, if approved, would require amending the definition of Urban Agriculture and/or creating a sub-use category for Urban Agriculture in addition to amending the District Use Matrix to allow for Urban Agriculture with the Business District.

SMS has cited the Comprehensive Plan in the their application, claiming that, “The proposed text amendment appears to be in compliance with Section I of the Comprehensive Plan, Goal 1.2 and 1.4 which addresses the effective use and conserving of resources and promotion of energy conservation as well as Section 14, which addresses school facilities and transportation.” They have also provided research from other municipalities where livestock has been allowed within urban areas.

The public hearing on July 14 is to determine how the Planning and Zoning Commission would like to proceed with this particular text amendment application.

Procedural History

The text amendments were submitted on November 20, 2014, however the applicant requested that the City of Hailey not process the application because the Syringa Mountain

School was still looking for a location for their school. In addition, the application was not certified complete until June 17, 2014. The application on file is complete. The school has now chosen a location in south Hailey and has asked to move forward on their text amendment request at this time. A public hearing will be held on July 14, 2014 with the Hailey Planning and Zoning Commission.

Summary

The Commission shall hold a public hearing and determine whether the proposed amendments are in accordance with the Comprehensive Plan. If the Commission finds the amendments acceptable, Staff will draft an ordinance with the proposed amendments and present the ordinance at the next available Planning and Zoning meeting. At that time, the Commission shall recommend to the Council that the proposed amendment be granted or denied, or that a modified amendment is granted and state the reasons for the recommendation.

Upon approval of an amendment, the Council shall pass an ordinance making the amendment part of Hailey Zoning Ordinance 532. The draft ordinance has not been written and staff is waiting for direction from the Planning and Zoning Commission.



Cultivating intellectually curious, eco-literate, compassionate, self-reliant thinkers, prepared to solve the challenges of our world.

6-19-14

Micah Austin
Community Development Director
City of Hailey
115 Main Street South, Suite H
Hailey, ID 83333

Dear Micah,

Please accept this request for Text Amendments to the sections of City Code that address the keeping of animals and Urban Agriculture: Zoning Ordinance, Articles 5.4 and Article 7.1.

By this letter, Syringa Mountain School is requesting the Urban Agriculture section of the City Code be amended with language that would allow educational institutions, including SMS, located within the Hailey City limits, to house a reasonable number of livestock on their properties when used for educational purposes. Presently, City Code permits two horses per acre, 5 chickens per acre and bee hives on property falling within the city limits. Syringa Mountain School is requesting permission to have 1 sheep, 1 goat, and up to 24 chickens on their leased city property for educational purposes. We are requesting these animals specifically because they are sustainable on the size property presently being leased from the city, and because they are of value to the delivery of our garden, farm and sustainability curriculum that is integral to Waldorf education. SMS would properly house and care for the animals. SMS would be responsible for mitigating any waste product disposal, smells and noise. As part of the biodynamic garden, all waste products will be treated and used for sustaining the garden.

Property Description: This request is predicated on the opening of Syringa Mountain School, September 2, 2014 on property within the city limits of Hailey. The SMS Board holds a four year lease (renewable for up to an additional 8 years and with the option to buy) on a 17,600 square foot building on 4021 Glenbrook Road. Additionally, the SMS Board has entered into a 4 year lease agreement with the City of Hailey for two land parcels adjacent to the school facility for development of community gardens and play spaces.

School Description: Syringa Mountain School is being heralded as the next generation of public schools for a variety of reasons. It is ...

- the first charter school in our county
- the first Waldorf charter school in Idaho
- the first Waldorf charter school in any of the surrounding states of Montana, Washington, Utah, and Wyoming
- the first Idaho public elementary school to incorporate farm, garden and sustainability as an integral part of their science and arts curriculum

Although we usually associate animal husbandry curriculum with vocational high schools and college agriculture programs, animals are an integral education component of Syringa Mountain School. As part of the Waldorf curriculum, children participate in daily garden and animal husbandry lessons as part of their integrated science and arts studies. Students in Waldorf schools not only learn to care for animals,



but they use their products to benefit the biodynamic garden, for crafts such as felting using the animals fur, and for sustainability such as collecting and using eggs from the chickens. In addition the curriculum emphasizes hands-on, real world work experiences so that children develop a deep connection with nature and an understanding of living sustainably and responsibly on the earth.

Other non-profit educational organizations: Syringa Mountain School is partnering with a number of local organizations including the City of Hailey, to not only create demonstration gardens and sustainable play spaces, but to model and share these green practices with our larger community. And Syringa Mountain School represents a larger and growing public interest for educational organizations to have the ability to incorporate sustainability curriculum and practices into students' education. For example, most recently the Nurture Foundation and the newly formed School Food Action Group presented their request to the Blaine County School District Board of Trustees to create gardens on all public school campuses, to purchase locally grown foods as part of the school food service offerings and to include nutrition education as part of the public school curriculum. The Environmental Research Center has for a number of years provided a sustainability curriculum to all third graders attending Blaine County Schools. The Sawtooth Botanical Garden not only hosts field trips to their location but has "traveling" garden curriculum that can be checked out and taught in any school classrooms.

Request: Syringa Mountain School is requesting, as a non-profit education organization, permission to house a reasonable number of livestock on the leased city property adjacent to the school. SMS is specifically seeking permission to house 1 sheep, 1 goat and 24 chickens on the leased city property.

The proposed text amendment appears to be in compliance with Section I of the Comprehensive Plan, Goal 1.2 and 1.4 which addresses the effective use and conserving of resources and promotion of energy conservation as well as Section 14, which addresses school facilities and transportation.

Research: In our initial attempts to research other city ordinances that are "school friendly," we found it is not unusual for cities to make "exceptions" in their ordinances for school organizations such as Future Farmers of America (FFA), 4-H as well as vocational school programs. Although we were unable to find any examples from Idaho, we were able to find some pertinent language from other states as follows:

From City of Gonzales, Texas: All persons keeping the livestock upon any tract of land or lot within the city limits will be held responsible for the cleanliness and upkeep of the land or lot so as not to allow it to become a public nuisance by the odor that may be created by the keeping of the livestock.

Each animal shall have access to a covered, shelter area. The shelter area will be a minimum of fifteen feet by fifteen feet (15"x15") for each animal and shall be constructed in a sturdy, workman-like manner. A minimum of one wall will be attached to the shelter to provide a windbreak for the animal.

From Houston, Texas: They developed a formula based on a minimum of three acres within the city limits So that in the case of horses, one is allowed for the first three acres. One horse is permitted for each acre thereafter; up to a maximum of five. The same formula exists for other animals, including cows, sheep, goats and fowl on at least three acres. The ordinance has an exception for students enrolled in a



4-H Future Farmers project. The students must be a city resident who are members of the immediate household, and cannot have a neighbor within 200 feet of an animal's pen.

From Ferris, Texas: Their chickens within the city limits ordinance includes an exception-High School FFA projects shall be allowed on school property only. And High school projects do not require livestock specific use permits but shall be allowed on school property only.

From San Antonio, Texas: Under the excess animal permit they included a section entitled "The criteria used to evaluate the granting of an excess livestock permit are as follows: All livestock must be enclosed in a pen the nearest point being at least one hundred (100) feet from any dwelling or business building owned or occupied by any person other than the owner. All domestic fowl must be kept in an enclosure. An owner must have adequate facilities to house the livestock and domestic fowl and ensure adequate sanitation. The livestock and domestic fowl must also be kept housed or confined in a manner that does not allow them to create a nuisance. In addition, sanitation must be addressed in a manner that prevents the attraction of pests.

Under the section for Livestock permits, they give additional permission to keep swine within the city limits at vocational schools with properly constructed facilities, and under limited approved and permitted conditions at the home of FFA or 4-H enrollees under the direct supervision of the county agricultural agent. "

In some instances, when the property was zoned agricultural within the city limits, livestock were allowed.

In closing, Syringa Mountain School is requesting the Urban Agriculture section of the City Code, Articles 5.4 and Article 7.1. be amended to allow the keeping of a minimal amount of livestock by educational organizations on properties within the Hailey city limits, when used for educational purposes. This would not only serve to benefit our children and families, but would also pave the way for an "education friendly" city ordinance that supports educating our future citizenry to live sustainably and responsibly.

Thank you for your consideration and assistance with our request.

A handwritten signature in cursive script, appearing to read 'Mary Gervase'.

Representing the Syringa Mountain School Board, and SMS families,

Mary Gervase, PhD
Director, Syringa Mountain School
mgervase@syringamountainschool.org
208-720-6327



www.syringamountainschool.org



Who We Are



Syringa Mountain School is answering the call from Wood River Valley families to provide a unique, rigorous, holistic, Waldorf public school of choice. Slated to open K-5 in August 2014, growing to K-8.

Contact Us

Phone: 208-720-6327

mgervase@syringamountainschool.org

www.syringamountainschool.org

Facebook:

<https://www.facebook.com/SyringaMountainSchool>



Syringa Mountain School
PO Box 3531
Hailey, Idaho 83333

Syringa Mountain School provides a rigorous, arts integrated Waldorf education in the public school setting and develops students who are compassionate, eco-literate, critical thinkers prepared to meet the demands of their world.



Syringa Mountain School

Opening the next generation of public charter schools August 2014 in Hailey, Idaho

Envision a school that...

- ✓ Nurtures the intellect, imagination, and emotional life of each child.
- ✓ Offers a rigorous, relevant, balanced, and memorable liberal arts education.
- ✓ Incorporates sustainable living practices and experiential learning through daily animal husbandry and gardening skills.
- ✓ Infuses academics with singing, painting, drawing, flute, violin, storytelling, handwork, and drama.
- ✓ Touches children's hearts and kindles their imaginations.
- ✓ Inspires children to live engaged and successful lives, prepared to meet the demands of their world



"I believe that Waldorf education possesses unique educational features that have considerable potential for improving public education in America... Waldorf schools provide a program that...not only fosters conventional forms of academic achievement, but also puts a premium on the development of imagination and the refinement of the sensibilities."

— Elliot Eisner, Professor of Education at Stanford University and former President, American Association for Educational Research

Why Waldorf?

Waldorf's unique pedagogical approach differs significantly from traditional education offering a holistic, balanced approach to education which is shown to produce better academic results. Waldorf schooled students have been found to equal or surpass their peers on studied parameters such as math, science, and reading achievement. Equally imperative, are the observed but less easily documented cognitive capacities such as emotional intelligence, social interaction, flexibility, and tenacity that these students exhibit.



What we offer...

Sustainability emphasizes nature and environmental stewardship taught through daily science lessons consisting of observation and experience with the natural environment.

Practical arts or handwork enhance intellectual development, concentration, coordination, perseverance, and imagination through real-life tasks such as housekeeping, cooking, fiber arts, wood working, gardening and animal husbandry.

World languages Spanish immersion begins for students in first grade.

Festivals: Seasonal studies and festivals are taught and celebrated throughout the year bringing rhythm, continuity and community building.

"Waldorf taught me how to think for myself, to be accountable for my actions, to be a good listener, and to be sensitive to the needs of others." —Kenneth Chenault, CEO and Chairman of American Express, Former Waldorf student



Waldorf curriculum is offered in a public school setting. As a public charter school the Idaho State Standards are met using Waldorf teaching methodologies and curriculum.

Age-Appropriate Schooling means content is presented in alignment with age appropriate developmental windows.

Teacher Looping allows students to move through multiple grades together with the same class teacher.

Consistent daily rhythm balances concentration and relaxation, mental and practical work, movement and stillness, listening and participation, observing and doing. Lessons are also taught in blocks.

The Arts such as painting, drawing, drama, singing, and instrumental music are combined with core academics to enrich and enliven learning.

Story and Ethics is used to awaken imagination, build vocabulary, retain attention and teach all subjects.

Imagination and creativity are developed intentionally through Waldorf teaching methods. Technology is de-emphasized in early years. Early introduction to technology and over-exposure to electronic media has been found to hamper the development of the child's imagination, memory, and overall well-being.

Parental Involvement includes attending lectures, class meetings, parent advisories, participating in site work days and school fundraisers. 40 volunteer hours are expected of each family per year.

High-Quality Materials & Aesthetics: Classrooms and instructional materials foster beauty, simplicity: student made main lesson books, natural furniture, and natural materials such as chalk, beeswax crayons, silk, wood, wool.

Hi Micah and P&Z members,

A redefinition of urban agriculture to include a "reasonable number of livestock on private and public school properties within the Hailey city limits for educational purposes only."

What does this mean? I wonder if someone should sit down with the SMS and explain the purpose of zoning "Like land uses being grouped together". One would expect SMS to define their goals and then find land that is zoned to accommodate their uses, not petition to rezone a parcel to fit their needs. Surly SMS knew or should have know that "Urban AG" was not allowed in the Business Zone before they bought or signed a lease for the building. They appear to arrive at an idea, jump in to implement it without due diligence, then present it to the city planners and expect the P&Z to figure out how to make it work.

There are so many questions that pop up - What is reasonable number of livestock? What is defined as livestock? Are horses livestock? How much land is needed per animal? Per type of animal? Does SMS own their land?

Are they leasing it? For how long? What then when their lease runs out?

Who makes this application the lessee or the lessor? And so many more questions.

Thank you,
Judy Harrison

Micah,

According to a 6-26-14 news update from the Mountain Express (http://www.mtexpress.com/vu_breaking_story.php?bid=100018#.U7WeW89OXTY), Syringa Mountain School ("SMS") has proposed a revision to the definition of "Urban Agriculture" to include "'a reasonable number" of livestock on private and public school properties within the Hailey city limits for educational purposes only." The update states that the apparent zoning text amendment application will be heard by P&Z on Jul 14. I could not verify that there is such an item on the agenda since the Jul 14 agenda is not yet available online.

If there is such an application or proposal, please email the application/proposal and any Staff Reports.

I am concerned about a revision to the definition of "Urban Agriculture" since it is a permitted use in all Hailey residential districts. And, ironically, it is not allowed in the Business district, which I believe was the 2009 zoning of the SMS Woodside location. What is current zoning of that location?

Category	Description (Excerpt)	Districts & Corridors											
		RGB	LR-1	LR-2	GR	NB	LB	TN	B	LI	TI	A	SCI-SO
AGRICULTURAL USES													
Agriculture	Horses, a maximum of two (2) horses per acre on lots of one (1) acre minimum size	N	C	C	C	N	N	N	N	N	N	N	N
	Urban Agriculture	N	P	P	P	N	P	P	N	N	N	N	N

My questions and comments include the following:

1. What is Staff's view on the issue? (you likely have already identified the following issues)
2. Would the Use Matrix be amended to permit Urban Agriculture in other Districts (e.g. Business, Light Industrial, etc.)?
3. Would there be a special rule for school properties?
4. What is the definition of "livestock"?
5. "Horses" are currently separately regulated (e.g., they require a CUP in residential districts). Would they be excluded from "livestock"?
6. How would the revision affect and/or interact with the recent School CUP revisions?

7. For example, would such use be considered an expansion of use requiring a CUP?
8. Since the apparent intent is to allow livestock for educational purposes, why shouldn't such use be part of the School CUP process?
9. In other words, if a school needs a CUP for children, why shouldn't it need a CUP for livestock?
10. Perhaps a better solution would be to leave "Urban Agriculture" as it is currently defined and to adopt an "Agricultural Overlay Zone" in appropriate areas after providing notice to the neighbors of such areas.

I look forward to reviewing any materials and to discussing the matter with you.

Thanks!

Happy 4th.

Regards,

Marty

Martin A. Flannes

FLANNES LAW pllc

126 S. Main St.

PO Box 1090

Hailey, Idaho 83333

208.788.1315

208.788.1316 fax

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**DESIGN REVIEW
STAFF REPORT**

TO: Hailey Planning and Zoning Commission

FROM: Micah Austin, Community Development Director

RE: Design Review application by Sun Valley Roasters LLC, represented by Jolyon Sawrey, for Design Review of an addition to an existing commercial building, located at Hailey Townsite, Block 29, Lots 10 (219 South Main Street), within the Business (B) and Townsite Overlay (TO) Zoning Districts.

HEARING: July 14, 2014

Applicant: Sun Valley Roasters LLC, represented by Jolyon Sawrey

Request: Design Review of an addition to an existing commercial building locating within the Hailey Townsite Overlay

Location: Hailey Townsite, Lot 10, Block 29

Zoning: Business (B), and within the Townsite Overlay (TO)

Notice

Notice for the public hearing was published in the Idaho Mountain Express on June 25, 2014 and mailed to property owners within 300 feet on June 25, 2014.

Application

Sun Valley Roasters LLC has submitted a Design Review application for an addition to the existing Hailey Coffee Company building. The addition will house a bakery on the main floor and an Accessory Dwelling Unit (Mixed Use) on the second floor. The addition will be connected to the existing building with a single story hallway. The proposed new construction totaled 2,258 square feet with 1,031 on the main floor and 998 square feet on the second floor. The connector between the buildings comprises 165 square feet and the second floor vestibule comprises 64 square feet. With the new addition, the new building and the old building will comprise 3,597 square feet. The lot is an original Old Hailey Townsite lot comprising 3,600 square feet (30' X 120'). Parking is providing within the public Right-of-Way and with one on-site parking space provided for the second story living space.

Procedural History

On June 19, 2014 the application was submitted to the City of Hailey and certified complete. A public hearing will be held with the Planning and Zoning Commission on July 14, 2014 to consider the design review application.

General Requirements for all Design Review Applications				
Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.5 (B)	Complete Application
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Department and Boards/Commissions Comments	Engineering: -
				Life/Safety: - Police Department - Fire Department
				Water Sewer -
				Building: - Proposed building plans, as presented, seem to be compliance with the 2012 IBC.
				Streets: - The sidewalk adjacent to Walnut will need to be carefully planned so as to take into account the large Siberian Elm. - The Tree Committee will need to address whether the Siberian Elm should stay or go. If it stays, there will be concerns about ADA accessibility and pedestrian access. In addition, there will be a loss of parking spaces. - Siberian Elm: Public Works (PW) met on-site and discussed alternatives to removing the Siberian Elm. While we are open to developing a creative solution to the required installation of sidewalk and parking infrastructure, it is difficult to do so at this particular site. The root system is large, growing and above grade. It would require an alternative material, such as decking, be used around the tree to limit the damage done to the installed infrastructure due to future root growth. This area would extend almost to the building and would eliminate 2-3 parking spaces. Meeting minimum ADA slope requirements, the desire for the project to be designed and installed to minimize maintenance and replacement costs into the future and to adequately remove snow is a challenge at this location if the tree were to remain. - Weighing all the challenges and benefits to keeping the tree, PW is

				<p>ok with removing the tree provided the Tree Committee does not find the tree to be of significant value and so long as replacement tree(s) are installed.</p> <ul style="list-style-type: none"> - The applicant will be responsible for all costs associated with the required right-of-way improvements, including the removal of the tree. - Irrigation should be extended from Main St. and installed and provided to any new trees. - The design and construction of infrastructure shall meet city standards and design per Title 18 of the Hailey Municipal Code. - An ADA parking space shall be installed.
				<p><u>Boards and Commissions</u></p> <p>Tree Committee: The following recommendation was received by the Tree Committee, as submitted by Chair Tom Ward, on July 8, 2014: The Hailey Tree Committee met Monday, July 7th, 2014 to discuss the SV Roasters design review application. A quorum was established with Jeff Beacham, Juerg Stauffacher and Tom Ward in attendance. The applicant and architect were also in attendance.</p> <p>We chose to enter into a round table discussion of the application and the merits for/or against keeping the Siberian Elm. The HTC was unable to find adequate solutions for keeping the Siberian Elm as it relates to this application.</p> <p><i>Therefore, The HTC supports the application to remove the Siberian Elm and develop the property with the following recommendations:</i></p> <ul style="list-style-type: none"> - 1. The four (4) new trees should be any combination of - Linden, Canadian Choke Cherry or Japanese Lilac, in 2.5" to 3" caliper. - 2. Vaults should be considered to allow better root growth. - 3. Edging (metal or other) should be installed around the root zone and covered with compost or mulch to prevent compaction of soils around the trees. - 4. Redesign alignment of tree locations in relation to parking spaces and install vertical curb to protect trees from vehicles. - 5. Install trees to ANSI - 231 standards.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	8.2 Signs	<p>8.2 Signs: The applicant is hereby advised that a sign permit is required for any signage exceeding four square feet in sign area. Approval of signage areas or signage plan in Design Review does not constitute approval of a sign permit.</p>
			Staff Comments	<ul style="list-style-type: none"> - A possible sign location is proposed on the site plan. The sign location and design is not finalized and will be submitted with an application for a sign permit at a later date.

				<ul style="list-style-type: none"> - <i>With no plans for a sign submitted but the intention to install a sign, the Commission may consider a requirement to present the sign permit on the Consent Agenda when that is filed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	9.4 On-site Parking Req.	See Section 9.4 for applicable code.
			Staff Comments	<ul style="list-style-type: none"> - <i>Application is 2,200 square feet of commercial space open to the public, therefore 2 spaces are required</i> - <i>(8) eight parking spaces are provided in the public right of way</i> - <i>Public Right of ways are allowed for parking in the Business District according to Section 4.15 of the Zoning Ordinance</i> - <i>One parking space is required for the Accessory Dwelling Unit and one space has been provided.</i> - <i>According to 9.4.7, one bike parking space is required and has been provided.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	9.4.7 (A) Bicycle Parking Req.	All multi-family residential and commercial development including new construction and additions, must provide at least three (3) bicycle parking spaces or bicycle spaces equivalent to twenty five (25%) of the required number of vehicle parking spaces, whichever is greater
			Staff Comments	<ul style="list-style-type: none"> - <i>With 2 vehicle spaces required, one bike parking space is required.</i> - <i>(4) three bike parking spaces are provided.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	8B.4.1 Outdoor Lighting Standards	8B.4.1 General Standards <ol style="list-style-type: none"> a. All exterior lighting shall be designed, located and lamped in order to prevent: <ol style="list-style-type: none"> 1. Overlighting; 2. Energy waste; 3. Glare; 4. Light Trespass; 5. Skyglow. b. All non-essential exterior commercial and residential lighting is encouraged to be turned off after business hours and/or when not in use. Lights on a timer are encouraged. Sensor activated lights are encouraged to replace existing lighting that is desired for security purposes. c. Canopy lights, such as service station lighting shall be fully recessed or fully shielded so as to ensure that no light source is visible from or causes glare on public rights of way or adjacent properties. d. Area lights. All area lights are encouraged to be eighty-five (85) degree full cut-off type luminaires. e. Idaho Power shall not install any luminaires after the effective date of this Article that lights the public right of way without first receiving approval for any such application by the Lighting Administrator.
			Staff Comments	<ul style="list-style-type: none"> - <i>All new lighting is shown to be compliant with the Outdoor Lighting ordinance</i> - <i>All existing is compliant with the Outdoor lighting ordinance.</i> - <i>Proposed lights are shown on the plans and are compliant with Article 8B</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.7 (A) Required Street Improvements Required	Sidewalks and drainage improvements are required in all zoning districts, except as otherwise provided herein.
			Staff Comments	<ul style="list-style-type: none"> - Sidewalks along Walnut Street are not existing and will need to be installed along the length of the property. - Applicant will install sidewalks and drainage improvement according to City Standards along Walnut Street according to Title 18. See explanation and analysis below.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.7 (B) Required Water System Improvements	In the Townsite Overlay District, any proposal for new construction or addition of a garage accessing from the alley, where water main lines within the alley are less than six (6) feet deep, the developer shall install insulating material (blue board insulation or similar material) for each and every individual water service line and main line between and including the subject property and the nearest public street, as recommended by the City Engineer.
			Staff Comments	N/A
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Bulk Req.	4.13.6 Bulk Requirements.
			Staff Comments	<p style="text-align: center;">a. Minimum Lot Size – Limited Business, Transitional, General Residential districts: 4,500 square feet. Original Townsite Lots that are slightly less than 3,000 square feet (in blocks where lots are 25 feet wide) or 3,600 square feet (in blocks where lots are 30 feet wide) may be rounded up to 3,000 square feet or 3,600 square feet, as the case may be, provided that the original Townsite Lot meets the definition of a Lot of Record.</p> <ul style="list-style-type: none"> - Lot is an original Old Hailey Townsite lot with the following dimensions: 30' X 120' <p style="text-align: center;">b. Minimum Lot Width – Limited Business, Transitional, General Residential districts: 37.5 feet.</p> <ul style="list-style-type: none"> - The lot width is 30', a standard Old Hailey Lot <p style="text-align: center;">c. Maximum Building Height – Business district: 35 feet.</p> <ul style="list-style-type: none"> - Tallest proposed building is 29' 4" <p style="text-align: center;">d. Minimum Setbacks in LR, GR, TN, and LB Districts:</p> <ol style="list-style-type: none"> 1. Setback from any street right-of-way – twelve (12) feet. The following exceptions apply: <p style="text-align: center;">a. Unenclosed features of a residence (e.g. front porches, stoops, and decks without walls) shall be no closer than five (5) feet to the lot</p>

				<p>line. Such features do not include carports.</p> <ul style="list-style-type: none"> . No garage door shall be within twenty (20) feet of any street right-of-way, as measured from the garage door to the street providing access to the garage. <ul style="list-style-type: none"> - N/A <ul style="list-style-type: none"> 2. Setback from any alley right-of-way – six (6) feet. There shall be a six (6) feet horizontal separation between any building and a water meter vault. (Water meter vault and water service line locations may be moved at the property owner’s expense, subject to notification and approval by the City.) <ul style="list-style-type: none"> - Building is setback 13’ to the alley setback. <ul style="list-style-type: none"> 3. Setback from any Idaho Transportation Department/Wood River Trail right-of-way – five (5) feet. The following exception applies: <ul style="list-style-type: none"> a. No garage door shall be within twenty (20) feet of the right-of-way, as measured from the garage door to the right-of-way, where this right-of-way provides access to the garage. <ul style="list-style-type: none"> - N/A <ul style="list-style-type: none"> 4. Setback from property lines abutting other private property -- <ul style="list-style-type: none"> a. Base setback – fifteen percent (15%) of lot width, or 10 feet, whichever is less. However, no such setback shall be less than six (6) feet. (See Table 1) <ul style="list-style-type: none"> - Zero setbacks are allowed in the Business District. - Building is setback 2’ from the north property line with a 9” encroachment on the existing building. The applicant is not altering the existing building and has a current agreement with the property owner concerning the encroachment - Building is located on the property line on the south side, facing Walnut.
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<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.010 Street Classifications, Types, and Designations <i>Staff Comments</i> - Walnut is classified Business Collector, therefore all standards for a Business Collector apply
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.012 Street Design and Guideline Standards <i>Staff Comments</i> - All access drives and sidewalks that connect to roads and sidewalks off-site shall be completed according to the requirements of 18.06.012 (C)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.016 Traffic Calming <i>Staff Comments</i> - A current bulb out exists and will be improved with curb and gutter extending to the alley. -
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.022 Pedestrian Facility Design Guidelines and Standards <i>Staff Comments</i> - Total sidewalk area is 16' with the following: <ul style="list-style-type: none"> o A 10' sidewalk is proposed o 4' furnishings and landscape zone o 2' curb and gutter cross section area o Proposed trees have not been determined by the Tree Committee at this time.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.024 Bicycle Facility Design Guidelines and Standards <i>Staff Comments</i> - Four bike parking spaces are provided. The applicant is proposing the "lollipop" style as approved in the Mobility Design Ordinance.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.026 Street Tree Guidelines and Standards <i>Staff Comments</i> - Four new street trees are proposed and recommended per the Street Superintendent - One existing tree will remain. It is a Siberian Elm and the applicant requests that it remain for providing shade the existing building and business. -
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.028 Streetscape Elements Guidelines and Standards <i>Staff Comments</i> - Tree grates will be provided for the two new trees. The existing is too large for traditional tree grates. Grates will be built according to City Standards. - Open seating will be provided on the sidewalk, however the owner prefers this reserved for customers but acknowledges it is public right of way.

Design Review Requirements for Non-Residential, Multifamily, and/or Mixed Use Buildings within the City of Hailey

1. Site Planning: 6A.8 (A) 1, items (a) thru (n)

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1a	<p>a. The location, orientation and surface of buildings shall maximize, to the greatest extent possible sun exposure in exterior spaces to create spaces around buildings that are usable by the residents and allow for safe access to buildings</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - The building is predominantly south facing with an overhead garage door provided for an indoor/outdoor dining area in the summer. - A large overhead awning is provided to provide mid-day shade and minimize cooling requirements. - On the second story, the south facing windows on the dwelling provide natural light. - On the second story, the angle of the roof has been designed to accommodate roof mounted solar panels at a future date. - Access stairs to the second floor have been located on the south/west side to minimize ice.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1b	<p>a. All existing plant material shall be inventoried and delineated, to scale, and noted whether it is to be preserved, relocated or removed. Removal of trees larger than 6 inch caliper proposed to be removed require an arborist review. Any tree destroyed or mortally injured after previously being identified to be preserved, or removed without authorization, shall be replaced with a species of tree found in the Tree Guide and shall be a minimum of 4 inch caliper.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - A large, mature Siberian Elm is proposed for removal to accommodate an ADA compliant sidewalk.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1c	<p>Site circulation shall be designed so pedestrians have safe access to and through the site and to building.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - Entire frontage of building is located along the public right of way of Walnut Street, providing excellent access and visibility
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1d	<p>Building services including loading areas, trash storage/pickup areas and utility boxes shall be located at the rear of a building; the side of the building adjacent to an internal lot line may be considered as an alternate location. These areas shall be designed in a manner to minimize conflict among uses and shall not interfere with other uses, such as snow storage. These areas shall be screened with landscaping, enclosures, fencing or by the principal building.</p>

			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Trash will be located in the alley so that the garbage trucks will access the trash at an angle.</i> - <i>Clear Creek Disposal has reviewed the plans and approved. See letter from Mike Goitiandia.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)1e	a. Where alleys exist, or are planned, they shall be utilized for building services.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Alley will be used for trash collection and access to the recycling areas.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)1f	a. Vending machines located on the exterior of a building shall not be visible from any street.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>No vending machines are proposed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1g	a. On-site parking areas shall be located at the rear of the building and screened from the street. Parking and access shall not be obstructed by snow accumulation. (NOTE: If project is located in Airport West Subdivision, certain standards may apply that are not listed here. See code for details.)
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>One parking space is located at the rear of the building to accommodate the residential space on the second floor.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1h	a. Access to on-site parking shall be from the alley or, if the site is not serviced by an alley, from a single approach to the street to confine vehicular/pedestrian conflict to limited locations, allow more buffering of the parking area and preserve the street frontage for pedestrian traffic.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The single on-site parking space is accessed from the alley and meets minimum standards for a 90' parking stall</i> - <i>The parking space is 18' deep and 9' wide, meeting minimum standards.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1i	a. Snow storage areas shall be provided on-site where practical and sited in a manner that is accessible to all types of snow removal vehicles of a size that can accommodate moderate areas of snow.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Snow storage for the trash, recycling, and on-site parking has been provide adjacent to the alley.</i> - <i>A total impermeable surface required for snow removal is 306 square feet, therefore the total area required is 76.5 square feet.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1j	a. Snow storage areas shall not be less than 25% of the improved parking and vehicle and pedestrian circulation areas.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>A total impermeable surface required for snow removal is 306 square feet, therefore the total area required is 76.5 square feet.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1k	a. A designated snow storage area shall not have any dimension less than 10 feet.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Existing snow storage areas are at least 10 feet in any dimension.</i> - <i>Snow storage area is 13 feet wide by 6 feet wide.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1l	l. Hauling of snow from downtown areas is permissible where other options are not practical.

			<i>Staff Comments</i>	- All snow will be stored and retained on site.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1m	m. Snow storage areas shall not impede parking spaces, vehicular and pedestrian circulation or line of sight, loading areas, trash storage/pickup areas, service areas or utilities.
			<i>Staff Comments</i>	- Snow storage is shown on the plan as hatched areas and does not impede pedestrian access or circulation. - Snow storage will be located at the rear of the lot.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)1n	n. Snow storage areas shall be landscaped with vegetation that is salt-tolerant and resilient to heavy snow.
			<i>Staff Comments</i>	- Existing snow storage areas are located at non-landscaped area of the site.
2. Building Design: 6A.8 (A) 2, items (a) thru (m)				
Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2a	a. The proportion, size, shape and rooflines of new buildings shall be compatible with surrounding buildings.
			<i>Staff Comments</i>	- The proportion of the building is consistent with the commercial themes of the surrounding neighborhood while retaining both commercial and residential themes.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2b	Standardized corporate building designs are prohibited.
			<i>Staff Comments</i>	- The elevations are not standard corporate designs.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2c	At ground level, building design shall emphasize human scale, be pedestrian oriented and encourage human activity and interaction.
			<i>Staff Comments</i>	- An entry alcove has been added to encourage and emphasize a pedestrian entrance - The awning has also been added to emphasize human scale of the building and provide shade.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2d	The front façade of buildings shall face the street and may include design features such as windows, pedestrian entrances, building off-sets, projections, architectural detailing, courtyards and change in materials or similar features to create human scale and break up large building surfaces and volumes.
			<i>Staff Comments</i>	- All windows of the building are at a pedestrian level to encourage display of merchandise and goods at a street level - Pedestrian level wainscot of 5' has been added to protect the building with the area of the building most accessible to people. - A dog watering area has been added to emphasize "canine scale"

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2e	Any addition onto or renovation of an existing building shall be designed to create a cohesive whole.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The roof forms of the existing building and the architectural detail of the roof has been added to the gable end of the new addition.</i> - <i>The roof pitches have also been mimicked on the new building to match and be consistent with the existing building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2f	All exterior walls of a building shall incorporate the use of varying materials, textures and colors.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Exterior materials include a rusted corrugated metal for roof and wainscot</i> - <i>Front entry is composed of a painted fiber-cement board</i> - <i>Overhead door is anodized aluminum with a dark bronze finish.</i> - <i>Windows do not have any tinting but are metal clad</i> - <i>Stucco will be a dark gray and accent the second story and portions of the first story.</i> -
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2g	Exterior buildings colors and materials shall be integrated appropriately into the architecture of the building and be harmonious within the project and with surrounding buildings.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The proposed exterior materials are shown on the exterior materials sheet and have been selected to be harmonious with the other buildings in the downtown area.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2h	Flat-roofed buildings over two stories in height shall incorporate roof elements such as parapets, upper decks, balconies or other design elements.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>A flat roof connector is proposed between the existing building and the new building.</i> - <i>Flat roof is not over two stories and is 12' 6" high</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2i	<p>All buildings shall minimize energy consumption by utilizing alternative energy sources and/or passive solar techniques. At least three (3) of the following techniques, or an approved alternative, shall be used to improve energy cost savings and provide a more comfortable and healthy living space:</p> <ul style="list-style-type: none"> i) Solar Orientation. If there is a longer wall plane, it shall be placed on an east-west axis. A building's wall plane shall be oriented within 30 degrees of true south. ii) South facing windows with eave coverage. At least 40% of the building's total glazing surface shall be oriented to the south, with roof overhang or awning coverage at the south. iii) Double glazed windows. iv) Windows with Low Emissivity glazing. v) Earth berming against exterior walls vi) Alternative energy. Solar energy for electricity or water heating, wind energy or another approved alternative shall be installed on-site. vii) Exterior light shelves. All windows on the southern most facing side of the building shall have external light shelves installed.
			<i>Staff</i>	The applicant has indicated they are pursuing the following techniques to

			<i>Comments</i>	achieve optimal energy efficiency. Staff has reviewed and finds them acceptable: 1. South facing wall pane is the longest wall plane of the building and has been oriented to maximize solar gain. 2. More than 60% of total windows are south facing. 3. All windows proposed are double glazed windows. 4. Low e double paned windows are proposed.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2j	Gabled coverings, appropriate roof pitch, or snow clips and/or gutters and downspouts shall be provided over all walkways and entries to prevent snow from falling directly onto adjacent sidewalks.
			<i>Staff Comments</i>	- <i>Sloped roofs all have snow clips and the second story roof has a gutter and down spout that runs to the dry well.</i> - <i>Recessed entry protects the main entrance from snow and rain</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2k	Downspouts and drains shall be located within landscape areas or other appropriate locations where freezing will not create pedestrian hazards.
			<i>Staff Comments</i>	- <i>Based on roof configurations, downspouts are oriented to flow to the dry wells.</i> - <i>All downspouts will be equipped with heat tape.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)2l	Vehicle canopies associated with gas stations, convenience stores or drive-through facilities shall have a minimum roof pitch of 3/12 and be consistent with the colors, material and architectural design used on the principal building(s).
			<i>Staff Comments</i>	- <i>N/A</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)2m	a. A master plan for signage is required to ensure the design and location of signs is compatible with the building design and compliance with Article 8.
			<i>Staff Comments</i>	- <i>A possible sign location is proposed on the floor plan.</i> - <i>The sign location and design is not finalized</i> - <i>Applicant proposes to submit a sign permit to the Commission at a later date for approval.</i>

3. Accessory Structures, Fences and Equipment/Utilities: 6A.8 (A) 3, items (a) thru (i)

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3a	Accessory structures shall be designed to be compatible with the principal building(s).
			<i>Staff Comments</i>	- <i>No accessory structures are proposed</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3b	Accessory structures shall be located at the rear of the property.
			<i>Staff Comments</i>	- <i>N/A</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3c	Walls and fences shall be constructed of materials compatible with other materials used on the site.
			<i>Staff Comments</i>	- <i>The proposed fence on the west side of the project is made of corrugated metal to match the new building and wainscot.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3d	Walls and fencing shall not dominate the buildings or the landscape. Planting should be integrated with fencing in order to soften the visual impact.
			<i>Staff Comments</i>	- <i>Proposed fencing is located at the rear of the building along the alley and is not a dominate feature of the landscape.</i> - <i>Corrugated metal will be used for the fence, to blend the fence with the building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3e	All roof projections including, roof-mounted mechanical equipment, such as heating and air conditioning units, but excluding solar panels and Wind Energy Systems that have received a Conditional Use Permit, shall be shielded and screened from view from the ground level of on-site parking areas, adjacent public streets and adjacent properties.
			<i>Staff Comments</i>	- <i>Other than solar panels for the south side, no roof projections or roof mounted mechanical equipment are proposed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3f	The hardware associated with alternative energy sources shall be incorporated into the building's design and not detract from the building and its surroundings.
			<i>Staff Comments</i>	- <i>Hardware for solar panels will be located beneath the panel and not visible from the street.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3g	All ground-mounted mechanical equipment, including heating and air conditioning units, and trash receptacle areas shall be adequately screened from surrounding properties and streets by the use of a wall, fence, or landscaping, or shall be enclosed within a building.
			<i>Staff Comments</i>	- <i>No ground mounted equipment is proposed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)3h	All service lines into the subject property shall be installed underground.
			<i>Staff Comments</i>	- <i>All utilities will remain underground.</i> - <i>The existing overhead power line will be undergrounded to accommodate the new building.</i> - <i>There are no plans to bring any utilities above ground.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)3i	Additional appurtenances shall not be located on existing utility poles.
			<i>Staff Comments</i>	N/A

4. Landscaping: 6A.8 (A) 4, items (a) thru (n)

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4a	Only drought tolerant plant species and/or xeriscape specific plant materials shall be used, as specified by the Hailey Landscaping Manual or an approved alternative.

			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The only landscaping associated with project will be the street trees.</i> - <i>The private property has no proposed landscaping.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4b	a. All plant species shall be hardy to the Zone 4 environment.
			<i>Staff Comments</i>	- <i>All plants on site will be hardy to zone 4.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4c	a. At a minimum, a temporary irrigation system that fully operates for at least two complete growing seasons is required in order to establish drought tolerant plant species and/or xeriscape specific plant materials. Features that minimize water use, such as moisture sensors, are encouraged.
			<i>Staff Comments</i>	- <i>Street trees will be irrigated from the City water system.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4d	Landscaped areas shall be planned as an integral part of the site with consideration of the urban environment. A combination of trees shrubs, vines, ground covers and ornamental grasses shall be used. New landscaped areas having more than 10 trees, a minimum of 10% of the trees shall be at least 4-inch caliper, 20% shall be at least 3-inch caliper, and 20% shall be at least 2½ inch caliper and a maximum of 20% of any single tree species may be used in any landscape plan (excluding street trees). New planting areas shall be designed to accommodate typical trees at maturity. Buildings within the LI and SCI-I zoning district are excluded from this standard.
			<i>Staff Comments</i>	- <i>N/A</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4e	Seasonal plantings in planter boxes, pots, and/or hanging baskets shall be provided to add color and interest to the outside of buildings in the LI and SCI-I zoning districts.
			<i>Staff Comments</i>	- <i>The applicant will be utilizing planter boxes to enhance interest and bring color to the site during the summer months.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4f	Plantings for pedestrian areas within the B, LB, TN and SCI-O zoning districts shall be designed with attention to the details of color, texture and form. A variety of trees, shrubs, perennials, ground covers and seasonal plantings, with different shapes and distinctive foliage, bark and flowers shall be used in beds, planter boxes, pots, and/or hanging baskets.
			<i>Staff Comments</i>	- <i>A flower pot is proposed for the entryway to the bakery and new building along the south side.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8(A)4g	Storm water runoff should be retained on the site wherever possible and used to irrigate plant materials.
			<i>Staff Comments</i>	- <i>All drains and downspout drain to the proposed drywell.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4h	A plan for maintenance of the landscaping areas is required to ensure that the project appears in a well maintained condition (i.e., all weeds and trash removed, dead plant materials removed and replaced).
			<i>Staff Comments</i>	- <i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4i	Retaining walls shall be designed to minimize their impact on the site and the appearance of the site.

			<i>Staff Comments</i>	- <i>No retaining walls are proposed.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4j	Retaining walls shall be constructed of materials that are utilized elsewhere on the site, or of natural or decorative materials.
			<i>Staff Comments</i>	- <i>No retaining walls are proposed.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4k	Retaining walls, where visible to the public and/or to residents or employees of the project, shall be no higher than four feet or terraced with a three foot horizontal separation of walls.
			<i>Staff Comments</i>	- <i>No retaining walls are proposed.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4l	Landscaping should be provided within or in front of extensive retaining walls.
			<i>Staff Comments</i>	- <i>No retaining walls are proposed.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4m	Retaining walls over 24" high may require railings or planting buffers for safety.
			<i>Staff Comments</i>	- <i>No retaining walls are proposed.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8(A)4n	Low retaining walls may be used for seating if capped with a surface of at least 12 to 16 inches wide.
			<i>Staff Comments</i>	- <i>No retaining walls are proposed.</i>

Design Review Requirements for Non-Residential Buildings located in B, LB, or TN Zoning Districts (6.A.8.B. 1-3)				
1. Site Planning: 6A.8.B.1				
Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.B.1.a	The site shall support pedestrian circulation and provide pedestrian amenities. Sidewalks shall be provided along building fronts.
			<i>Staff Comments</i>	- <i>Proposed sidewalk area is 16' wide, according to the Mobility Design Ordinance.</i> - <i>The sidewalk zone is compliant with Title 18</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.B.1.b	Wider sidewalks are encouraged to provide additional amenities such as seating areas and bicycle racks underground utilities for new dwelling units.
			<i>Staff Comments</i>	- <i>In total, 16' sidewalks are proposed with areas for furnishing, street trees, curb, and gutter.</i>
2. Building Design: 6A.8.B.2.a-9				
Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.B.2.a	a. The main facade shall be oriented to the street. The main entrance(s) to the building

				<p>shall be located on the street side of the building. If the building is located on a corner, entrances shall be provided on both street frontages. If the design includes a courtyard, the main entrance may be located through the courtyard. Buildings with more than one retail space on the ground floor are encouraged to have separate entrances for each unit.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The main façade of both the new building and the existing building face Walnut Street.</i> - <i>Pedestrian entrances are emphasized with awnings and recessed entries.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.B.2.b	<p>Multi-unit structures shall emphasize the individuality of units or provide visual interest by variations in roof lines or walls or other human scale elements. Breaking the facades and roofs of buildings softens the institutional image which may often accompany large buildings.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The separate businesses have been emphasized with a flat roof separating the building forms.</i> - <i>The dwelling unit on the second floor has been emphasized with a sloped roof, consistent with residential designs.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.B.2.c	<p>Building designs shall maximize the human scale of buildings and enhance the small town “sense of place”. This can be achieved by utilizing voids and masses, as well as details, textures, and colors on building facades. Human scale can also be achieved by incorporating structural elements such as colonnades and covered walkways, overhangs, canopies, entries, and landscaping. Particular attention should be paid to creating interest at the street level.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Many features have been added emphasize the Hailey “Sense of Place”.</i> - <i>The main façade of both the new building and the existing building face Walnut Street.</i> - <i>Pedestrian entrances are emphasized with awnings and recessed entries.</i> - <i>The separate businesses have been emphasized with a flat roof separating the building forms.</i> - <i>The dwelling unit on the second floor has been emphasized with a sloped roof, consistent with residential designs.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.B.2.d	<p>d. Buildings that exceed 30 feet in height, the entire roof surface shall not project to the highest point of the roof. The Commission shall review building height relative to the other dimensions of width and depth combined with detailing of parapets, cornices, roof, and other architectural elements.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Proposed new building is 29’ 4”</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.e	<p>Livable outdoor spaces in multi-story buildings that create pleasing elements and reduce the mass of taller buildings are encouraged.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>N/A</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.2.f	<p>Fire department staging areas shall be incorporated into the design elements of the building</p>

			<i>Staff Comments</i>	- N/A
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.8.B.2.g	<p>New buildings adjacent to residential areas shall be designed to ensure that building massing and scale provide a transition to adjoining residential neighborhoods. Possible mitigation techniques include, but are not limited to the following:</p> <ul style="list-style-type: none"> i) Locating open space and preserving existing vegetation on the edge of the site to further separate the building from less intensive uses; ii) Stepping down the massing of the building along the site's edge; and iii) Limiting the length of or articulating building facades to reflect adjacent residential patterns
			<i>Staff Comments</i>	- <i>Proposed buildings are not adjacent to residential areas.</i>

4. Landscaping: 6A.8.B.3.a

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.8.B.3.a	<p>a. When abutting the LR, GR or TN zoning districts, a landscape buffer between the project and the residential property shall be provided. The buffer shall be at least eight foot wide to create a year-round visual screen of at least 6 feet in height. The buffer shall be designed to avoid the appearance of a straight line or wall of uniform plant material and shall be wide enough to accommodate the planted species when mature.</p>
			<i>Staff Comments</i>	- <i>Project does not abut LR, GR, or TN zones.</i> -

**Design Review Guidelines for Residential and Non-residential Buildings
 in the Townsite Overlay District (TO): 6A.9**

1. Site Planning: 6A.9.C.1

Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.1	<p>The pattern created by the Old Hailey town grid should be respected in all site planning decisions.</p>
			<i>Staff Comments</i>	- <i>The zero lot line development is consistent with Hailey town grid layout.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.1	<p>Site planning for new development and redevelopment shall address the following:</p> <ul style="list-style-type: none"> • scale and massing of new buildings consistent with the surrounding neighborhood; • building orientation that respects the established grid pattern of Old Hailey; • clearly visible front entrances; • use of alleys as the preferred access for secondary uses and automobile access; • adequate storage for recreational vehicles;

				<ul style="list-style-type: none"> • yards and open spaces; • solar access on the site and on adjacent properties where feasible, and where such decisions do not conflict with other Design Guidelines; • snow storage appropriate for the property; • underground utilities for new dwelling units.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The proposed project meets the above standards.</i> - <i>Building uses some architectural features in the roof form used in mining areas and common to Hailey's historical structures.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.1	The use of energy-conserving designs that are compatible with the character of Old Hailey are encouraged. The visual impacts of passive and active solar designs should be balanced with other visual concerns outlined in these Design Guidelines.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The proposed design meets the standards for energy conserving designs</i>

2. Bulk Requirements (Mass and Scale, Height, Setbacks): 6A.9.C.2

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.2	The perceived mass of larger buildings shall be diminished by the design.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>No concerns.</i>

3. Architectural Character: 6A.9.C.3

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.a	<p>General: New buildings should be respectful of the past, but may offer new interpretations of old styles, such that they are seen as reflecting the era in which they are built.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The proposed building utilizes old mining styles founding Old Hailey along with elements of the existing on-site building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.b	<p>Building Orientation: The front entry of the primary structure shall be clearly identified such that it is visible and inviting from the street.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The front entryway for both businesses prominently faces Walnut Street and is clearly visible.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.b	<p>Building Orientation: Buildings shall be oriented to respect the existing grid pattern. Aligning the front wall plane to the street is generally the preferred building orientation.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Building has been built according to the current grid pattern</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.c	<p>Building Form: The use of building forms traditionally found in Old Hailey is encouraged. Forms that help to reduce the perceived scale of buildings shall be incorporated into the design.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Materials used for the building consist of corrugated metal, sloped roof forms, wainscoting, and mining-era inspired roof form. These materials and roof forms are compatible with the character of surrounding buildings and found throughout Old Hailey</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.d	Roof Form: Roof forms shall define the entry to the building, breaking up the perceived mass of larger buildings, and to diminish garages where applicable.
			<i>Staff Comments</i>	- <i>Awnings and a recessed entry define the entry to the new building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.d	Roof Form: Roof pitch and style shall be designed to meet snow storage needs for the site. <ul style="list-style-type: none"> • Roof pitch materials and style shall retain snow on the roof, or allow snow to shed safely onto the property, and away from pedestrian travel areas. • Designs should avoid locating drip lines over key pedestrian routes. • Where setbacks are less than ten feet, special attention shall be given to the roof form to ensure that snow does not shed onto adjacent properties.
			<i>Staff Comments</i>	- <i>Roof pitch is 12/12 on the front and 3/12 on the back</i> - <i>Snow clips have been added to accommodate safe pedestrian access to the building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.d	Roof Form: The use of roof forms, roof pitch, ridge length and roof materials that are similar to those traditionally found in the neighborhood are encouraged.
			<i>Staff Comments</i>	- <i>The application is consistent with the neighborhood in regards to roof forms and materials</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.d	Roof Form: The roof pitch of a new building should be compatible with those found traditionally in the surrounding neighborhood.
			<i>Staff Comments</i>	- <i>The application is consistent with the neighborhood in regards to roof forms.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.e	Wall Planes: Primary wall planes should be parallel to the front lot line.
			<i>Staff Comments</i>	- <i>Proposed building walls are shown parallel to the adjacent property lines.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.e	Wall Planes: Wall planes shall be proportional to the site, and shall respect the scale of the surrounding neighborhood.
			<i>Staff Comments</i>	- <i>The proposed buildings are single story and respect the scale of the surrounding.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.e	Wall Planes: The use of pop-outs to break up longer wall planes is encouraged.
			<i>Staff Comments</i>	- <i>No pop-outs are used or proposed but windows, and entry awning, and a recessed entry breaks up the building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.f	Windows: Windows facing streets are encouraged to be of a traditional size, scale and proportion.
			<i>Staff Comments</i>	- <i>Windows facing the street are taller than they are wide, consistent with Old Hailey.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.f	Windows: Windows on side lot lines adjacent to other buildings should be carefully planned to respect the privacy of neighbors.
			<i>Staff Comments</i>	- <i>No windows are proposed facing the north.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.3.g	Decks and Balconies: Decks and balconies shall be in scale with the building and the neighborhood.
			<i>Staff Comments</i>	- <i>No decks or balconies are proposed.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.3.g	Decks and Balconies: Decks and balconies should be designed with the privacy of neighbors in mind when possible.
			<i>Staff Comments</i>	- <i>No decks or balconies proposed</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.h	Building Materials and Finishes: Materials and colors shall be selected to avoid the look of large, flat walls. The use of texture and detailing to reduce the perceived scale of large

				walls is encouraged. Building Materials and Finishes: Large wall planes shall incorporate more than one material or color to break up the mass of the wall plane.
			<i>Staff Comments</i>	- No large wall planes are proposed. - The exterior materials sheet calls out the proposed materials and colors that will provide visual interest. Materials include corrugated metal, stucco, and wood.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.h	Building Materials and Finishes: Large wall planes shall incorporate more than one material or color to break up the mass of the wall plane.
			<i>Staff Comments</i>	- No large wall planes are proposed.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.i	Ornamentation and Architectural Detailing: Architectural detailing shall be incorporated into the front wall plane of buildings.
			<i>Staff Comments</i>	- Materials used for the building consist of corrugated metal, sloped roof forms, wainscoting, and mining-era inspired roof form. These materials and roof forms are compatible with the character of surrounding buildings and found throughout Old Hailey
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.i	Ornamentation and Architectural Detailing: The use of porches, windows, stoops, shutters, trim detailing and other ornamentation that is reminiscent of the historic nature of Old Hailey is encouraged.
			<i>Staff Comments</i>	- No porch is proposed.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.3.i	Ornamentation and Architectural Detailing: Architectural details and ornamentation on buildings should be compatible with the scale and pattern of the neighborhood.
			<i>Staff Comments</i>	- N/A

4. Circulation and Parking: 6A.9.C.4

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.4	Guideline: Safety for pedestrians shall be given high priority in site planning, particularly with respect to parking, vehicular circulation and snow storage issues.
			<i>Staff Comments</i>	- Safe pedestrian routes have been provided from Main Street and the parking spaces including the ADA parking spaces.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.4	Guideline: The visual impacts of on-site parking visible from the street shall be minimized.
			<i>Staff Comments</i>	- A single on-site parking space is proposed to accommodate the ADU on the second floor. This is screened with fencing.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.4	Guideline: As a general rule, garages and parking should be accessed from the alley side of the property and not the street side.
			<i>Staff Comments</i>	On-site parking is accessed from the alley
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.4	Guideline: Detached garages accessed from alleys are strongly encouraged.
			<i>Staff Comments</i>	N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.4	Guideline: When garages must be planned on the street side, garage doors shall be set back and remain subordinate to the front wall plane.
			<i>Staff Comments</i>	N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.4	Guideline: When garages and/or parking must be planned on the street side, parking areas are preferred to be one car in width. When curb cuts must be planned, they should

				be shared or minimized.
			<i>Staff Comments</i>	N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.4	Guideline: Off-street parking space for recreational vehicles should be developed as part of the overall site planning.
			<i>Staff Comments</i>	N/A

5. Alleys: 6A.9.C.5

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.5	Guideline: Alleys shall be retained in site planning. Lot lines generally shall not be modified in ways that eliminate alley access to properties.
			<i>Staff Comments</i>	N/A
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.5	Guideline: Alleys are the preferred location for utilities, vehicular access to garages, storage areas (including recreational vehicles) and accessory buildings. Design and placement of accessory buildings that access off of alleys is encouraged.
			<i>Staff Comments</i>	- All utilities are existing and are shown on the site utilities plan.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.5	Guideline: Generally, the driving surface of alleys within Limited Residential and General Residential may remain a dust-free gravel surface, but should be paved within Business, Limited Business, and Transitional. The remainder of the City alley should be managed for noxious weed control, particularly after construction activity.
			<i>Staff Comments</i>	- All access roads are existing and are paved. The applicant is required to improve their frontage on Walnut Street with on-street parking.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.5	Guideline: Landscaping and other design elements adjacent to alleys should be kept simple, and respect the functional nature of the area and the pedestrian activity that occurs.
			<i>Staff Comments</i>	- N/A

6. Accessory Structures: 6A.9.C.6

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.6	Guideline: Accessory buildings shall appear subordinate to the main building on the property in terms of size, location and function.
			<i>Staff Comments</i>	- N/A
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.6	Guideline: In general, accessory structures shall be located to the rear of the lot and off of the alley unless found to be impractical.
			<i>Staff Comments</i>	- N/A

7. Snow Storage: 6A.9.C.7

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.7	Guideline: All projects shall be required to provide 25% snow storage on the site.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - Site plan shows existing snow storage locations and calculations for required snow storage areas. - A total 389 square feet of impermeable surfaces require snow

				<p><i>storage.</i></p> <ul style="list-style-type: none"> - 97.25 square feet of storage is required - 98 square feet has been provided.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.7	<p>Guideline: A snow storage plan shall be developed for every project showing:</p> <ul style="list-style-type: none"> • Where snow is stored, key pedestrian routes and clear vision triangles. • Consideration given to the impacts on adjacent properties when planning snow storage areas.
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - Snow storage areas do not restrict pedestrian access.

8. Existing Mature Trees and Landscaping: 6A.9.C.8

Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.8	<p>Guideline: Existing mature trees shall be shown on the site plan, with notations regarding retention, removal or relocation. Unless shown to be infeasible, a site shall be carefully planned to incorporate existing mature trees on private property into the final design plan.</p>
			<i>Staff Comments</i>	<p>The following recommendation was received by the Tree Committee, as submitted by Chair Tom Ward, on July 8, 2014:</p> <p>The Hailey Tree Committee met Monday, July 7th, 2014 to discuss the SV Roasters design review application. A quorum was established with Jeff Beacham, Juerg Stauffacher and Tom Ward in attendance. The applicant and architect were also in attendance.</p> <p>We chose to enter into a round table discussion of the application and the merits for/or against keeping the Siberian Elm. The HTC was unable to find adequate solutions for keeping the Siberian Elm as it relates to this application.</p> <p><i>Therefore, The HTC supports the application to remove the Siberian Elm and develop the property with the following recommendations:</i></p> <ul style="list-style-type: none"> - 1. The four (4) new trees should be any combination of - Linden, Canadian Choke Cherry or Japanese Lilac, in 2.5" to 3" caliper. - 2. Vaults should be considered to allow better root growth. - 3. Edging (metal or other) should be installed around the root zone and covered with compost or mulch to prevent compaction of soils around the trees. - 4. Redesign alignment of tree locations in relation to parking spaces and install vertical curb to protect trees from vehicles. - 5. Install trees to ANSI - 231 standards. -
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.8	<p>Guideline: Attention shall be given to other significant landscape features which may be present on the site. Mature shrubs, flower beds and other significant landscape features shall be shown on the site plan and be incorporated into the site plan where feasible.</p>

			<i>Staff Comments</i>	- <i>The site is flat with no proposed plantings.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.8	Guideline: Noxious weeds shall be controlled according to State Law.
			<i>Staff Comments</i>	- <i>All weeds will be removed from the site.</i>
9. Fences and Walls: 6A.9.C.9				
Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.9.C.9	Guideline: Fences and walls that abut public streets and sidewalks should be designed to include fence types that provide some transparency, lower heights and clearly marked gates.
			<i>Staff Comments</i>	- <i>A 5' 3" tall corrugated metal fence is proposed. Materials will match the addition and blend with the architecture.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.9	Guideline: Retaining walls shall be in scale to the streetscape.
			<i>Staff Comments</i>	N/A
11. Historic Structures: 6A.9.C.11 (NOTE: Applicable only to structures built prior to 1940)				
Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.10	General Guidelines: Any alteration to the exterior of a Historic Structure requiring design review approval shall meet the following guidelines: <ul style="list-style-type: none"> • The alteration should be congruous with the historical, architectural, archeological, educational or cultural aspects of other Historic Structures within the Townsite Overlay District, especially those originally constructed in the same Period of Significance. • The alteration shall be contributing to the Townsite Overlay District. Adaptive re-use of Historic Structures is supported while maintaining the architectural integrity of the original structure.
			<i>Staff Comments</i>	- <i>No historic buildings are on site.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	6A.9.C.9	Specific Guidelines. Any alteration to the exterior of a Historic Structure requiring design review approval shall meet the following specific guidelines: <ul style="list-style-type: none"> • The design features of repairs and remodels including the general streetscape, materials, windows, doors, porches, and roofs shall not diminish the integrity of the original structure. • New additions should be designed to be recognizable as a product of their own Period of Significance with the following guidelines related to the historical nature of the original structure: <ul style="list-style-type: none"> ○ The addition should not destroy or obscure important architectural features of the original building and/or the primary façade; ○ Exterior materials that are compatible with the original building materials should be selected; ○ The size and scale of the addition should be compatible with the original building, with the addition appearing subordinate to the primary building; ○ The visual impact of the addition should be minimized from the street; ○ The mass and scale of the rooftop on the addition should appear subordinate to the rooftop on the original building, and should avoid breaking the roof line of the original building;

				<ul style="list-style-type: none"> ○ The roof form and slope of the roof on the addition should be in character with the original building; <p>The relationship of wall planes to the street and to interior lots should be preserved with new additions.</p>
			<i>Staff Comments</i>	- <i>No historic buildings are on site.</i>

6A.6 Criteria.

- A. The Commission or Hearing Examiner shall determine the following before approval is given:**
 - 1. The project does not jeopardize the health, safety or welfare of the public.**
 - 2. The project conforms to the applicable specifications outlined in the Design Review Guidelines, as set forth herein, applicable requirements of the Zoning Ordinance, and City Standards.**

- B. Conditions. The Commission or Hearing Examiner may impose any condition deemed necessary. The Commission or Hearing Examiner may also condition approval of a project with subsequent review and/or approval by the Administrator or Planning Staff. Conditions which may be attached include, but are not limited to those which will:**
 - 1. Ensure compliance with applicable standards and guidelines.**
 - 2. Require conformity to approved plans and specifications.**
 - 3. Require security for compliance with the terms of the approval.**
 - 4. Minimize adverse impact on other development.**
 - 5. Control the sequence, timing and duration of development.**
 - 6. Assure that development and landscaping are maintained properly.**
 - 7. Require more restrictive standards than those generally found in the Zoning Ordinance.**

The following conditions are suggested to be placed on any approval of this application:

- a) All applicable Fire Department and Building Department requirements shall be met.
- b) Any change in use or occupancy type from that approved at time of issuance of Building Permit may require additional improvements and/or approvals. Additional parking may also be required upon subsequent change in use, in conformance with Hailey’s Zoning Ordinance at the time of the new use.
- c) All City infrastructure requirements shall be met. Detailed plans for all infrastructure to be installed or improved at or adjacent to the site shall be submitted for Department Head approval and shall meet City Standards where required. Infrastructure to be completed at the applicant’s sole expense include, but will not be limited to, the following requirements and improvements:
- d) The project shall be constructed in accordance with the application or as modified by these Findings of Fact, Conclusions of Law and Decision.

- e) All new and existing exterior lighting shall comply with the Outdoor Lighting Ordinance.
- f) Except as otherwise provided, all the required improvements shall be constructed and completed, or sufficient security provided as approved by the City Attorney, before a Certificate of Occupancy can be issued.
- g) This Design Review approval is for the date the Findings of Fact are signed. The Planning & Zoning Administrator has the authority to approve minor modifications to this project prior to, and for the duration of a valid Building Permit.
- h) All utilities will be located underground, consistent with 6A.9.C.1.
- i) The mature Siberian Elm shall be removed at the applicant's cost to be replaced with four new street trees and sidewalk, curb, and gutter.
- j) The following conditions shall be met regarding new street trees along Walnut Street:
 - 1. The four (4) new trees should be any combination of - Linden, Canadian Choke Cherry or Japanese Lilac, in 2.5" to 3" caliper.
 - 2. Vaults should be considered to allow better root growth.
 - 3. Edging (metal or other) should be installed around the root zone and covered with compost or mulch to prevent compaction of soils around the trees.
 - 4. Redesign alignment of tree locations in relation to parking spaces and install vertical curb to protect trees from vehicles.
 - 5. Install trees to ANSI - 231 standards.

C. Security. The applicant may, in lieu of actual construction of any required or approved improvement, provide to the City such security as may be acceptable to the City, in a form and in an amount equal to the cost of the engineering or design, materials and installation of the improvements not previously installed by the applicant, plus fifty percent (50%), which security shall fully secure and guarantee completion of the required improvements within a period of one (1) year from the date the security is provided.

- 1. **If any extension of the one year period is granted by the City, each additional year, or portion of each additional year, shall require an additional twenty percent (20%) to be added to the amount of the original security initially provided.**
- 2. **In the event the improvements are not completely installed within one (1) year, or upon the expiration of any approved extension, the City may, but is not obligated, to apply the security to the completion of the improvements and complete construction of the improvements.**

Motion Language

Approval:

Motion to approve the Design Review application submitted by Sun Valley Roasters LLC, represented by Jolyon Sawrey, for Design Review of an addition to an existing commercial building, located at Hailey Townsite, Block 29, Lots 10 (219 South Main Street), within the Business (B) and Townsite Overlay (TO) Zoning Districts, finding that the project does not jeopardize the health, safety or welfare of the public and the project conforms to the applicable specifications outlined in the Design Review Guidelines,

applicable requirements of the Zoning Ordinance, Title 18, and City Standards, provided conditions (a) through (j) are met.

Denial:

Motion to deny Design Review application submitted by Sun Valley Roasters LLC, represented by Jolyon Sawrey, for Design Review of an addition to an existing commercial building, located at Hailey Townsite, Block 29, Lots 10 (219 South Main Street), within the Business (B) and Townsite Overlay (TO) Zoning Districts, finding that _____ [the Commission should cite which standards are not met and provided the reason why each identified standard is not met].

Continuation:

Motion to continue the public hearing on Design Review application submitted by Sun Valley Roasters LLC, represented by Jolyon Sawrey, for Design Review of an addition to an existing commercial building, located at Hailey Townsite, Block 29, Lots 10 (219 South Main Street), within the Business (B) and Townsite Overlay (TO) Zoning Districts, to _____ [Commission should specify a date].

City of Hailey - Design Review Application

RECEIVED JUN 19 2014

Submittal Date: 06 / 19 / 2014

Project Name: HAILEY COFFEE

Parcel No. RPH 00000290100

Legal Description of Property: Subdivision HAILEY CITY OF HAILEY Lot(s) # 10, Block 29

Street Address of Property: 219 SOUTH MAIN STREET

Current Zoning of Property: BUSINESS Year of original construction: 1980's ?

(Only applicable if property is within the Townsite Overlay)

Existing building gross sq. ft. (if applicable) 1339

Proposed addition or new construction sq. ft. 2258

Name of Owner of the Property: Sun Valley Roaster's LLC

Mailing Address: POB 1529 City: Hailey State: ID Zip: 83333

Phone: () Fax: () Cell: (208) 450-9591

Email Address:

Property Owner Consent:

By signature hereon, the property owner acknowledges that City officials and/or employees may, in the performance of their functions, enter upon the property to inspect, post legal notices, and/or other standard activities in the course of processing this application, pursuant to Idaho Code §67-6507. The property owner is also hereby notified that members of the Planning and Zoning Commission and City Council are required to generally disclose the content of any ex parte discussion (outside the hearing) with any person, including the property owner or representative, regarding this application.

Property Owner's Signature: Carole Morgan Date: 6 / 18 / 14

Name of individual to contact on behalf of Trust or LLC (if applicable):

Mailing Address: City: State: Zip:

Phone: () Fax: () Cell: ()

Email Address: see above

Application Contact (if different than above): JOLYON H SAUREY

**Application Contact will be the Planning Department's primary point of contact for questions related to the application.

Mailing Address: 30 WYATT DRIVE City: BELLEVUE State: ID Zip: 83313

Phone: () Fax: () Cell: (208) 720-6315

Email Address: JOLYON@VITALSPECINC.COM

Signature: Date: / /

See attached checklist for items that must be submitted with this application in order for application to be considered complete. See the applicable Design Review Guidelines, including Townsite Overlay Guidelines if applicable, in Section 6A of the Hailey Zoning Ordinance.

Appeals: Any interested party may appeal in writing any final decision of the Planning and Zoning Administrator, Hearing Examiner, or Commission to the City Council by filing an appeal with the Hailey City Clerk within fifteen (15) days from the date of the decision. The appeal shall specifically state the decision appealed and reasons for the appeal. If no appeal is filed within the fifteen (15) day period, the decision shall be deemed final.

FOR CITY USE ONLY Fees: Cost of additional noticing, recording fees, and other direct costs will also be assessed.

<input checked="" type="checkbox"/> Commercial, Mixed-Use or Multi-Family.....	\$ 450.00
plus \$25 / 1,000 gross square feet.....	\$ 507.25
OR <input type="checkbox"/> Single-Family Dwelling, Duplex or Accessory Structure in TO.....	\$ 250.00
OR <input type="checkbox"/> No Substantial Impact..... (Mailing only).....	\$ 75.00
OR <input type="checkbox"/> Modification to DR Approval.. (No publication or mailing).....	\$ 50.00
OR <input type="checkbox"/> DR Exemption... (No publication or mailing).....	\$ 30.00
<input checked="" type="checkbox"/> Publication cost.....	\$ 40.00
<input checked="" type="checkbox"/> Mailing (# of addresses 26) x (.49 postage + .15 paper, envelope & label)	\$ 16.64
DO NOT COUNTY DUPLICATES OR CITY OF HAILEY	
Total Due.....	\$ 563.89

DESIGN REVIEW—APPLICATION CHECKLIST

Project Name: _____

City Use Only

Certified

Complete by: _____

Date: ____/____/____

The following items must be submitted with the application for the application to be considered complete (✓):

Yes	No	Item	Description
<input type="checkbox"/>	<input type="checkbox"/>	1.	The completed Design Review application form including project name and location, and applicant and representative names and contact information.
<input type="checkbox"/>	<input type="checkbox"/>	2.	One (1) 24" x 36" set of plans and survey and one (1) 11" x 17" set showing at a minimum the following:
<input checked="" type="checkbox"/>	<input type="checkbox"/>		a. Vicinity map, to scale, showing the project location in relationship to neighboring buildings and the surrounding area. Note: a vicinity map must show location of adjacent buildings and structures.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		b. Drainage plan (grading, catch basins, piping, and dry-wells).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		c. Utilities plan (location and size of water and sewer mains and services, gas, electric, TV and phone).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		d. Site plan, to scale, showing proposed parking (including parking stall dimensions), loading, general circulation, and snow storage. List square footage of subject property including lot dimensions.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		e. Landscape plan (existing landscaping on the site shown as retained, relocated or removed; proposed landscaping including species type, size and quantity).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		f. Floor plan. List gross square footage for each floor. List occupancy classification and type of construction.
<input type="checkbox"/>	<input type="checkbox"/>		g. Detailed elevations of all sides of the proposed building and other exterior elements (colors, materials).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		h. Exterior Lighting plan, pursuant to Article VIII B, of the Zoning Ordinance (location, height, type, and lumen output; spec sheets for fixtures; illuminance levels/photometrics for area lighting).
<input type="checkbox"/>	<input type="checkbox"/>		i. Sign plan (location, dimensions and lighting).
<input type="checkbox"/>	<input type="checkbox"/>	3.	Plans and drawings for all buildings, except Single Family Dwellings and Accessory Structures, shall be prepared and stamped by an Idaho licensed architect.
<input type="checkbox"/>	<input type="checkbox"/>	4.	A materials and colors sample board. Each sample should be approximately 12"x12" in size.
<input type="checkbox"/>	<input type="checkbox"/>	5.	One (1) colored rendering of at least one side of the proposed building.
<input type="checkbox"/>	<input type="checkbox"/>	6.	Staging and contractor parking plan. Statement of where staging will occur, and parking plan for contractors. If any staging or parking shall occur off-site, a staging/parking plan must be submitted including materials storage, excavation (backfill) stockpile areas, job trailers, blue rooms, dumpsters, contractor parking, etc.
<input type="checkbox"/>	<input type="checkbox"/>	7.	A list of the names and addresses of all property owners and residents within three hundred (300) feet of the exterior boundaries of the subject property, in a format acceptable to the City. (Names and addresses can be obtained using the Blaine County map server http://maps.co.blaine.id.us/ or from the Blaine County Assessor's office)
<input type="checkbox"/>	<input type="checkbox"/>	8.	Other information as required by the Administrator, Hearing Examiner or the Commission.
<input type="checkbox"/>	<input type="checkbox"/>	9.	Electronic PDF copies of all documents and materials listed above. (RECOMMENDED)
<input type="checkbox"/>	<input type="checkbox"/>	10.	Written statement of how each design review standard is met. (RECOMMENDED)
<input type="checkbox"/>	<input type="checkbox"/>	11.	Payment of applicable fees.

City Use Only:

- ____ Verify that application is complete
- ____ Double check address
- ____ Advise applicant if Lot Line Adjustment is needed

- ____ Check following basic standards:
- ____ Density
 - ____ Setbacks
 - ____ Height (plans must show elevation points of record grade)
 - ____ Lot coverage
 - ____ Floor area
 - ____ Required Parking Spaces

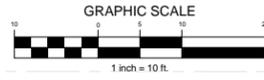
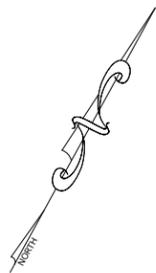
From: Miguel <mike@ccdisposal.com>
Subject: **RE: MOST ROTATED DESIGN**
Date: June 19, 2014 9:40:38 AM MDT
To: "Jolyon H. Sawrey" <jolyon@vitalinkarchitecture.com>

Jolyon,

This "MOST ROTATED DESIGN" will work for the garbage truck to be able to empty the dumpster. As you know the actual emptying will happen in the alley and West of the overhead power lines. The dumpster will be moved each time it is emptied by the truck out into the alley and then returned back to the permanent location. The recycle carts will need to be transported to the alley for emptying on collection day by onsite staff and then returned as well. Service for the dumpster will not be hindered, so long as, the "tenant" does not park a car too far North interfering with access. Please call with any questions and/or concerns.

Thank You,
Mike Goitiandia
Clear Creek Disposal

From: Jolyon H. Sawrey [mailto:jolyon@vitalinkarchitecture.com]
Sent: Thursday, June 19, 2014 8:24 AM
To: Miguel
Subject: MOST ROTATED DESIGN



DESIGN REVIEW SET

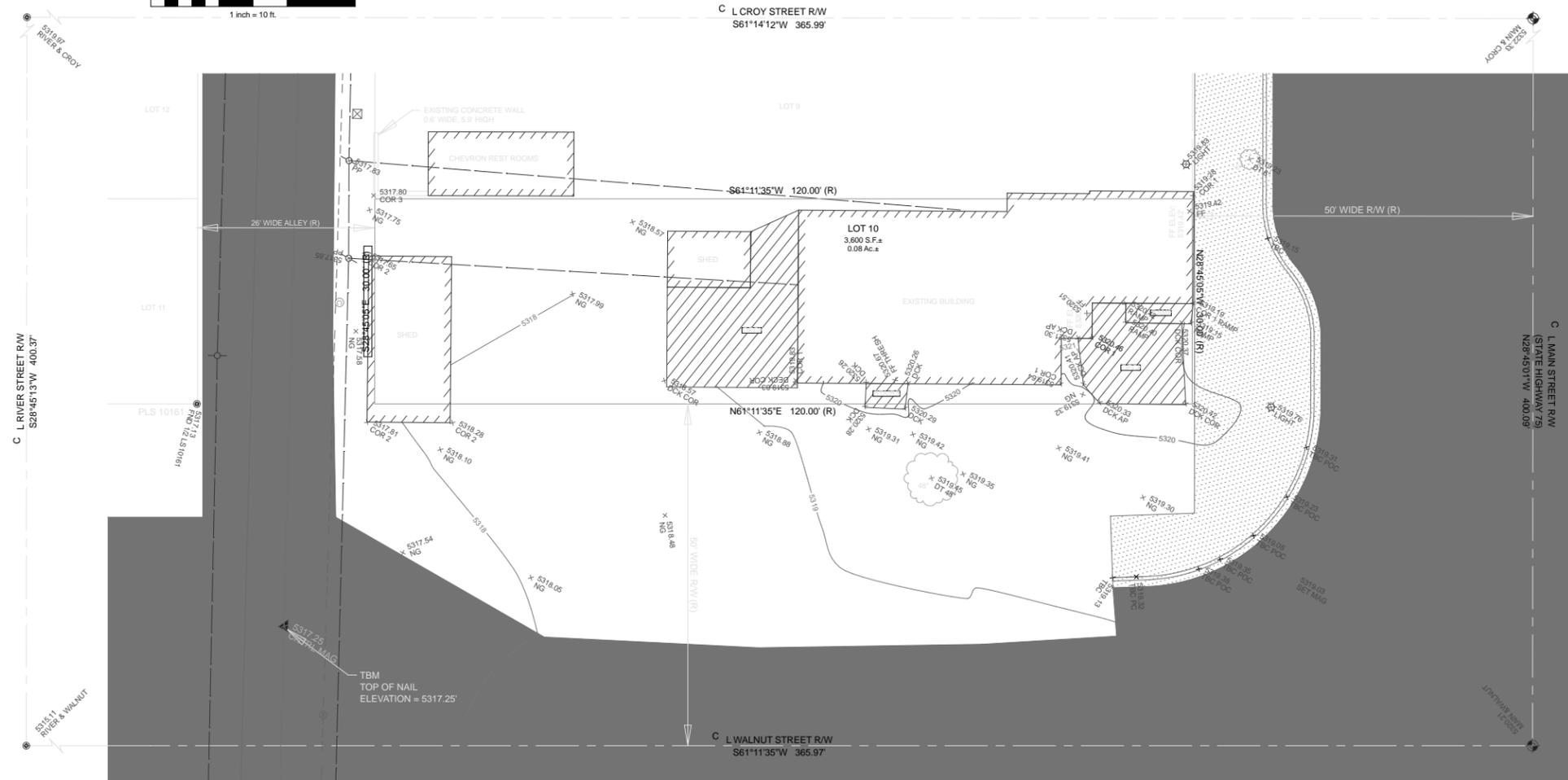


VICINITY MAP
NTS

LEGEND	
	Property Line
	Adjoins Lot Line
	Center Line of Right-of-Way
	1' Contour
	5' Contour
	Overhead Power Line
	Existing Water Main
	Existing Gas Main
	Existing Sewer Main
	Existing Building
	Existing Concrete
	Existing Asphalt
	Existing Vertical Curb and Gutter
	Existing Street Light
	Deciduous Tree
	Existing Water Valve Box
	Existing Water Valve
	Existing Power Pole
	Existing Sewer Clean Out
	Found 1/2" Rebar
	Found Brass Cap
	NG = Natural Ground
	(R) = Record Measurement
	R/W = Right-of-Way
	FF ELEV = Finish Floor Elevation
	TBM = Temporary Benchmark

NOTES

- Boundary information is approximate and based on record information. Please refer to the recorded plat of Hailey Townsite.
- Refer to the Plat Notes, Conditions, Covenants, and Restrictions on Original Plat.
- Utility locations are approximate and locations should be verified before any excavation.
- Galena Engineering Inc. has not received a Title Policy from the client and has not been requested to obtain one. Relevant information that may be contained within a Title Policy may therefore not appear on this map and may affect items shown hereon. It is the responsibility of the client to determine the significance of the Title Policy information and determine whether it should be included. If the client desires for the information to be included they must furnish said information to Galena Engineering, Inc. and request it be added to this map.
- Due to the weather conditions and snow on the existing Lot 10, Block 29 Galena Engineering Inc., shall not be responsible for any existing features which might not have been located due to such weather conditions. Edge of asphalt shown is approximate due to snow conditions at the time of survey.
- Contours and elevations as shown here on are based on TBM (Temporary Benchmark) elevation 5317.25' as shown here on.
- Building foot prints as shown do not include eaves or roof overhangs.



GREGORY L. HARBESON, P.L.S. 13004

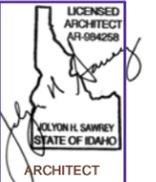
REVISIONS

Galena Engineering Inc.
Civil Engineers & Land Surveyors
690 Second Avenue North
P.O. Box 425
Ketchum, Idaho 83340
(208) 726-4729
(208) 726-4783 fax

REUSE OF DRAWINGS
These drawings, or any portion thereof, shall not be used on any Project or extensions of this Project except by agreement in writing with Galena Engineering, Inc.

DESIGNED BY	DATE
JSR	01/04/08
DRAWN BY	DATE

A TOPOGRAPHIC SURVEY SHOWING
LOT 10, BLK 29, HAILEY TOWNSITE
WITHIN SECTION 9, T.2N., R.18E., B.M., CITY OF HAILEY, BLAINE COUNTY, IDAHO
PREPARED FOR TONY ST. GEORGE



FACILITY EXPANSION FOR:
HAILEY COFFEE
219 SOUTH MAIN STREET
HAILEY, IDAHO

Vital ink PLLC
Environmental Architecture
& Consulting AIA
(208) 720-6315 Ph
30 Watt Drive
Bellevue, Idaho 83313

REVISIONS

DATE
18 JUNE, 2014

1.0
DATED: 6/18/14 11:54 AM



From: Miguel <mike@cdsdisposal.com>
 Subject: RE: MOST ROTATED DESIGN
 Date: June 19, 2014 9:42:38 AM MDT
 To: "Jolyon H. Sawrey" <jolyon@vitalinkarchitecture.com>

Jolyon,

This "MOST ROTATED DESIGN" will work for the garbage truck to be able to empty the dumpster. As you know the actual emptying will happen in the alley and West of the overhead power lines. The dumpster will be moved each time it is emptied by the truck out into the alley and then returned back to the permanent location. The recycle carts will need to be transported to the alley for emptying on collection day by onsite staff and then returned as well. Service for the dumpster will not be hindered, so long as, the "tenant" does not park a car too far North interfering with access. Please call with any questions and/or concerns.

Thank You,
 Mike Gottiandia
 Clear Creek Disposal

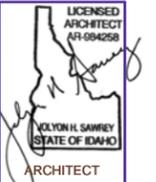
From: Jolyon H. Sawrey [mailto:jolyon@vitalinkarchitecture.com]
 Sent: Thursday, June 19, 2014 8:24 AM
 To: Miguel
 Subject: MOST ROTATED DESIGN

NOTE: THE PLAN THAT MIKE REFERENCES WAS A DRAFT OF THE DESIGN SHOWN. JOLYON H. SAWREY

DESIGN REVIEW SET

NOTES:

1. ALL SERVICES ARE TO BE RUN UNDERGROUND.
2. ALL PLUMBING, MECH VENTS TO BE ON BACK SIDE OF ROOFS IF POSSIBLE.
3. ALL FINISH GRADE TO BE BELOW TOP OF STEM WALLS AND SHALL SLOPE AWAY FROM BLDG PER CODE.
4. CITY OF HAILEY TO PROVIDE IRRIGATION TO NEW TREE AND TO ANY TREE THAT IS TO REMAIN.
5. NEW STREET IMPROVED AREAS SHALL COMPLY WITH THE CITY OF HAILEY STANDARDS FOR CONSTRUCTION. THIS SHALL INCL. MATERIALS, THICKNESS, PREP, SIZE ETC. FOR ROADWAYS, CURB/ GUTTER, SIDEWALKS, DRYWELLS ETC. FOR ALL WORK IN THE CITY RIGHT OF WAYS.
6. BIKE RACKS SHALL BE OF THE "LOLI-POP" STYLE THAT ARE PER THE CITY OF HAILEY STANDARDS
7. NO EXISTING VEGETATION TO REMAIN. THE LILAC BUSH IS BEING OFFERED TO WHOM EVER WILL REMOVE IT. THE LARGE SIBERIAN ELM TO BE REMOVED
- (4) NEW DECIDUOUS TREES WILL BE INSTALLED



SNOW STORAGE TABULATION CHART

HARD SURFACE AREA SF	25% SNOW STORAGE REQ.	SNOW STORAGE PROVIDED
389	97.25	98

NOTES:
 1. ONSITE AREA FOR PARKING, GARBAGE AND ASNOW STORAGE HAS A COMPACTED ROADMIX SURFACE. THE AREA IS DESIGNED TO SLOPE TO A ONSITE DRYWELL. IF BUDGET ALLOWS THE SURFACE WILL BE EITHER ASPHALT OR CONCRETE.

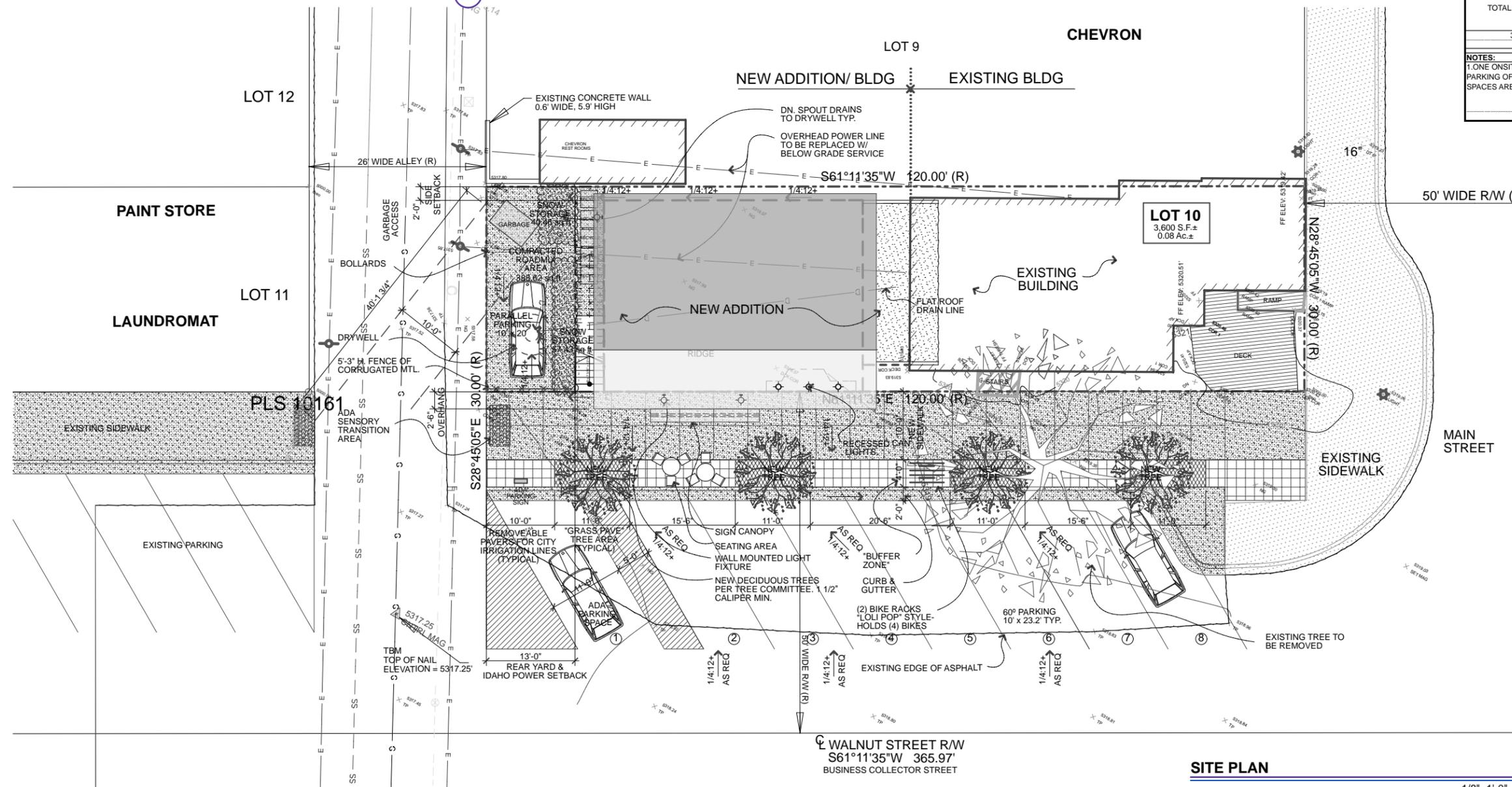
PARKING SPACES TABULATION CHART

TOTAL BLDG. SF	SPACES REQ. (1) SPACE/100SF & (1) PER RESIDENTIAL.	PARKING SPACES PROVIDED
3597	4,597	8

NOTES:
 1. ONE ONSITE PARKING SPACE IS PROVIDED FOR PARALLEL PARKING OFF OF THE ALLEY. THE REMAINING PARKING SPACES ARE PROVIDED IN THE CITY RIGHT OF WAY.

SOUTH ELEVATION- EXISTING TREE

2



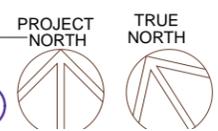
FACILITY EXPANSION FOR:
HAILEY COFFEE
 HAILEY, IDAHO
 219 SOUTH MAIN STREET

Vital ink LLC
 Environmental Architecture & Consulting AIA
 30 Watt Drive
 Bellevue, Idaho 83313
 (208) 720-6315 Ph

REVISIONS

DATE
 18 JUNE, 2014

1.1

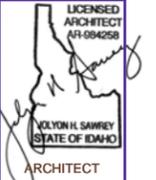


SITE PLAN

1/8"=1'-0"

1

CREDIT: 6/19/14 11:54 AM



FACILITY EXPANSION FOR:
HAILEY COFFEE
 HAILEY, IDAHO
 219 SOUTH MAIN STREET

Vital ink PLLC
 Environmental Architecture
 & Consulting AIA

30 Wyatt Drive
 Bellevue, Idaho 83313
 (208) 720-6315 Ph

REVISIONS

DATE
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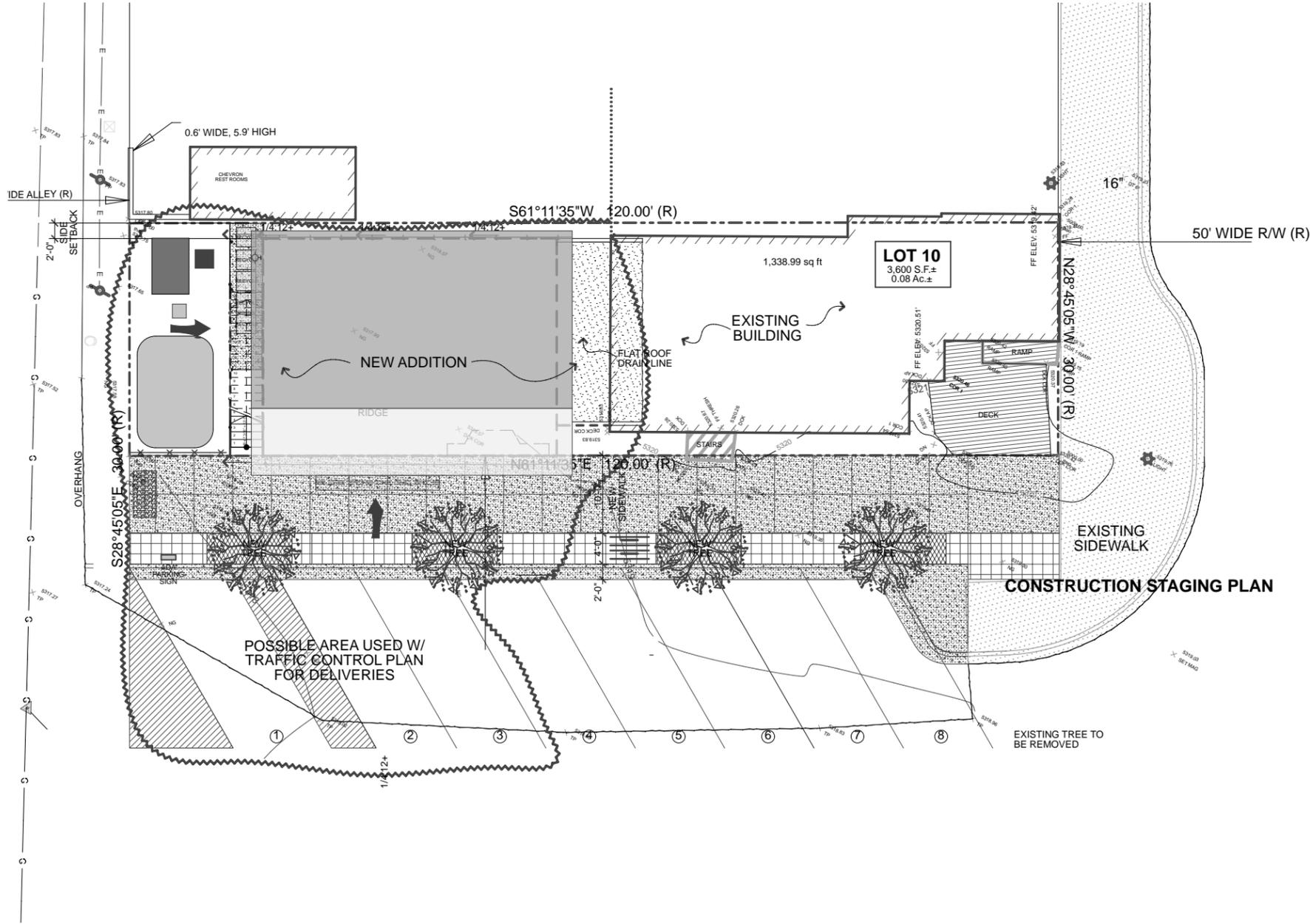
1.2
NOTED: 6/19/14 1:41 PM

- NOTES**
1. ALL CONSTRUCTION TO ABIDE BY SUN VALLEY CITY ORDINANCES.
 2. CONSTRUCTION HOURS LIMITED TO MONDAY-FRIDAY 7:30 a.m. TO 7:30 p.m. SATURDAY 9:00 a.m. to 6:00 p.m. and SUNDAY (NO WORK)
 3. NO CONSTRUCTION PARKING ALLOWED ON CITY STREETS
 4. SNOW STORAGE: NO ADDITIONAL SNOW STORAGE IS NEEDED FOR THIS PROJECT AS THE AREAS REQUIRED TO BE PLOWED FOR RESIDENTIAL ACCESS TO THE HOME ARE NOT BEING CHANGED/ EFFECTED.
 5. CONSTRUCTION ACCES ROADWAY WILL NOT EXCEED A 10% SLOPE. THIS DRAWING SHOWS FINAL GRADING AND EXISTING GRADE BUT DOES NOT REFLECT THE GRADING FOR THE TEMPORARY CONSTRUCTION ACCESS ROAD

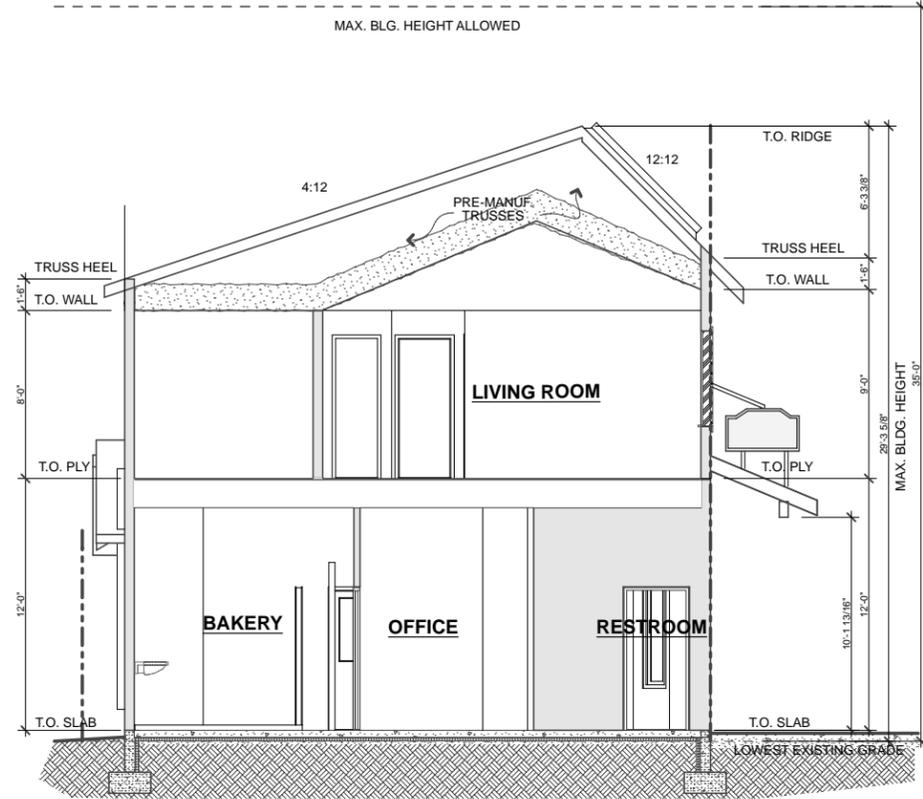
SYMBOL LEGEND

CONSTRUCTION

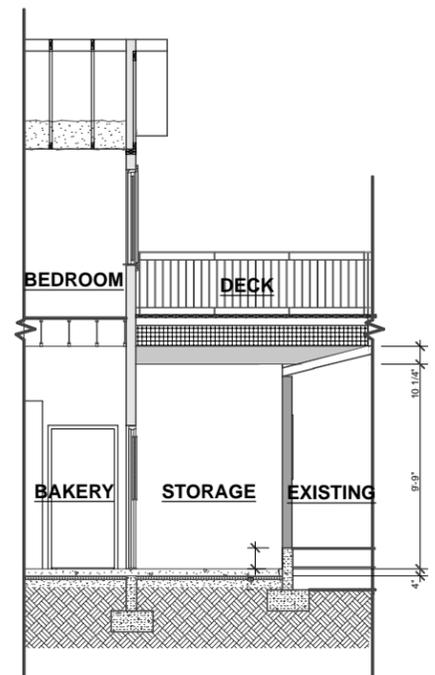
- CONSTRUCTION PARKING-OFF SITE AT PARK & RIDE LOT
- CONSTRUCTION STAGING AREA
- CONSTRUCTION DUMPSTER
- RECYCLING BIN
- PORTABLE RESTROOM
- CONSTRUCTION AREA OF DISTURBANCE
- CONSTRUCTION ACCESS



CONSTRUCTION STAGING PLAN



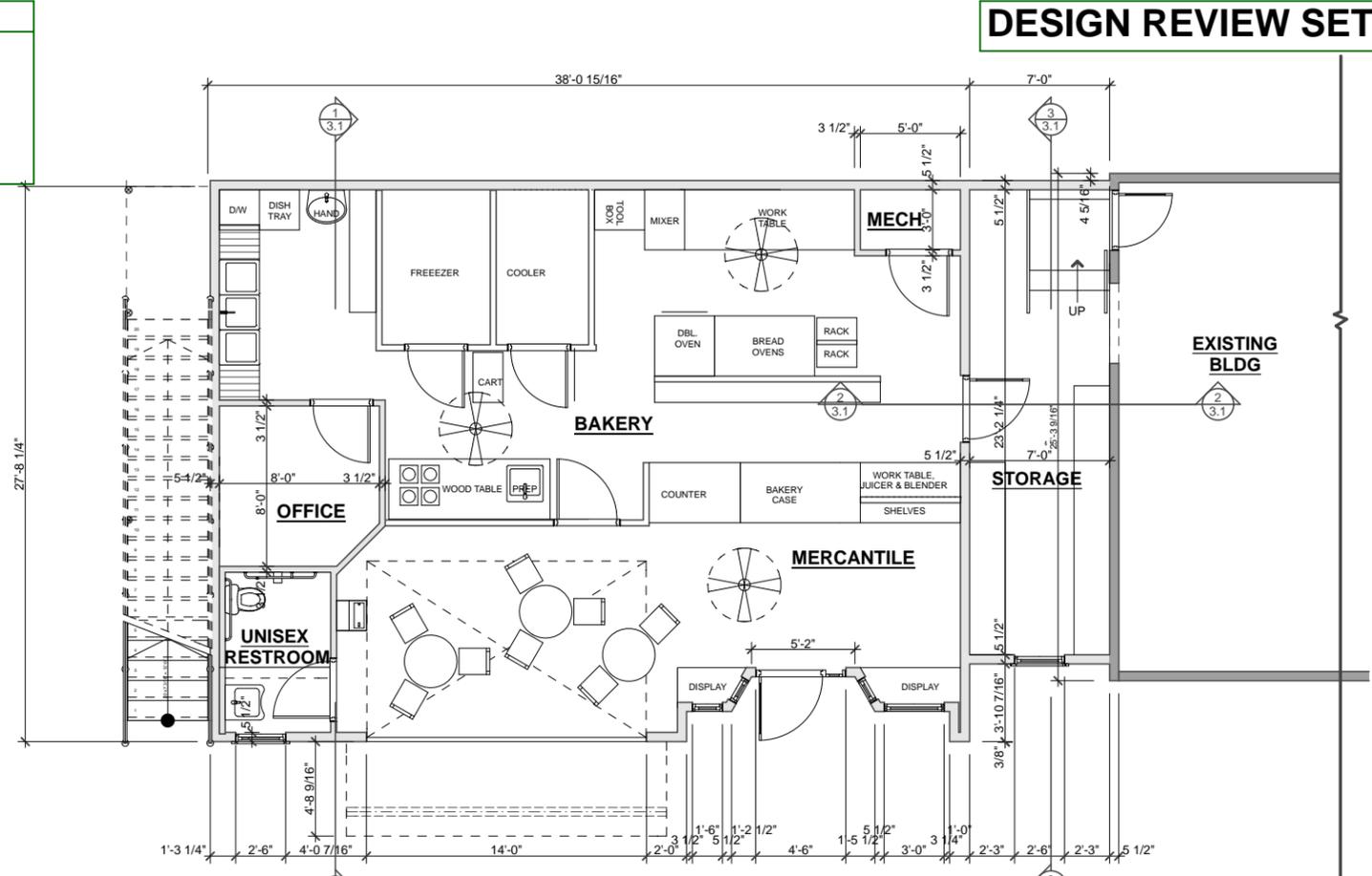
SECTION 3
1/4" = 1'-0"



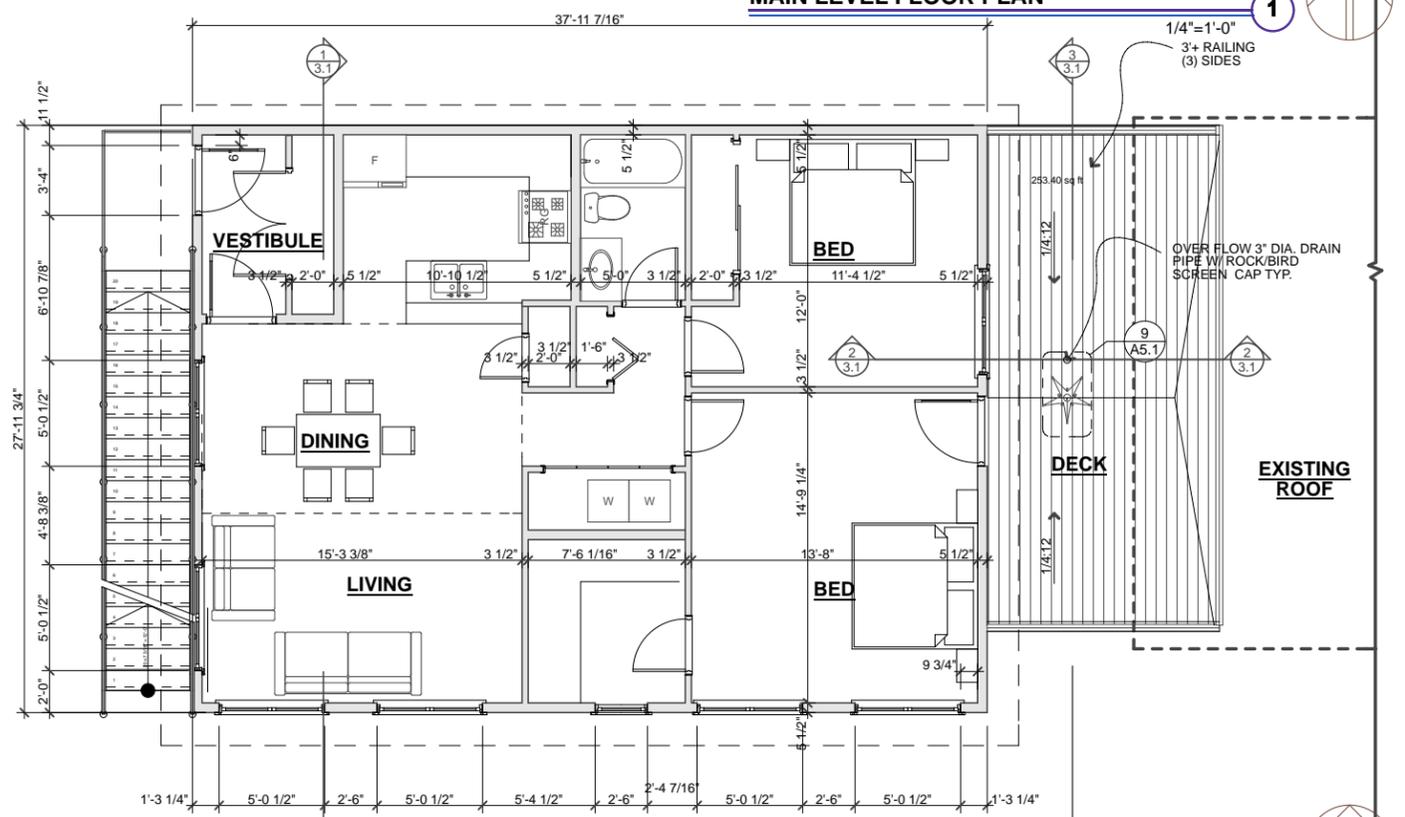
SECTION 4
1/4" = 1'-0"

SYMBOL LEGEND

- (N) FRAME WALLS
- (E) FRAME WALLS
- CONTROL JOINT



MAIN LEVEL FLOOR PLAN 1
1/4" = 1'-0"



UPPER LEVEL FLOOR PLAN 2
1/4" = 1'-0"

DESIGN REVIEW SET

LICENSED ARCHITECT
AR-684258
WOLYON H. SAWREY
STATE OF IDAHO
ARCHITECT

FACILITY EXPANSION FOR:
HAILEY COFFEE
HAILEY, IDAHO
219 SOUTH MAIN STREET

(208) 720-6315 Ph
Vital ink LLC
Environmental Architecture & Consulting AIA
30 Warr Drive
Bellevue, Idaho 83313

REVISIONS

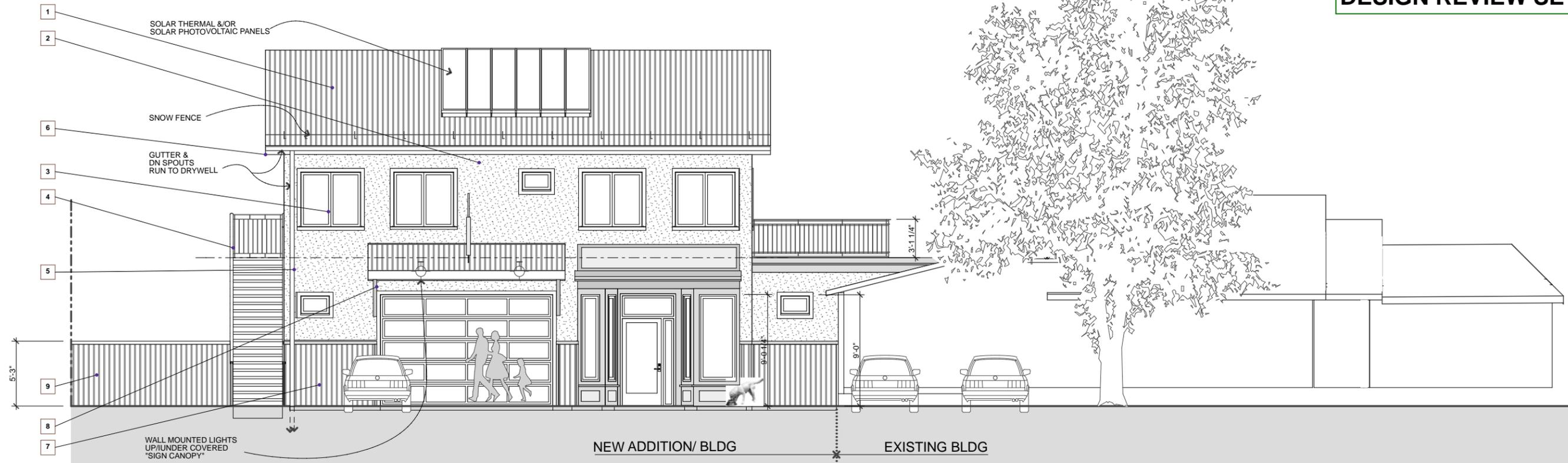
DATE
18 JUNE, 2014

2.1
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DESIGN REVIEW SET

LICENSED ARCHITECT
 AR-684258
 KOLYON H. SAWREY
 STATE OF IDAHO
 ARCHITECT

FACILITY EXPANSION FOR:
HAILEY COFFEE
 219 SOUTH MAIN STREET
 HAILEY, IDAHO



NEW ADDITION/ BLDG

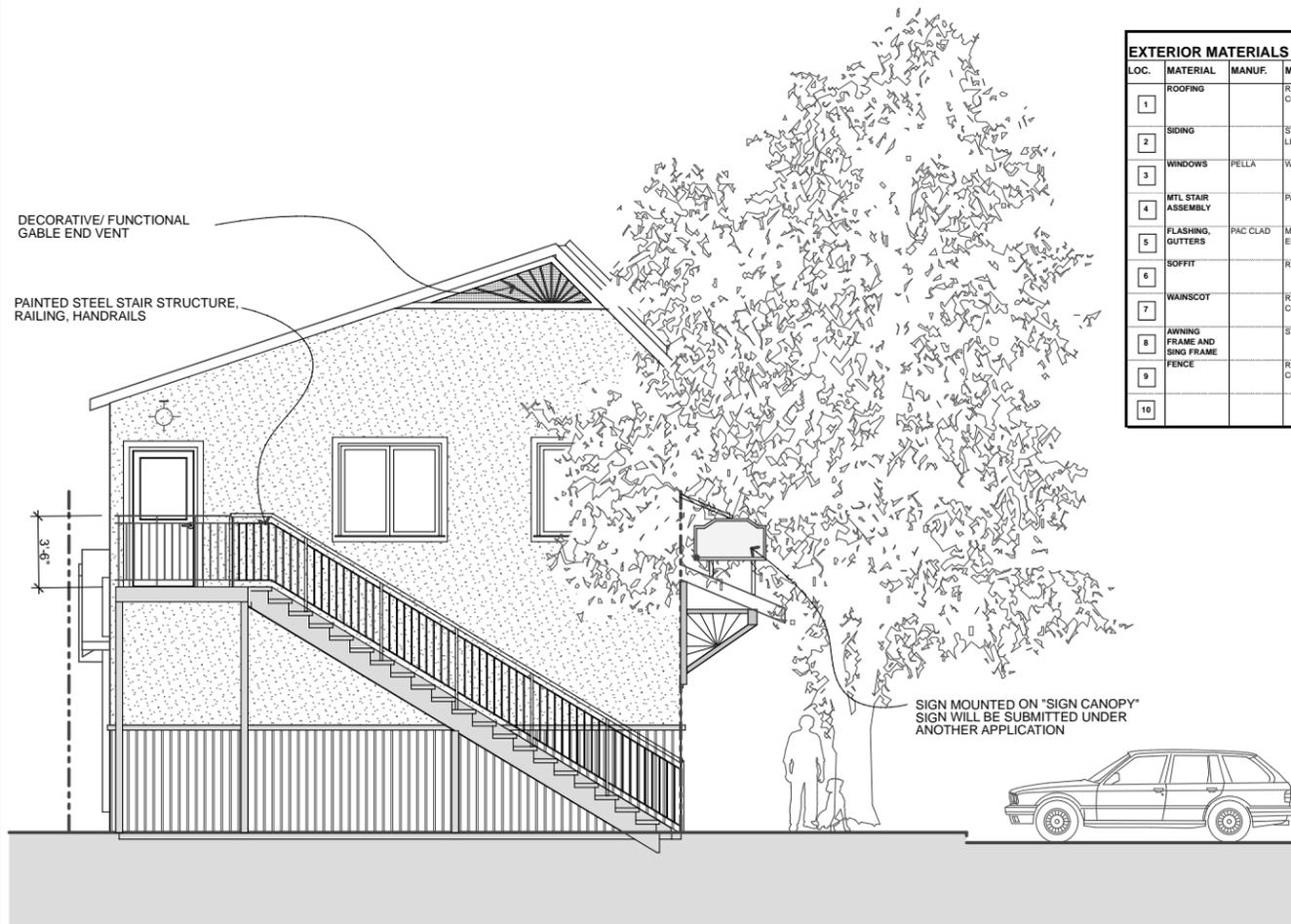
EXISTING BLDG

ELEVATION - SOUTH

1/4"=1'-0"

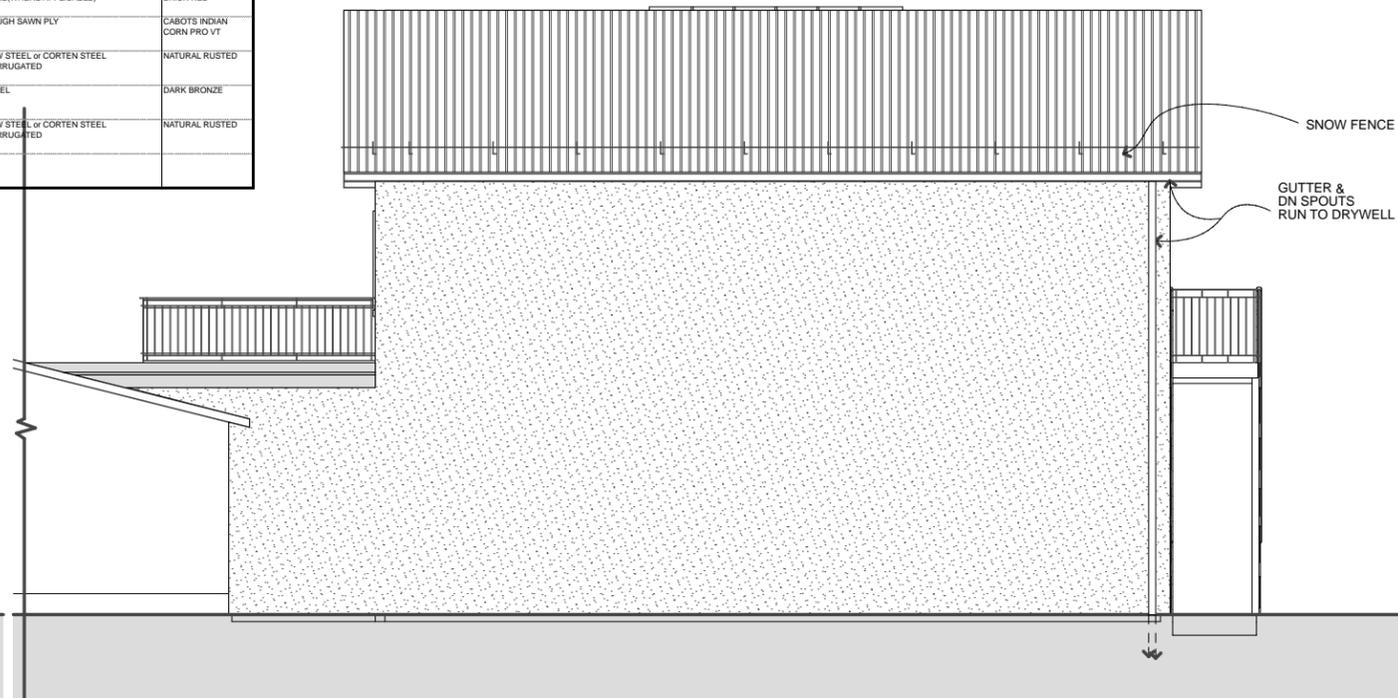
1

EXTERIOR MATERIALS				
LOC.	MATERIAL	MANUF.	MODEL	FINISH COLOR
1	ROOFING		RAW STEEL or CORTEN STEEL CORRUGATED	NATURAL RUSTED
2	SIDING		STUCCO TROWELED TO LOOK SMOOTH LIKE CONCRETE	SMOKE EMBERS
3	WINDOWS	PELLA	WOOD/ METAL CLAD -PELLA (PRO-LINE)	BRICK RED
4	MTL STAIR ASSEMBLY		PAINTED STEEL	MATCH CABOTS INDIAN CORN
5	FLASHING, GUTTERS	PAC CLAD	METAL FLASHING WITH THEMED DRIP EDGE(WHERE APPLICABLE)	DARK BRONZE & BRICK RED
6	SOFFIT		ROUGH SAWN PLY	CABOTS INDIAN CORN PRO VT
7	WAINSCOT		RAW STEEL or CORTEN STEEL CORRUGATED	NATURAL RUSTED
8	AWNING FRAME AND SIGN FRAME		STEEL	DARK BRONZE
9	FENCE		RAW STEEL or CORTEN STEEL CORRUGATED	NATURAL RUSTED
10				



ELEVATION - WEST

2



ELEVATION - NORTH

3

(208) 720-6315 Ph

Vital ink LLC
 Environmental Architecture
 & Consulting AIA

30 Watt Drive
 Bellevue, Idaho 83313

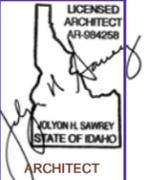
REVISIONS

DATE
 18 JUNE, 2014

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DESIGN REVIEW SET



FACILITY EXPANSION FOR:
HAILEY COFFEE
 HAILEY, IDAHO
 219 SOUTH MAIN STREET

(208) 720-6315 Ph



30 Watt Drive
 Bellevue, Idaho 83313

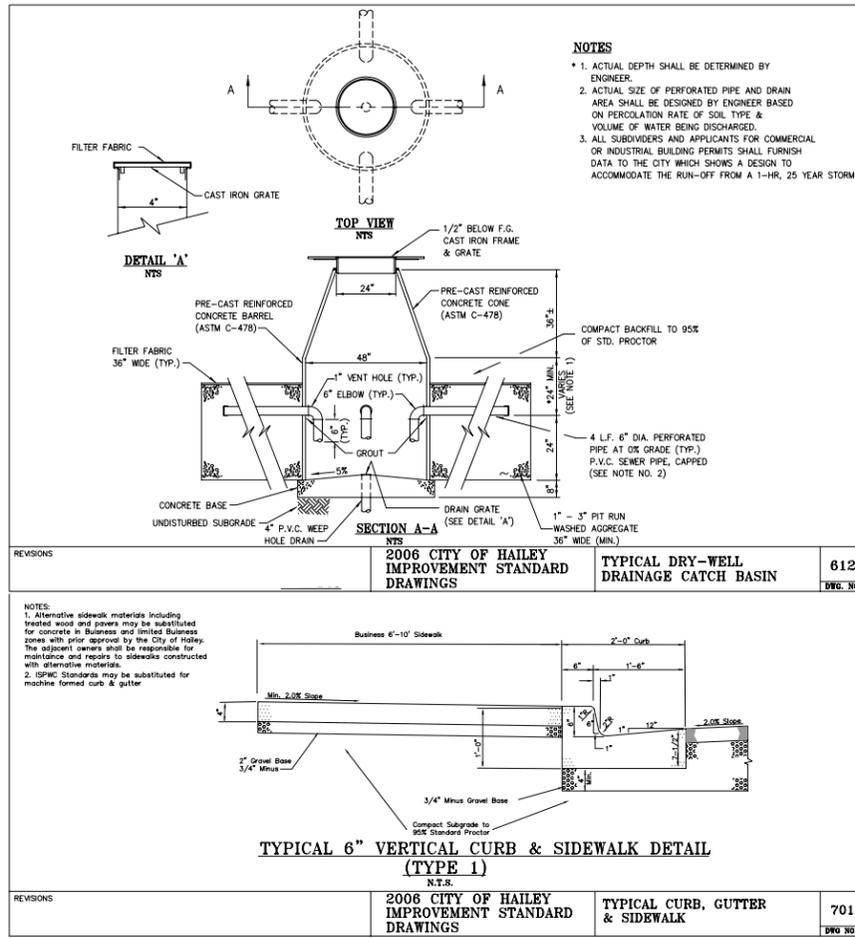
REVISIONS

DATE

18 JUNE, 2014

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CITY STND. DETAILS

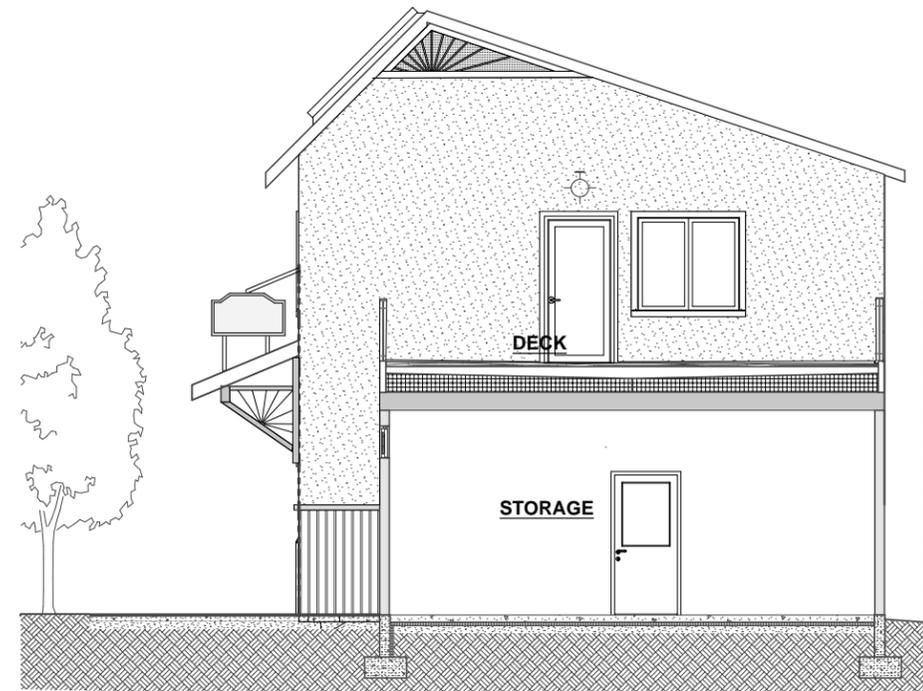
1/4"=1'-0" **3**



60W EQUIVALENT LED(10.5 W ACTUAL) LIGHT BULB, 800 LUMEN SHIELDED/CUT-OFF FIXTURE DOME, DOWN CAST LIGHTING USED UNDER SIGN CANOPY, AT VESTIBULE ENTRY(WEST) AND DECK ACCESS AT BEDROOM (EAST)

DOWN CAST/ CUT-OFF LIGHT FIXTURE

1/4"=1'-0" **2**



SECTION ELEVATION - EAST

To: Hailey Tree Committee
 From: Micah Austin, Community Development Director
 Re: DR Application: Siberian Elm at Hailey Coffee Company
 Date: July 7, 2014

**Options Considered for the Sun Valley Roaster Siberian Elm
 PZ Hearing: July 14, 2014**

Save/Remove	Options	Benefits	Concerns	Questions/Notes
Save the Tree	<u>Decking.</u> Construct decking around the tree to provide enough space for the tree to grow and	<ul style="list-style-type: none"> - One of the largest trees in the downtown business district is saved - Tree decking can be used as outdoor dining space - Alternative materials (wood, composite, etc.) could be used for the deck, which would be easier to modify as the tree grows. 	<ul style="list-style-type: none"> - Meeting ADA accessibility requirements will be difficult due to ramp incline necessary for the decking to be built around the tree roots and growth area. - In the best situation for the, the tree decking allows for only 48 inches of aisle width for pedestrian traffic, which does not meet City Standards and Title 18 requirements. - Eliminates three parking spaces, which may have an economic impact to Hailey Coffee Company 	<ul style="list-style-type: none"> - Who pays to save the tree? The Zoning Ordinance requires sidewalks to be installed at the owner's expense, however it does not address situations like this where significant costs would be required to save the tree. - If the business owner is not responsible for the costs to save the tree, the City will need to
	<u>Concrete Sidewalk.</u> Construct a traditional sidewalk with curb, gutter, and drainage facilities	<ul style="list-style-type: none"> - Sidewalk would conform to City Standards - Tree is saved 	<ul style="list-style-type: none"> - Due to construction of the sidewalk, the tree may not live. This is the concern Carl addresses. -Eliminates three parking spaces - Tree would damage the sidewalk over time, leading to costly repairs and unsafe conditions 	<ul style="list-style-type: none"> - This clearly is not the best option because we end up damaging the tree and likely killing it in the process.
Remove the Tree	<u>Concrete Sidewalk and Four New Trees.</u> This is what the Design Review application shows, subject to Tree Committee recommendations and PZ approval	<ul style="list-style-type: none"> - Sidewalks, street trees, drainage, and access can all be designed to meet standards. - Sidewalk can be designed as an outdoor dining area for Hailey Coffee 	<ul style="list-style-type: none"> - Removing one of the largest trees in downtown Hailey, which provides shade and enjoyment 	<ul style="list-style-type: none"> - Who pays for the tree to be removed? This is still being discussed by City Staff.



CITY OF HAILEY
Planning Administrator
Attn. Micah Austin

RE: Construction Staging and Parking Plan

Micah,

Due to the small size of this projects' property and that the new addition greatly covering the site the following Construction Staging and Parking Practices will apply.

CONSTRUCTION STAGING

All building materials and deliveries will either be able to be temporarily stored onsite in the back (west end of the site) where the proposed parking/ garbage area is located. For items that are too large or sizeable quantities, those items will be delivered and temporarily stored in the public right of way in parking stall areas 1 through 3. This process of use will follow any required "Traffic Control Plan" and coordination with the City of Hailey Street Department. Garbage and recycling along with temporary bathroom facilities will be placed in the west/ parking area on site.

PARKING PLAN

All general laborers/ tradesmen will be required to park off site and either be shuttled or walk to the jobsite. Initial drop off of smaller tools and supplies will occur from the alley. The suggested parking area is the Park and Ride lot just 1 block away to the North on River Street.

Sincerely,

Jolyon H. Sawrey, Architect/Land Planner



115 MAIN STREET S. SUITE H
 HAILEY, ID 83333
 PHONE: (208) 788-4221
 FAX: (208) 788-2924

INVOICE #	INVOICE DATE
111839621	06/20/2014
DUE DATE	CUSTOMER ACCOUNT NUMBER
06/21/2014	2701
AMOUNT DUE	TERMS:
563.89	Open Terms

BILL TO:

SUN VALLEY ROASTERS LLC

PO BOX 1529
 HAILEY ID 83333

PLEASE DETACH AND RETURN THIS TOP PORTION
 WITH YOUR PAYMENT BY DUE DATE TO:

CITY OF HAILEY
 115 S MAIN ST STE H
 HAILEY, ID 83333

INVOICE

DESCRIPTION	QUANTITY	CHARGE	EXT. PRICE
PLANNING DESIGN REVIEW	1	507.25	507.25
PLANNING PUBLICATION	1	40.00	40.00
PLANNING POSTAGE	1	16.64	16.64
			563.89
			TOTAL AMOUNT DUE

THANK YOU FOR YOUR PROMPT PAYMENT
 For Billing Inquiries Call: (208)788-4221
 Office Hours: 9:00 a.m. - 5:00 p.m.
 Monday thru Friday

CITY OF HAILEY
115 MAIN ST SOUTH STE H
HAILEY ID 83333

208-788-4221

Receipt No: 2.047527

Jun 20, 2014

2701
SUN VALLEY ROASTERS LLC
PO BOX 1529
HAILEY ID 83333

Previous Balance:	563.89
Accounts Receivable	
A/R Payments	563.89
001-00-10700	
Accts Rec Cash Clearing Acct	
Total:	563.89
New Balance:	.00
Check - MWB	
Check No: 6187	563.89
Total Applied:	563.89
Change Tendered:	.00

Duplicate Copy
06/20/2014 10:35AM

STAFF REPORT

TO: Hailey Planning and Zoning Commission
FROM: Micah Austin, Community Development Director
RE: Preliminary Plat – Sunburst Hills Subdivision
HEARING: July 14, 2014

Applicant: Tanner Investments, represented by Brant Tanner and Brian Yeager
Project: Sunburst Hills Cottage Townhouse Sub-lot Subdivision Development
Request: Preliminary Plat approval
Location: Lots 7, 8, 9 of Block 62, Woodside Subdivision #15
Zoning: General Residential (GR)

Notice

Notice for the public hearing was published in the Idaho Mountain Express on April 23, 2014; the notice was mailed to property owners within 300 feet on April 22, 2014. The site was posted on April 22, 2014.

Application

Tanner Investments, represented by Brant Tanner (owner) and Brian Yeager of Galena Engineering, has submitted an application for Preliminary Plat approval for the cottage townhouse sub-lot subdivision of Lot 7-9, Block 62, Woodside Subdivision #15 into 12 residential lots ranging in size from 4,260 square feet to 5,852 square feet with three parcels dedicated to public use by the residents and neighborhood association. The total land area of the subdivision is 1.78 acres.

Standards of Evaluation for a Subdivision				
Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6A.5 (B)	Complete Application
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Department Comments	<p>Engineering: - Comments not received yet.</p> <p>Life/Safety: - The current preliminary plat reflects all changes and revisions recommended and requested by the Fire Chief, Craig Aberbach and Fire Marshal, Mike Baledge. His changes included:</p> <ul style="list-style-type: none"> - Original private drive on the north end of the project was longer than 150' and did not comply with IFC standards. The applicant has corrected this in the current version of the preliminary plat by making Sunburst Lane a private thru street. <p>Wastewater: - The current preliminary plat reflects all changes and revisions recommended and requested by, Roger Parker, Wastewater Superintendent. His changes included:</p> <ul style="list-style-type: none"> o Plan shows sewer main along Winterhaven where no sewer line exists. This must be revised to accurately reflect the infrastructure. o Add plat note stating that the City shall have the right to maintain/clean the sewer lines within the subdivision on the private drives. o Sewer laterals should be positioned in the center of the private drives - The subdivision will be subject to the following inspection prior to issuance of a building permit: pressure tests, manhole vacuum tests, bedding inspections, proper piping, pipe lettering up, and proper service Y's <p>Water: - The current preliminary plat reflects all changes and revisions recommended and requested by, Cole Balis, Water Supervisor. His changes included:</p> <ul style="list-style-type: none"> ▪ Valves should be installed at the property lines at entrances to the subdivision. ▪ Hot tap or t-junctions acceptable for valve stub-outs <p>Streets: - Stop signs shall be placed at the intersections of Winterhaven Dr. and Sunburst Ln. - Sidewalks, crosswalks, ramps, shall be built according to City Standards and ADA requirements.</p>
				<p>Planning and Zoning: The following changes have been requested and are reflected on the current Preliminary</p>

				<p>Plat:</p> <ol style="list-style-type: none"> 1. Change Sunburst Drive to Sunburst Lane (or some other approve name, see 4.1.10.4) 2. Change title of plat to incorporate the words, " Townhouse Cottage Sub Lots" 3. A 10' snow storage easement must be shown on the plat to comply with 4.1.10.5. This will bring the easement into the lots adjacent to Sunburst Lane 4. NOTE: Our code prohibits naming private streets that service 5 or fewer lots, however I am recommending the street be named because it provides access to the back sides of several other lots. No change here, just wanted to let you know. 5. Two additional parking spaces per interior cottage (located along Sunburst) need two additional parking spaces per 4.1.10.6. This can be parallel parking spots, but need to be shown on the prelim plat. 6. Show all driveways on the prelim plat with widths called to meet standards of 4.1.11.1 7. Please submit a preliminary grading plan per 4.8.1.2. Show on this plan that the development will not have an adverse effect on adjoining properties in terms of drainage. Also, see 4.8.2.1 for other guidance with the grading plan 8. Parks requirements must be submitted according to the Subdivision Ordinance. Kelly will provide a list of projects in Keefer park that could qualify for the in-lieu fee contribution. The Parks and Lands Board must submit a recommendation concerning the In-Lieu fees prior to public hearing with the PZ Commission. 9. Mailboxes must be shown on the preliminary plat 10. The private drive must be named. 11. Crosswalks and stop signs must be called out on prelim plat. 12. 5' sidewalk is acceptable, but 6' is preferred. 13. Sidewalk must extend and meet up with existing sidewalks on both sides of the development. (This requires paving a sidewalk over Parcel O.)
				<p>Parks and Lands Board:</p> <p>- On July 2, 2014, the Parks and Lands Board recommended approval of the proposed fees in-lieu of park land dedication in the amount of \$41,000.000. The Parks and Lands Board did not specify how the funds should be used but will discuss this at a later meeting to formulate a recommendation for the City Council.</p> <p>- The Subdivision Ordinance states that any in-lieu fees "should be used, whenever feasible or practicable, on improvements within walking distance of the residents of the subdivision (4.10.8.3)"</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.0 General Standards	<p>The configuration and development of proposed subdivisions shall be subject to and meet the provisions and standards found in this Ordinance, the Zoning Ordinance and any other applicable Ordinance or policy of the City of Hailey, and shall be in accordance with general provisions of the Comprehensive Plan.</p>
			<i>Staff Comments</i>	<i>See specific standards below.</i>
4.1 Streets				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1	<p>Streets shall be provided in all subdivisions where necessary to provide access and shall meet all standards below.</p>
			<i>Staff Comments</i>	<i>A private street, named Sunburst Lane, is proposed to service the subdivision. Five lots(Lots 2-6) will be accessed from Sunburst Lane and seven lots (Lot 1 and Lots 7-12) will be access from Winterhaven Drive.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.1	<p>All streets in the subdivision must be platted and developed with a width, alignment, and improvements such that the street is adequate to safely accommodate existing and anticipated vehicular and pedestrian traffic and meets City standards. Streets shall be</p>

				aligned in such a manner as to provide through, safe and efficient access from and to adjacent developments and properties and shall provide for the integration of the proposed streets with the existing pattern.
			<i>Staff Comments</i>	<i>The private street has been platted as a separate, unbuildable parcel and is 36' wide. The drivable surface of the street is 20 feet and meets City Standards</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.2	<p>Cul-de-sacs or dead end streets shall be allowed only if connectivity is not possible due to surrounding topography or existing platted development. Where allowed, such cul-de-sacs or dead end streets shall comply with all regulations set forth in the IFC and other applicable codes and ordinances. Street rights-of-way extended into un-platted areas shall not be considered dead end streets.</p> <p style="text-align: center;">More than one access may be required based on the potential for impairment of a single access by vehicle congestion, terrain, climatic conditions or other factors that could limit access.</p>
			<i>Staff Comments</i>	<p>- No cul-de-sacs or dead end streets are proposed.</p> <p>- The interior lots of the subdivision are serviced from Sunburst Lane, which has two entry/egress points.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.3	<p>Streets shall be laid out so as to intersect as nearly as possible at right angles and no street shall intersect any other street at less than eighty (80) degrees. Where possible, four way intersections shall be used. A recommended distance of 500 feet, with a maximum of 750 feet, measured from the center line, shall separate any intersection. Alternatively, traffic calming measures including but not limited to speed humps, speed tables, raised intersections, traffic circles or roundabouts, meanderings, chicanes, chokers, and/or neckdowns shall be a part of the street design. Alternate traffic calming measures may be approved with a recommendation by the City Engineer. Three way intersections shall only be permitted where most appropriate or where no other configuration is possible. A minimum distance of 150 feet, measured from the center line, shall separate any two three-way intersections.</p>
			<i>Staff Comments</i>	<p>- Sunburst Lane enters Winterhave Drive at right angles in both intersections.</p> <p>- The streets are not 500 feet apart, however the current layout of the Sunburst Drive is acceptable to the Public Works Director and Street Superintendent.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.4	<p>Street center lines which deflect more than five (5) degrees shall be connected by a curve. The radius of the curve for the center line shall not be more than 500 feet for an arterial street, 166 feet for a collector street and 89 feet for a residential street. Alternatively, traffic calming measures including but not limited to speed humps, speed tables, raised intersections, traffic circles or roundabouts, meanderings, chicanes, chokers, and/or neckdowns shall be a part of the street design. Alternate traffic calming measures may be approved with a recommendation by the City Engineer.</p>
			<i>Staff Comments</i>	- Sunburst Lane is a private drive and is curved through the subdivision to service the interior lots.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.5	<p>Street width is to be measured from property line to property line. The minimum street width, unless specifically approved otherwise by the Council, shall be as specified in City Standards for the type of street.</p>
			<i>Staff Comments</i>	<p>- Private streets are required a minimum width of 36'.</p> <p>- Sunburst Lane is 36' feet wide with a drivable surface of 20' wide.</p>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.6	Roadway, for the purpose of this section, shall be defined as the area of asphalt from curb face to curb face or edge to edge. Roadway includes areas for vehicle travel and may include parallel or angle in parking areas. The width of roadways shall be in accordance with the adopted City Standards for road construction.
			<i>Staff Comments</i>	<i>- Proposed Roadway is 20' wide. According to Standard Drawing 18.14.012.F.2, the pavement width varies according to the street.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.7	Road Grades shall be at least two percent (2%) and shall not generally exceed six percent (6%). Grade may exceed 6%, where necessary, by 1% (total 7%) for no more than 300 feet or 2% (total 8%) for no more than 150 feet. No excess grade shall be located within 200 feet of any other excess grade nor there any horizontal deflection in the roadway greater than 30 degrees within 300 feet of where the excess grade decreases to a 2% slope.
			<i>Staff Comments</i>	<i>- Road grades are proposed at 2.0%.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.8	The Developer shall provide storm sewers and/or drainage areas of adequate size and number to contain any runoff within the streets in the subdivision in conformance with the applicable Federal, State and local regulations. The developer shall provide copies of state permits for shallow injection wells (drywells). Drainage plans shall be reviewed by City Staff and shall meet the approval of the City Engineer. Developer shall provide a copy of EPA's "NPDES General Permit for Stormwater Discharge from Construction Activity" for all construction activity affecting more than one acre.
			<i>Staff Comments</i>	<i>- All storm drainage for the subdivision will be contained by three proposed dry wells located on-site. - The applicant has not submitted copies of the DEQ permits for these drywells but they are required prior to final plat approval.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.9	The Developer shall provide and install all street and traffic control signs in accordance with City Standards.
			<i>Staff Comments</i>	<i>- Two stop signs are required at the intersection of Winterhaven Dr. and Sunburst Lane - The signs shall be installed according to City Standards.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.10	All streets and alleys within any subdivision shall be dedicated for public use, except as provided herein. New street names (public and private) shall not be the same or similar to any other street names used in Blaine County.
			<i>Staff Comments</i>	<i>- Sunburst Lane is a private street, however it is dedicated to public use and will not have any access restrictions.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.10.1	Private streets may be allowed (a) to serve a maximum of five (5) residential dwelling units, (b) within Planned Unit Developments, or (c) within commercial developments in the Business, Limited Business, Neighborhood Business, Light Industrial, Technological Industry, and Service Commercial Industrial districts. Private streets are allowed at the sole discretion of the Council, except that no Arterial or Major Street, or Collector or Secondary Street may be private. Private streets shall have a minimum total width of 36 feet, shall be constructed to all other applicable City Standards including paving, and shall be maintained by an owner's association.
			<i>Staff Comments</i>	<i>- Sunburst Lane services five (5) interior lots. - The parcel dedicated for the street is 36 feet wide - Sunburst Lane shall be maintained by the homeowner's association.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.10.2	Private streets, wherever possible, shall provide interconnection with other public streets and private streets.
			<i>Staff Comments</i>	<i>- Sunburst Lane is interconnected with Winterhaven Dr. via a loop to</i>

				<i>service five interior lots. Winterhaven Dr. is a public street.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.10.3	The area designated for private streets shall be platted as a separate parcel according to subsection 4.5.3 below. The plat shall clearly indicate that the parcel is unbuildable except for public vehicular and public pedestrian access and ingress/egress, utilities or as otherwise specified on the plat.
			<i>Staff Comments</i>	<i>- Sunburst Lane has been platted as Parcel A with the following plat note: Plat Note 5) Parcel A is reserved for Common Access, public utilities and Snow Storage to benefit and be maintained by Lots within this subdivision. This area is unbuildable except for ingress/egress or utilities. Costs for utilities, snow removal, onsite street maintenance, and maintenance of Parcel A shall be shared on a pro rata basis between the cottage lots.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.10.4	Private street names shall not end with the word "Road", "Boulevard", "Avenue", "Drive" or "Street". Private streets serving five (5) or fewer dwelling units shall not be named.
			<i>Staff Comments</i>	<i>- Sunburst Lane complies with the requirements or a privately owned street. - As Sunburst Lane provides access to the backsides of several lots, in addition to servicing the five interior lots, staff recommends naming the street for public safety purposes. - For public safety purposes, staff recommends naming the private street Sunburst Lane.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.10.5	Private streets shall have adequate and unencumbered 10-foot wide snow storage easements on both sides of the street, or an accessible dedicated snow storage easement representing not less than twenty-five (25%) of the improved area of the private street. Private street snow storage easements shall not be combined with, or encumber, required on-site snow storage areas.
			<i>Staff Comments</i>	<i>- The preliminary plat shows a 10' snow storage easement along the length of Sunburst Lane.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.10.6	Subdivisions with private streets shall provide two (2) additional parking spaces per dwelling unit for guest and/or overflow parking. These spaces may be located (a) within the residential lot (e.g., between the garage and the roadway), (b) as parallel spaces within the street parcel or easement adjacent to the travel lanes, (c) in a designated guest parking area, or (d) as a combination thereof. Guest/overflow parking spaces are in addition to the minimum number of parking spaces required pursuant to Article IX of the Hailey Zoning Ordinance. The dimension of guest/overflow parking spaces shall be no less than 10' by 20' if angle parking, or 10' by 24' if parallel. Guest overflow parking spaces shall be improved with asphalt, gravel, pavers, grass block, or other all-weather dustless surface. No part of any required guest/overflow parking spaces shall be utilized for snow storage.
			<i>Staff Comments</i>	<i>-Sunburst Lane services access to five interior lots, therefore 10 additional spaces are required. - More than 10 parallel parking spaces can be accommodated along Sunburst Lane.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.1.11	Driveways may provide access to not more than two (2) residential dwelling units. Where a parcel to be subdivided will have one lot fronting on a street, not more than one additional single family lot accessed by a driveway may be created in the rear of the

				subdivisions in the Townsite Overlay District, the requirement for sidewalk may be waived for any lot line adjustment associated with a residential remodel or addition; sidewalks shall be required for new primary dwellings.
			<i>Staff Comments</i>	- A five (5) foot sidewalk will be installed along the length of the subdivision adjacent to Winterhaven Dr. - The proposed sidewalk will match existing sidewalks on the north side of the project and on the south side. Both existing sidewalks are 5' in width. - The developer is required to extend the sidewalk on both sides to connect with the existing sidewalks, which will require paving a sidewalk across the City of Hailey owned parcel on the south side of the project to meet up with the existing sidewalk. The preliminary plat reflects this requirement.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.2.2	Pathways. The Developer shall install all non-vehicular pathways, to City Standards, in all areas within or adjacent to the property to be developed where Pathways are depicted upon the Master Plan. Pathways are provided from previous construction.
			<i>Staff Comments</i>	- Apart from the sidewalk, no other pathways are proposed
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.2.3	The Developer may, at Developer's option, propose alternatives to either the standard sidewalk configuration required in Section 4.2.1, or the planned non-vehicular pathway required in Section 4.2.2. The Hearing Examiner or Commission and Council shall ensure that the alternative configuration shall not reduce the level of service or convenience to either residents of the development or the public at large.
			<i>Staff Comments</i>	- No sidewalk or pathway alternative has been presented or required.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.2.4	After receiving a recommendation by the Hearing Examiner or Commission, the Council may in its discretion approve and accept voluntary cash contributions in-lieu of the improvements described in this Section 4.2, which contributions must be segregated by the City and not used for any purpose other than the provision of these improvements. The contribution amount shall be 110% of the estimated costs of concrete sidewalk and drainage improvements provided by a qualified contractor, plus associated engineering costs, as approved by the City Engineer. Any approved in-lieu contribution shall be paid before the City signs the final plat. In-lieu contributions for sidewalks shall not be accepted in Business, Limited Business, Neighborhood Business Technological Industry and Service Commercial Industrial districts.
			<i>Staff Comments</i>	- The applicant is not requesting a fee in-lieu of sidewalks but will be constructing the sidewalks along the length of the project adjacent to Winterhaven Dr.
4.3 Alleys				
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.3.1	Alleys shall be provided in all Business District and Limited Business District developments where feasible.
			<i>Staff Comments</i>	- No alleys are proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.3.2	The minimum width of an alley shall be 26 feet.
			<i>Staff Comments</i>	- No alleys are proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.3.3	All alleys shall be dedicated to the public or provide for public access.
			<i>Staff Comments</i>	- No alleys are proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.3.4	All infrastructures to be installed underground shall, where possible, be installed in the

				alleys platted.
			<i>Staff Comments</i>	- No alleys are proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.3.5	Alleys in commercial areas shall be improved with drainage as appropriate and which the design meets the approval of the City Engineer. The Developer shall provide storm sewers and/or drainage areas of adequate size and number to contain any runoff within the streets in the subdivision upon the property in conformance with the latest applicable Federal, State and local regulations. The developer shall provide copies of state permits for shallow injection wells (drywells). Drainage plans shall be reviewed by City Staff and shall meet the approval of the City Engineer.
			<i>Staff Comments</i>	- No alleys are proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.3.6	Dead-end alleys shall not be allowed..
			<i>Staff Comments</i>	- No alleys are proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.3.7	Where alleys are not provided, easements of not less than ten (10) feet in width may be required on each side of all rear and/or side lot lines (total width = 20 feet) where necessary for wires, conduits, storm or sanitary sewers, gas and water lines. Easements of greater width may be required along lines, across lots, or along boundaries, where necessary for surface drainage or for the extension of utilities.
			<i>Staff Comments</i>	- No alleys are proposed.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.3.8	Easements. Easements, defined as the use of land not having all the rights of ownership and limited to the purposes designated on the plat, shall be placed on the plat as appropriate. Plats shall show the entity to which the easement has been granted. Easements shall be provided for the following purposes:
			<i>Staff Comments</i>	- Parcel A is reserved to provide public utility access to all interior lots and services to all lots.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.3.8.1	To provide access through or to any property for the purpose of providing utilities, emergency services, public access, private access, recreation, deliveries or such other purpose. Any subdivision that borders on the Big Wood River shall dedicate a 20-foot wide fisherman's access easement, measured from the Mean High Water Mark, which shall provide for non-motorized public access. Additionally, in appropriate areas, an easement providing non-motorized public access through the subdivision to the river shall be required as a sportsman's access.
			<i>Staff Comments</i>	- Parcel A provides an access, provides a space for utilities and snow storage, and emergency access.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.3.8.2	To provide protection from or buffering for any natural resource, riparian area, hazardous area, or other limitation or amenity on, under, or over the land. Any subdivision that borders on the Big Wood River shall dedicate a one hundred (100) foot wide riparian setback easement, measured from the Mean High Water Mark, upon which no permanent structure shall be built, in order to protect the natural vegetation and wildlife along the river bank and to protect structures from damage or loss due to river bank erosion. A twenty-five (25) foot wide riparian setback easement shall be dedicated adjacent to tributaries of the Big Wood River. Removal and maintenance of live or dead vegetation within the riparian setback easement is controlled by the applicable bulk requirement of the Flood Hazard Overlay District. The riparian setback easement shall be fenced off during any construction on the property.
			<i>Staff Comments</i>	- No natural resource, riparian area, hazardous area, or other limitation requires an easement for this subdivision.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.3.8.3	To provide for the storage of snow, drainage areas or the conduct of irrigation waters. Snow storage areas shall be not less than twenty-five percent (25%) of parking, sidewalk and other circulation areas. No dimension of any snow storage area may be less than 10 feet. All snow storage areas shall be accessible and shall not be located over any above

				ground utilities, such as transformers.
			<i>Staff Comments</i>	<i>- A 10' wide snow storage easement along the length of Sunburst Lane is provided.</i>
4.4 Blocks				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.4.1	The length, width and shape of blocks shall be determined with due regard to adequate building sites suitable to the special needs of the type of use contemplated, the zoning requirements as to lot size and dimensions, the need for convenient access and safe circulation and the limitations and opportunities of topography.
			<i>Staff Comments</i>	<i>- The subdivision is comprised of one block with all lots appropriately sited to maximize the density and buildable lot size.</i>
4.5 Lots				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.5.1	All lots shown on the subdivision plat must conform to the minimum standards for lots in the District in which the subdivision is planned. The City will generally not approve single-family residential lots larger than one-half acre (21,780 square feet). In the event a single-family residential lot greater than one-half acre is platted, irrigation shall be restricted to not more than one-half acre, pursuant to Idaho Code §42-111, and such restriction shall be included as a plat note. District regulations are found in the Zoning Ordinance.
			<i>Staff Comments</i>	<i>- All lots in Sunburst Subdivision are Townhouse Cottage Sub-lots, which do not have a minimum lot size but are required to meet the density requirements of the zone, - General Residential (GR) has a maximum density of 10 lots per acre, or 0.10 acre per lot. All lots are equal to or larger than 0.10 acre. - The smallest lot is 0.10 acre (Lot 2) and the largest lot is 0.21 acre (Lot 7).</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.5.1.1	If lots are more than double the minimum size required for the zoning district, the Developer may be required to arrange lots in anticipation of future resubdivision and provide for future streets where necessary to serve potential lots, unless the plat restricts further subdivision.
			<i>Staff Comments</i>	<i>- Lots are not more than double the minimum size.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.5.2	Double frontage lots shall be prohibited except where unusual topography, a more integrated street plan, or other conditions make it undesirable to meet this requirement. Double frontage lots are those created by either public or private streets, but not by driveways or alleys. Subdivisions providing a platted parcel of 25 feet or more between any street right-of-way and any single row of lots shall not be considered to have platted double frontage lots. The 25-foot wide parcel provided must be landscaped to provide a buffer between the street and the lot(s).
			<i>Staff Comments</i>	<i>- No double frontage lots are proposed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.5.3	No unbuildable lots shall be platted. Platted areas that are not buildable shall be noted as such and designated as "parcels" on the plat. Green Space shall be clearly designated as such on the plat.
			<i>Staff Comments</i>	<i>- All lots are buildable. - Parcel A is shown as a private street dedicated for public access and public utilities.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.5.4	A single flag lot may be permitted at the sole discretion of the Hearing Examiner or Commission and Council, in which the "flagpole" projection is serving as a driveway as provided herein, providing connection to and frontage on a public or a private street. Once established, a flag lot may not be further subdivided, but a lot line adjustment of a

				<p>flag lot is not considered a further subdivision. The “flagpole” portion of the lot shall be included in lot area, but shall not be considered in determining minimum lot width. The “flagpole” shall be of adequate width to accommodate a driveway as required by this ordinance, fire and other applicable codes. Flag lots within the Townsite Overlay District are not allowed, except where parcels do not have street access, such as parcels adjacent to the ITD right-of-way.</p> <p><i>Staff Comments</i> - Lot 3 is the only flag lot proposed in Sunburst Hills.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.5.5	<p>All lots shall have frontage on a public or private street. No frontage width shall be less than the required width of a driveway as provided under Sections 4.1.11.1 and 4.5.4 of this Ordinance. Townhouse Sub-Lots are excluded from this requirement; provided, however, that Townhouse Developments shall have frontage on a street.</p> <p><i>Staff Comments</i> - All lots have frontage on either Winterhaven Dr. or Sunburst Lane. - Lots 1, 7, 8, 9, 10, 11, and 12 have frontage on Winterhaven Dr. - Lots 2, 3, 4, 5, and 6 have frontage on Sunburst Lane.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.5.6	<p>In the Townsite Overlay District, original Townsite lots shall be subdivided such that the new platted lots are oriented the same as the original lots, i.e. lots shall be subdivided in such a way as to maintain frontage on both the street and alley. Exceptions may be made for corner properties with historic structures.</p> <p><i>Staff Comments</i> - Project is not located in the Townsite Overlay.</p>
4.6 Orderly Development				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.6.1	<p>Development of subdivisions shall be phased to avoid the extension of City services, roads and utilities through undeveloped land.</p> <p><i>Staff Comments</i> - All city services are existing along Winterhaven Dr. and any extension will be the responsibility of the Developer.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.6.2	<p>Developers requesting phased subdivisions shall enter into a phasing agreement with the City. Any phasing agreement shall be approved and executed by the Council and the Developer on or before the preliminary plat approval by the Council.</p> <p><i>Staff Comments</i> - No phasing is requested for Sunburst Hills.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.6.3	<p>No subdivision shall be approved which affects the ability of political subdivisions of the state, including school districts, to deliver services without compromising quality of service delivery to current residents or imposing substantial additional public costs upon current residents, unless the Developer provides for the mitigation of the effects of subdivision. Such mitigation may include, but is not limited to the following:</p> <ul style="list-style-type: none"> • Provision of on-site or off-site street or intersection improvements. • Provision of other off-site improvements. • Dedications and/or public improvements on property frontages. • Dedication or provision of parks or green space. • Provision of public service facilities. • Construction of flood control canals or devices. • Provisions for ongoing maintenance. <p><i>Staff Comments</i> - Sunburst Hills does not affect the ability of political subdivisions of the state to deliver services.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.6.4	<p>When the Developer of Contiguous Parcels proposes to subdivide any portion of the Contiguous Parcels, an Area Development Plan shall be submitted and approved. The Commission and Council shall evaluate the following basic site criteria and make appropriate findings of fact:</p> <p style="text-align: center;">a) Streets, whether public or private, shall provide an</p>

				<p>interconnected system and shall be adequate to accommodate anticipated vehicular and pedestrian traffic.</p> <p>b) Non-vehicular circulation routes shall provide safe pedestrian and bicycle ways and provide an interconnected system to streets, parks and green space, public lands, or other destinations.</p> <p>c) Water main lines and sewer main lines shall be designed in the most effective layout feasible.</p> <p>d) Other utilities including power, telephone, cable, and gas shall be designed in the most effective layout feasible.</p> <p>e) Park land shall be most appropriately located on the Contiguous Parcels.</p> <p>f) Grading and drainage shall be appropriate to the Contiguous Parcels.</p> <p>g) Development shall avoid easements and hazardous or sensitive natural resource areas.</p> <p>The Commission and Council may require that any or all Contiguous Parcels be included in the subdivision.</p>
			<i>Staff Comments</i>	<i>- Sunburst Hills does not include any phasing that impacts any contiguous or adjacent parcels and is not a phased project.</i>
4.7 Perimeter Walls, Gates and Berms				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.7	The City of Hailey shall not approve any residential subdivision application that includes any type of perimeter wall or gate that restricts access to the subdivision. This regulation does not prohibit fences on or around individual lots. The City shall also not allow any perimeter landscape berm more than 3' higher than the previously existing (original) grade.
			<i>Staff Comments</i>	<i>- No walls or gates are proposed. - No perimeter landscape berms are proposed</i>
4.8 Cuts, Fills, Grading and Drainage.				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.8.1	Proposed subdivisions shall be carefully planned to be compatible with natural topography, soil conditions, geology and hydrology of the site, as well as to minimize cuts; fills, alterations of topography, streams, drainage channels; and disruption of soils or vegetation. Fill within the floodplain shall comply with the requirements of the Flood Hazard Overlay District of the Zoning Ordinance.
			<i>Staff Comments</i>	<i>- The subdivision has been designed to retain all storm water on site utilizing three dry wells. - Some cut and fill will be necessary to develop the site, however the project is not located in a floodplain and no streams or drainage channels will be disrupted.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.8.1.1	A preliminary soil report prepared by a qualified engineer may be required by the Hearing Examiner or Commission and/or Council as part of the preliminary plat application.
			<i>Staff Comments</i>	<i>- A soil report has not been required because the site contains no known hazards and has not been developed at any time in the past.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.8.1.2	A preliminary grading plan prepared by a civil engineer may be required by the Hearing

			<p>Examiner or Commission and/or the Council as part of the preliminary plat application, to contain the following information:</p> <p style="text-align: center;"> Proposed contours at a maximum of two (2) foot contour intervals; Cut and fill banks in pad elevations; Drainage patterns; Areas where trees and/or natural vegetation will be preserved; Location of all street and utility improvements including driveways to building envelopes; and Any other information which may reasonably be required by the Administrator, Hearing Examiner, Commission and/or Council. </p>
			<p><i>Staff Comments</i></p> <p>- A preliminary grading plan has been submitted and is acceptable.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>4.8.2.1</p> <p>Grading shall be designed to blend with natural land forms and to minimize the necessity of padding or terracing of building sites, excavation for foundations, and minimize the necessity of cuts and fills for streets and driveways.</p> <p style="text-align: center;">Grading design will conform to the requirements.</p>
			<p><i>Staff Comments</i></p> <p>- The grading plan, as submitted minimizes necessary cuts and blends with the existing natural land forms.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>4.8.2.2</p> <p>Areas within a subdivision which are not well suited for development because of existing soil conditions, steepness of slope, geology or hydrology shall be allocated for Green Space for the benefit of future property owners within the subdivision.</p>
			<p><i>Staff Comments</i></p> <p>- Even though the site is not flat, all areas within the subdivision are suitable for development.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>4.8.2.3</p> <p>Where existing soils and vegetation are disrupted by subdivision development, provision shall be made by the Developer for Revegetation of disturbed areas with perennial vegetation sufficient to stabilize the soil upon completion of the construction, including temporary irrigation for a sufficient period to establish perennial vegetation. Until such time as the vegetation has been installed and established, the Developer shall maintain and protect all disturbed surfaces from erosion.</p>
			<p><i>Staff Comments</i></p> <p>- The applicant is hereby advised that all restoration of the site is the responsibility of the developer.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>4.8.2.4</p> <p>Where cuts, fills or other excavation are necessary, the following development standards shall apply:</p>
			<p><i>Staff Comments</i></p> <p>- The submitted grading plan is in compliance with the standards listed below.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>4.8.4.1</p> <p>Fill areas for structures or roads shall be prepared by removing all organic material detrimental to proper compaction for soil stability.</p>
			<p><i>Staff Comments</i></p> <p>- The submitted grading plan complies with this standard, although an on-site inspection by the City Engineer is recommended to verify the standard.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>4.8.2.4.2</p> <p>Fill for structures or roads shall be compacted to at least 95 percent of maximum density as determined by American Association State Highway Transportation Officials (AASHTO) and American Society of Testing & Materials (ASTM).</p>
			<p><i>Staff Comments</i></p> <p>- The submitted grading plan complies with this standard, although an on-site inspection by the City Engineer is recommended to verify the standard.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>4.8.2.4.3</p> <p>Cut slopes shall be no steeper than two horizontal to one vertical. Subsurface drainage shall be provided as necessary for stability.</p>

			<i>Staff Comments</i>	<i>- The submitted grading plan complies with this standard, although an on-site inspection by the City Engineer is recommended to verify the standard.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.8.2.4.4	Fill slopes shall be no steeper than three horizontal to one vertical. Neither cut nor fill slopes shall be located on natural slopes of three to one or steeper, or where fill slope toes out within twelve (12) feet horizontally of the top of existing or planned cut slope.
			<i>Staff Comments</i>	<i>- The submitted grading plan complies with this standard, although an on-site inspection by the City Engineer is recommended to verify the standard.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.8.2.4.5	Tops and toes of cut and fill slopes shall be set back from structures and property lines as necessary to accommodate drainage features and drainage structures.
			<i>Staff Comments</i>	<i>- The submitted grading plan complies with this standard, although an on-site inspection by the City Engineer is recommended to verify the standard.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.8.2.5	The Developer shall provide storm sewers and/or drainage areas of adequate size and number to contain the runoff upon the property in conformance with the applicable Federal, State and local regulations. The Developer shall provide copies of state permits for shallow injection wells (drywells). Drainage plans shall be reviewed by Planning Staff and shall meet the approval of the City Engineer. Developer shall provide a copy of EPA's "NPDES General Permit for Stormwater Discharge from Construction Activity" for all construction activity affecting more than one acre.
			<i>Staff Comments</i>	<i>- All storm drainage for the subdivision will be contained by three proposed dry wells located on-site. - The applicant has not submitted copies of the DEQ permits for these drywells but they are required prior to final plat approval.</i>
4.9 Overlay Districts				
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.9.1	Flood Hazard Overlay District
			<i>Staff Comments</i>	<i>- Project is not located in a Flood Hazard Overlay District</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.9.1.1	Subdivisions or portions of subdivision located within the Flood Hazard Overlay District shall comply with all provisions of Section 4.10 of the Zoning Ordinance.
			<i>Staff Comments</i>	<i>- Project is not located in a Flood Hazard Overlay District</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.9.1.2	Subdivisions located partially in the Flood Hazard Overlay District shall have designated building envelopes outside the Flood Hazard Overlay District to the extent possible.
			<i>Staff Comments</i>	<i>- Project is not located in a Flood Hazard Overlay District</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.9.1.3	Any platted lots adjacent to the Big Wood River or its tributaries shall have designated building envelopes.
			<i>Staff Comments</i>	<i>- Project is not located adjacent to the Big Wood River or any of its tributaries.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.9.2	Hillside Overlay District
			<i>Staff Comments</i>	<i>- Project is not located in the Hillside Overlay District.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.9.2.1	Subdivisions or portions of subdivisions located within the Hillside Overlay District shall comply with all provisions of Section 4.14, of the Zoning Ordinance.
			<i>Staff Comments</i>	<i>- Project is not located in the Hillside Overlay District.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.9.2.2	Subdivisions located partially in the Hillside Overlay District shall have designated building envelopes outside the Hillside Overlay District.
			<i>Staff Comments</i>	<i>- Project is not located in the Hillside Overlay District.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.9.2.3	All approved subdivisions shall contain a condition that a Site Alteration Permit is

				required before any development occurs.
			Staff Comments	- The developer shall obtain a Site Alteration Permit prior to any development occurring.
4.10 Parks, Pathways and Other Green Spaces.				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.10.1	Parks and Pathways. Unless otherwise provided, every subdivision shall set aside a Park and/or Pathway(s) in accordance with standards set forth herein.
			Staff Comments	The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.10.1.1	<p>Parks. The Developer of any subdivision, or any part thereof, consisting of three (3) or more residential lots, including residential townhouse sub-lots and residential condominium units, without regard to the number of phases within the subdivision, shall set aside or acquire land area within, adjacent to or in the general vicinity of the subdivision for Parks. Parks shall be developed within the City of Hailey and set aside in accordance with the following formula:</p> <p style="text-align: center;">P = x multiplied by .0277</p> <p style="text-align: center;">“P” is the Parks contribution in acres “x” is the number of single family lots, residential townhouse sub-lots or residential condominium units contained within the plat. Where multi-family lots are being platted with no fixed number of units, “x” is maximum number of residential lots, sub-lots, and units possible within the subdivision based on current zoning regulations</p> <p>In the event the subdivision is located in the Business (B), Limited Business (LB), Neighborhood Business (NB), or Transitional (TN) zoning districts, the area required for a Park shall be reduced by 75%, but in no event shall the area required for a Park/Cultural Space exceed 17.5% of the area of the lot(s) being developed.</p>
			Staff Comments	<p>This subdivision, Sunburst Hills, is located in the GR Zoning District, therefore the park requirement of .0277 acres per lot applies. This subdivision proposes 12 lots, resulting in 0.33 acres:</p> <p style="text-align: center;">Parks Contribution in Acres (P) = 12 (lots) X .0277 P = .33 acres required</p> <p>However, the total area proposed for subdivision is 1.78 acres or 76,143. According to 4.10.1.1, the maximum required park space shall not exceed 17.5% of the total area proposed for development. In this case, 17.5% of 76,143 square feet is 13,325 square feet or .31 acre. <u>Therefore, .31 acre is required for park dedication</u>, either as land dedication or through a fee paid in lieu of land dedication.</p> <p>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.1.2	Pathways. The Developer of any subdivision, or any part thereof, shall provide Pathways for all trails and paths identified in the Master Plan that are located on the property to be subdivided or on City property adjacent to the property to be subdivided, and sidewalks

				required by this ordinance.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.2	<p>Multiple Ownership. Where a parcel of land is owned or otherwise controlled, in any manner, directly or indirectly,</p> <p style="margin-left: 40px;">a. by the same individual(s) or entity(ies), including but not limited to corporation(s), partnership(s), limited liability company(ies) or trust(s), or</p> <p style="margin-left: 40px;">b. by different individuals or entities, including but not limited to corporations, partnerships, limited liability companies or trusts where a) such individual(s) or entity(ies) have a controlling ownership or contractual right with the other individual(s) or entity(ies), or b) the same individual(s) or entity(ies) act in any manner as an employee, owner, partner, agent, stockholder, director, member, officer or trustee of the entity(ies),</p> <p style="margin-left: 40px;">multiple subdivisions of the parcel that cumulatively result in three (3) or more residential lots, townhouse sub-lots or condominium units, are subject to the provisions of this ordinance, and shall provide the required improvements subject to the required standards at or before the platting or development of the lots, sub-lots or units.</p>
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	4.10.3	<p>Parks and Lands Board. The Parks and Lands Board shall review and make a recommendation to the Hearing Examiner or Commission and Council regarding each application subject to the provisions of Section 4.10 of this ordinance. Such recommendation will be based on compliance with the Master Plan and provisions of this ordinance.</p>
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i> - <i>The Parks and Lands Board will address the applicant's request at their July meeting.</i>
			4.10.4	Minimum Requirements
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.4.a	<p>Private Green Space. Use and maintenance of any privately owned Green Space shall be controlled by recorded covenants or restrictions which run with the land in favor of the future owners of the property within the tract and which cannot be modified without the consent of the Council.</p>
			<i>Staff Comments</i>	- <i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.4.b	<p>Neighborhood Park. A Neighborhood Park shall include finished grading and ground cover, large grassy areas, trees and shrubs, sheltered picnic table(s), trash container(s), dog station(s), bike racks, park bench(es), parking as required by ordinance, and two or more of the following: play structure, restrooms, an athletic field, trails, hard surface multiple use court (tennis or basketball courts), or gardens that demonstrate conservation principles. Neighborhood Parks shall provide an average of 15 trees per acre, of which at least 15% shall be of 4" caliper or greater. A maximum of 20% of any single tree species may be used. Landscaping and irrigation shall integrate water conservation. A Neighborhood Park shall be deeded to the City upon completion, unless otherwise agreed upon by the Developer and City.</p>

			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.4.c	Mini Park. A Mini Park shall include finished grading and ground cover, trees and shrubs, picnic table(s), trash container(s), dog station(s), bike racks and park bench(es). All Mini Parks shall provide an average of 15 trees per acre, of which at least 15% shall be of 4" caliper or greater. A maximum of 20% of any single tree species may be used. Landscaping and irrigation shall integrate water conservation.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.4.d	Park/Cultural Space. A Park/Cultural Space shall include benches, planters, trees, public art, water features and other elements that would create a gathering place. Connective elements, such as parkways or enhanced sidewalks may also qualify where such elements connect two or more Parks or Park/Cultural Spaces.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.4.e	Pathway. Pathways shall have a minimum twenty foot (20') right-of-way width and shall be paved or improved as recommended by the Parks and Lands Board. Construction of Pathways shall be undertaken at the same time as other public improvements are installed within the development, unless the Council otherwise allows when deemed beneficial for the project. The Developer shall be entitled to receive a Park dedication credit only if the Developer completes and constructs a Pathway identified in the Master Plan, or completes and constructs a Pathway not identified in the Master Plan where the Pathway connects to existing or proposed trails identified in the Master Plan. The City may permit easements to be granted by Developers for Pathways identified in the Master Plan, thereby allowing the Developer to include the land area in the determination of setbacks and building density on the site, but in such cases, a Park dedication credit will not be given. A Developer is entitled to receive a credit against any area required for a Park for every square foot of qualified dedicated Pathway right-of-way
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.5	Specific Park Standards. All Parks shall meet the following criteria for development, location and size (unless unusual conditions exist that prohibit meeting one or more of the criteria):
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.5.1	Shall meet the minimum applicable requirements required by Section 4.10.4.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.5.2	Shall provide safe and convenient access, including ADA standards.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.5.3	Shall not be gated so as to restrict access and shall not be configured in such a manner that will create a perception of intruding on private space. If a Park is privately owned and maintained, the use of the park shall not be exclusive to the homeowners, residents or employees of the development.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.5.4	Shall be configured in size, shape, topography and improvements to be functional for the intended users. To be eligible for Park dedication, the land must, at a minimum, be located on slopes less than 25 degrees, and outside of drainways, floodways and wetland areas. Mini Parks shall not be occupied by non-recreational buildings and shall be

				available for the use of all the residents or employees of the proposed subdivision.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.5.5	Shall not create undue negative impact on adjacent properties and shall be buffered from conflicting land uses.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.5.6	Shall require low maintenance, or provide for maintenance or maintenance endowment.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.6	Specific Pathway Standards. All Pathways shall meet the following criteria for development, location and size (unless unusual conditions exist that prohibit meeting one or more of the criteria):
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.6.1	Shall meet the minimum applicable requirements required by Section 4.10.4.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.6.2	Shall be connected in a useful manner to other Parks, Pathways, Green Space and recreation and community assets.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.7	Specific Green Space Standards. If green space is required or offered as part of a subdivision, townhouse or condominium development, all green space shall meet the following criteria for development, location and size (unless unusual conditions exist that prohibit meeting one or more of the criteria):
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.7.1	Shall meet the minimum applicable requirements required by Section 4.10.4.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.7.2	Public and private green spaces on the same property or adjacent properties shall be complementary to one another. Green space within proposed developments shall be designed to be contiguous and interconnecting with any adjacent Green Space (both existing and potential future space).
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.7.3	The use of the private green space shall be restricted to Parks, Pathways, trails or other recreational purposes, unless otherwise allowed by the City.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	4.10.7.4	The private ownership and maintenance of green space shall be adequately provided for by written agreement.
			<i>Staff Comments</i>	<i>The applicant is requesting to pay a fee in lieu of park land dedication, which is addressed in Section 4.10.8 below.</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>4.10.8</p> <p><i>Staff Comments</i></p>	<p><u>In-Lieu Contributions.</u></p> <p><i>This subdivision, Sunburst Hills, is located in the GR Zoning District, therefore the park requirement of .0277 acres per lot applies. This subdivision proposes 12 lots, resulting in 0.33 acres:</i></p> <p style="text-align: center;"><i>Parks Contribution in Acres (P) = 12 (lots) X .0277</i> <i>P = .33 acres required</i></p> <p><i>However, the total area proposed for subdivision is 1.78 acres or 76,143. According to 4.10.1.1, the maximum required park space shall not exceed 17.5% of the total area proposed for development. In this case, 17.5% of 76,143 square feet is 13,325 square feet or .31 acre. Therefore, .31 acre is required for park dedication, either as land dedication or through a fee paid in lieu of land dedication.</i></p> <p style="text-align: center;"><i>- Required Park Dedication: 0.31 Acre</i></p> <p><i>The applicant is requesting to pay a fee in lieu of park land dedication.</i></p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<p>4.10.8.1</p> <p><i>Staff Comments</i></p>	<p>After receiving a recommendation by the Parks and Lands Board, the Council may at their discretion approve and accept voluntary cash contributions in lieu of Park land dedication and Park improvements.</p> <p><i>- On July 1, 2014, the Parks and Lands Board recommended a fee in lieu of park dedication in the amount of \$41,000, which was calculated according Section 4.10.8 of the Subdivision Ordinance.</i></p> <p><i>- Recommended In-Lieu Fee: \$41,000.000</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>4.10.8.2</p> <p><i>Staff Comments</i></p>	<p>The voluntary cash contributions in lieu of Park land shall be equivalent to the area of land (e.g., square footage) required to be dedicated under this ordinance multiplied by the fair market value of the land (e.g., \$/square foot) in the development at the time of preliminary plat approval by the Council. The City shall identify the location of the property to be appraised, using the standards in Sections 4.10.5.4 and 4.10.5.5 of this ordinance. The appraisal shall be submitted by a mutually agreed upon appraiser and paid for by the applicant.</p> <p><i>- The location identified to be appraised is the subject project for the proposed subdivision, comprising Lots 7-9, Block 62, Woodside Subdivision #15. These lots meet the criteria addressed , 4.10.5.4 and 4.10.5.5 and were recently appraised by the applicant.</i></p> <p><i>- The appraiser submitted by the applicant is Chandler Appraisal, represented by Lois Chandler. The appraisal submitted is acceptable to the Administrator and has been paid for by the applicant.</i></p> <p><i>- The property was appraised at \$2.59/square foot or \$112,820 per</i></p>

				<p>acre.</p> <p>- The appraisal for .31 acre, or 13,325 square feet, is \$34,511.75.</p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	4.10.8.3	<p>Except as otherwise provided, the voluntary cash contribution in lieu of Park land shall also include the cost for Park improvements, including all costs of acquisition, construction and all related costs. The cost for such improvements shall be based upon the estimated costs provided by a qualified contractor and/or vendor. In the Business (B), Limited Business (LB), Neighborhood Business (NB) and Transitional (TN) zoning districts, in-lieu contributions will not include the cost for Park improvements.</p>
			<i>Staff Comments</i>	<p>- The applicant proposes an in-lieu fee of \$34,511.75 for the real estate valuation. This amount does not include the costs of any improvements as required by 4.10.4.b.</p> <p>- According to 4.10.4.c, this project best meets the criteria of a Mini Park. Mini Parks are required to provide the following minimum amenities:</p> <ol style="list-style-type: none"> 1. Finished grading and ground cover 2. Trees and shrubs 3. Picnic table 4. Trash container 5. Dog station 6. Bike rack(s) 7. Park bench(es) 8. At least 5 tree of 4" caliper (15 X .31 acre= 4.65 or ~5) 9. Landscaping and Irrigation <p>- The applicant has submitted a list of costs that account for each of the above amenities. The total costs for amenities are \$6,100.00.</p> <p>- Total costs proposed for In-lieu fee consideration is \$40,611.75.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4.10.8.4	<p>In-lieu contributions must be segregated by the City and not used for any other purpose other than the acquisition of Park land and/or Park improvements, which may include upgrades and replacement of Park improvements. Such funds should be used, whenever feasible or practicable, on improvements within walking distance of the residents of the subdivision.</p>
			<i>Staff Comments</i>	<p>All fees paid in lieu of park dedication requirements will be segregated by the City Treasurer as required.</p>
5.0 Improvements Required.				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.1	<p>It shall be a requirement of the Developer to construct the minimum infrastructure improvements set forth herein and any required infrastructure improvements for the subdivision, all to City Standards and procedures, set forth in Title 18 of the Hailey Municipal Code and adopted by ordinance in accordance with the notice and hearing procedures provided in Idaho Code §67-6509. Alternatives to the minimum improvement standards may be recommended for approval by the City Engineer and approved by the City Council at its sole discretion only upon showing that the alternative is clearly superior in design and effectiveness and will promote the public health, safety and general welfare.</p>
			<i>Staff Comments</i>	<p>- The plat shows the minimum requirements will be constructed.</p>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.1.1	Six (6) copies of all improvement plans shall be filed with the City Engineer and made available to each department head. Upon final approval two (2) sets of revised plans shall be returned to the Developer at the pre-construction conference with the City Engineer's written approval thereon. One set of final plans shall be on-site at all times for inspection purposes and to note all field changes upon.
			<i>Staff Comments</i>	<i>- Upon approval, six (6) copies of all plans will be filed with the City Engineer. All other requirements of this section will be enforced by the City Engineer or designee.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.1.2	Prior to the start of any construction, it shall be required that a pre-construction meeting be conducted with the Developer or his authorized representative/engineer, the contractor, the City Engineer and appropriate City departments. An approved set of plans shall be provided to the Developer and contractor at or shortly after this meeting.
			<i>Staff Comments</i>	<i>- Pre-construction meetings will be scheduled prior to an construction by the City Engineer or designee.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.1.3	The Developer shall guarantee all improvements pursuant to this Section for no less than one year from the date of approval of all improvements as complete and satisfactory by the City Engineer, except that parks shall be guaranteed and maintained by the Developer for a period of two years.
			<i>Staff Comments</i>	<i>- The developer is hereby required to guarantee all improvement pursuant to this Section for no less than one year from the date of approval of all improvements as complete and satisfactory by the City Engineer.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.2	The Developer shall construct all streets, alleys, curb and gutter, lighting, sidewalks, street trees and landscaping, and irrigation systems to meet City Standards, the requirements of this ordinance, the approval of the Council, and to the finished grades which have been officially approved by the City Engineer as shown upon approved plans and profiles. The Developer shall pave all streets and alleys with an asphalt plant-mix, and shall chip-seal streets and alleys within one year of construction.
			<i>Staff Comments</i>	<i>- The developer is hereby advised that all infrastructures shall be constructed according to City Standards and is subject to inspection by the City of Hailey at any time. - If any infrastructures are deemed insufficient, the Developer shall replace and/or repair them solely at their own cost to meet City Standards.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.2.1	Street cuts made for the installation of services under any existing improved public street shall be repaired in a manner which shall satisfy the Street Superintendent, shall have been approved by the Hailey City Engineer or his authorized representative, and shall meet City Standards. Repair may include patching, skim coats of asphalt or, if the total area of asphalt removed exceeds 25% of the street area, the complete removal and replacement of all paving adjacent to the development. Street cut repairs shall also be guaranteed for no less than one year.
			<i>Staff Comments</i>	<i>- Two street cuts are required to provide sewer service to Sunburst Hills Subdivision. These street cuts shall meet City Standards and are subject to inspection by the City of Hailey at any time.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.2.2	Street name signs and traffic control signs shall be erected by the Developer in accordance with City Standard, and the street name signs and traffic control signs shall thereafter be maintained by the City.
			<i>Staff Comments</i>	<i>- Street signs shall be constructed according to City Standards and shall be subject to inspection at any time by the City of Hailey</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.2.3	Street lights in the Recreational Green Belt, Limited Residential, General Residential, and Transitional zoning districts are not required improvements. Where proposed, street lighting in all zoning districts shall meet all requirements of Chapter VIII B of the Hailey Zoning Ordinance.

			<i>Staff Comments</i>	- Sunburst Hills is located in the General Residential zoning district, therefore not streetlights are required. - No street lights are proposed Sunburst Hills Subdivision.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.3	The Developer shall construct a municipal sanitary sewer connection for each and every developable lot within the development. The Developer shall provide sewer mains of adequate size and configuration in accordance with City standards, and all federal, state, and local regulations. Such mains shall provide wastewater flow throughout the development. All sewer plans shall be submitted to the City Engineer for review and approval. At the City Engineer's discretion, plans may be required to be submitted to the Idaho Department of Environmental Quality (DEQ) for review and comments.
			<i>Staff Comments</i>	- All 12 lots have sewer service dedicated for the dwelling unit. - Sewer plans have been reviewed by the Wastewater Superintendent and have been approved.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.4	The Developer shall construct a municipal potable water connection, water meter and water meter vault in accordance with City Standards, or other equipment as may be approved by the City Engineer, for each and every developable lot within the development. The Developer shall provide water mains and services of adequate size and configuration in accordance with City Standards, and all federal, state, and local regulations. Such water connection shall provide all necessary appurtenances for fire protection, including fire hydrants, which shall be located in accordance with the IFC and under the approval of the Hailey Fire Chief. All water plans shall be submitted to the City Engineer for review and approval. At the City Engineer's discretion, plans may be required to be submitted to the Idaho Department of Environmental Quality (DEQ) for review and comments.
			<i>Staff Comments</i>	- All 12 lots have separate water service connections dedicated for the dwelling unit. - All water infrastructure plans have been reviewed by the Water Superintendent and the Fire Chief. These plans have been approved.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.4.1	Within the Townsite Overlay District, where water main lines within the alley are less than six (6) feet deep, the developer shall install insulating material (blue board insulation or similar material) for each and every individual water service line and main line between and including the subject property and the nearest public street, as recommended by the City Engineer.
			<i>Staff Comments</i>	- Project is not within the Townsite Overlay
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.5	The Developer shall provide drainage areas of adequate size and number to meet the approval of the Street Superintendent and the City Engineer or his authorized representative.
			<i>Staff Comments</i>	- All storm drainage for the subdivision will be contained by three proposed dry wells located on-site and meet City Standards.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.6	The Developer shall construct each and every individual service connection and all necessary trunk lines, and/or conduits for those improvements, for natural gas, electricity, telephone, and cable television to the property line before placing base gravel for the street or alley.
			<i>Staff Comments</i>	- All service connections have been reviewed and approved.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.7	The Developer shall improve all parks and Green Space areas as presented to and approved by the Hearing Examiner or Commission and Council.
			<i>Staff Comments</i>	- Developer is requesting to pay a fee in lieu of park land dedication.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.8	All improvements are to be installed under the specifications and inspection of the City Engineer or his authorized representative. The minimum construction requirements shall meet City Standards or the Department of Environmental Quality (DEQ) standards,

				whichever is the more stringent.
			<i>Staff Comments</i>	<i>- The developer is hereby advised that all improvements shall be installed according to City Standards and are subject to inspection at any time. If improvements are not satisfactory to the City Engineer or his designee, the developer will be required to repair or replace them at their own cost.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.9	Installation of all infrastructure improvements must be completed by the Developer, and inspected and accepted by the City prior to signature of the plat by City representatives, or according to a phasing agreement. A post-construction conference shall be requested by the Developer and/or contractor and conducted with the Developer and/or contractor, the City Engineer, and appropriate City departments to determine a punch list of items for final acceptance.
			<i>Staff Comments</i>	<i>- The developer is hereby advised that all improvements shall be installed according to City Standards and are subject to inspection at any time. If improvements are not satisfactory to the City Engineer or his designee, the developer will be required to repair or replace them at their own cost.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.9.1	The Developer may, in lieu of actual construction, provide to the City security pursuant to Section 3.3.7, for all infrastructure improvements to be completed by Developer after the final plat has been signed by City representatives.
			<i>Staff Comments</i>	<i>- Developer is requesting to pay a fee in lieu of park land dedication.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5.10	Prior to the acceptance by the City of any improvements installed by the Developer, three (3) sets of "as-built plans and specifications" certified by the Developer's engineer shall be filed with the City Engineer
			<i>Staff Comments</i>	<i>- Developer is hereby advised that three (3) sets of "as-built plans and specifications" certified by the Developer's engineer shall be filed with the City Engineer prior to acceptance by the City of Hailey.</i>

Title 18: Mobility Design Ordinance Requirements				
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.010 Street Classifications, Types, and Designations	
			<i>Staff Comments</i>	<i>- Winterhaven is classified as a Residential Local, therefore all requirements for Residential Local streets shall apply.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.012 Street Design and Guideline Standards	
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - Residential/Local streets require the following standards: <ul style="list-style-type: none"> o Sidewalk Zone (includes curb, buffer, ped and frontage zones): <ul style="list-style-type: none"> ▪ 11ft total width with a minimum 5 ft pedestrian zone o Bicycle Facilities: <ul style="list-style-type: none"> ▪ 10-12 ft shared lane with Sharrow o Parking: <ul style="list-style-type: none"> ▪ May vary based on needs or neighborhood and ROW width o Drainage

				<ul style="list-style-type: none"> ▪ <i>Natural Swale</i> - <i>The plans show a sidewalk zone of 17 feet from edge of asphalt to the property line.</i> - <i>A sidewalk width of 5 feet is shown, offset from the property line by 2 feet.</i> - <i>Winterhaven lanes will be shared by bicyclists.</i> - <i>Applicant is advised that all Sharrow markings required will be charged to the developer and must be paid prior to issuance of a building permit for the project.</i> - <i>Parallel parking is proposed, consistent with the residential character of the street and neighborhood.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.016 Traffic Calming	
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Stop signs are required at the two intersections of Sunburst Hills and Winterhaven Dr.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.022 Pedestrian Facility Design Guidelines and Standards	
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>The proposed 5' sidewalk is located 10.5' from the end of asphalt on Winterhaven Dr. providing a safe buffer between vehicular traffic and pedestrian traffic.</i> - <i>All curb ramps will be designed to meet current ADA standards.</i> - <i>The sidewalk cross section shown on the preliminary plat is acceptable and meets standards.</i> - <i>Staff is working with applicant to install audible warning pads to comply with ADA standards and will be acceptable to the City of Hailey.</i> - <i>The pedestrian crossings at the intersections of Sunburst Ln and Winterhaven Dr. will be striped according to City Standards and ADA requirements.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	18.06.024 Bicycle Facility Design Guidelines and Standards	
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>For this street classification, the street lanes can be shared and used for bicycle traffic, as well as the sidewalk.</i> - <i>The sidewalk is not the preferred location for bicycle traffic, but acceptable when pedestrians are not present.</i> - <i>The number of Sharrow markings required shall be determined by the Public Works Department, according to the most current version of MUTCD Standards</i> - <i>Sharrow Markings are required and shall be paid by the developer prior to issuance of a building permit for the project.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.26 Street Tree Guidelines and Standards	
			<i>Staff Comments</i>	<ul style="list-style-type: none"> - <i>Street trees are not required for Residential/Local streets.</i>

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	18.06.028 Streetscape Elements Guidelines and Standards
		<i>Staff Comments</i>	- <i>Streetscape elements are not required for Residential/Local streets.</i>

Summary and Suggested Conditions

The Commission shall review the proposed plat and continue the public hearing, approve, conditionally approve, or deny the preliminary plat. If approved, the plat application will be forwarded to the Council. If the Short Plat process is used, only the Final Plat is required for Council review.

The following conditions are suggested to be placed on any approval of this application:

- a) All Fire Department and Building Department requirements shall be met. Items to be completed at the applicant’s sole expense include, but will not be limited to, the following requirements and improvements:
- b) All City infrastructure requirements shall be met as outlined in Section 5 of the Hailey Subdivision Ordinance. Detailed plans for all infrastructure to be installed or improved at or adjacent to the site shall be submitted for Department approval and shall meet City Standards where required. Infrastructure to be completed at the applicant’s sole expense include, but will not be limited to, the following requirements and improvements:
- c) The final plat shall include plat notes 1 through 9 as stated on the approved preliminary plat [with the following amendments and additions: if applicable]
- d) Issuance of permits for the construction of buildings within the proposed subdivision shall be subject to Section 2.9 of the Subdivision Ordinance.
- e) All improvements and other requirements shall be completed and accepted, or surety provided pursuant to Sections 3.3.7 and 5.9.1 of the Subdivision Ordinance, prior to recordation of the final plat.
- f) The final plat must be submitted within one (1) calendar year from the date of approval of the preliminary plat, unless otherwise allowed for within a phasing agreement.
- g) Any subdivision inspection fees due shall be paid prior to recording the final plat.
- h) In-lieu of dedicated a park as required by Section 4.10 of the Subdivision Ordinance, the applicant shall pay \$41,000 to the City of Hailey, according to Section 4.10.8. The in-lieu fee shall be paid prior to recording the final plat.

Motion Language:

Approval:

Motion to approve the Preliminary Plat for Sunburst Hills Subdivision , submitted by Tanner Investments LLC and represented by Brant Tanner and Brian Yeager, finding that the application meets City Standards.

Denial:

Motion to deny _____ application for _____ located at _____), finding that _____ [the Commission should cite which standards are not met and provided the reason why each identified standard is not met].

Continuation:

Motion to continue the public hearing upon the _____ application for _____ to _____ [the Commission should specify a date].

Proposed Name of Subdivision: SUNBURST HILLS CITY OF HAILEY
Street Address or General Location of Property: 2541/2621/2641 WINTERHAVEN DRIVE
Legal Description of Property: LOTS 7, 8 & 9 WOODSIDE SUBD #10, BLOCK 62
Current Zoning of Property: GR Total Area of Property: 1.78 Number of Lots/Units: 12

Check the one box that applies:

- Regular Plat - 5 or more residential parcels, 3 or more non-residential parcels (Commission and Council Review).
- Short Plat - 4 or fewer residential parcels, 2 non-residential parcels, townhouse or condominium units in existing or approved structures, or lot line adjustment creating more than 1 lot in the Townsite Overlay District (Commission Review only).
- Lot Line Adjustment (Administrative Review).

Name of Owner of the Property: BRANT TANNER-TANNER INVESTMENTS LLC
Mailing Address: PO BOX 353 City: HAILEY State: ID Zip: 83333
Phone: 208-578-0329 Fax: 208-578-7745 Cell: 208-720-5476
Email Address: tannertaylor@msn.com

Property Owner Consent:

By signature hereon, the property owner acknowledges that City officials and/or employees may, in the performance of their functions, enter upon the property to inspect, post legal notices, and/or other standard activities in the course of processing this application, pursuant to Idaho Code §67-6507. The property owner is also hereby notified that members of the Planning and Zoning Commission and City Council are required to generally disclose the content of any *ex parte* discussion (outside the hearing) with any person, including the property owner or representative, regarding this application.

Property Owner's Signature: Kimberlee M Johnson Date: 4/7/14
GALENA ENGINEERING - REP

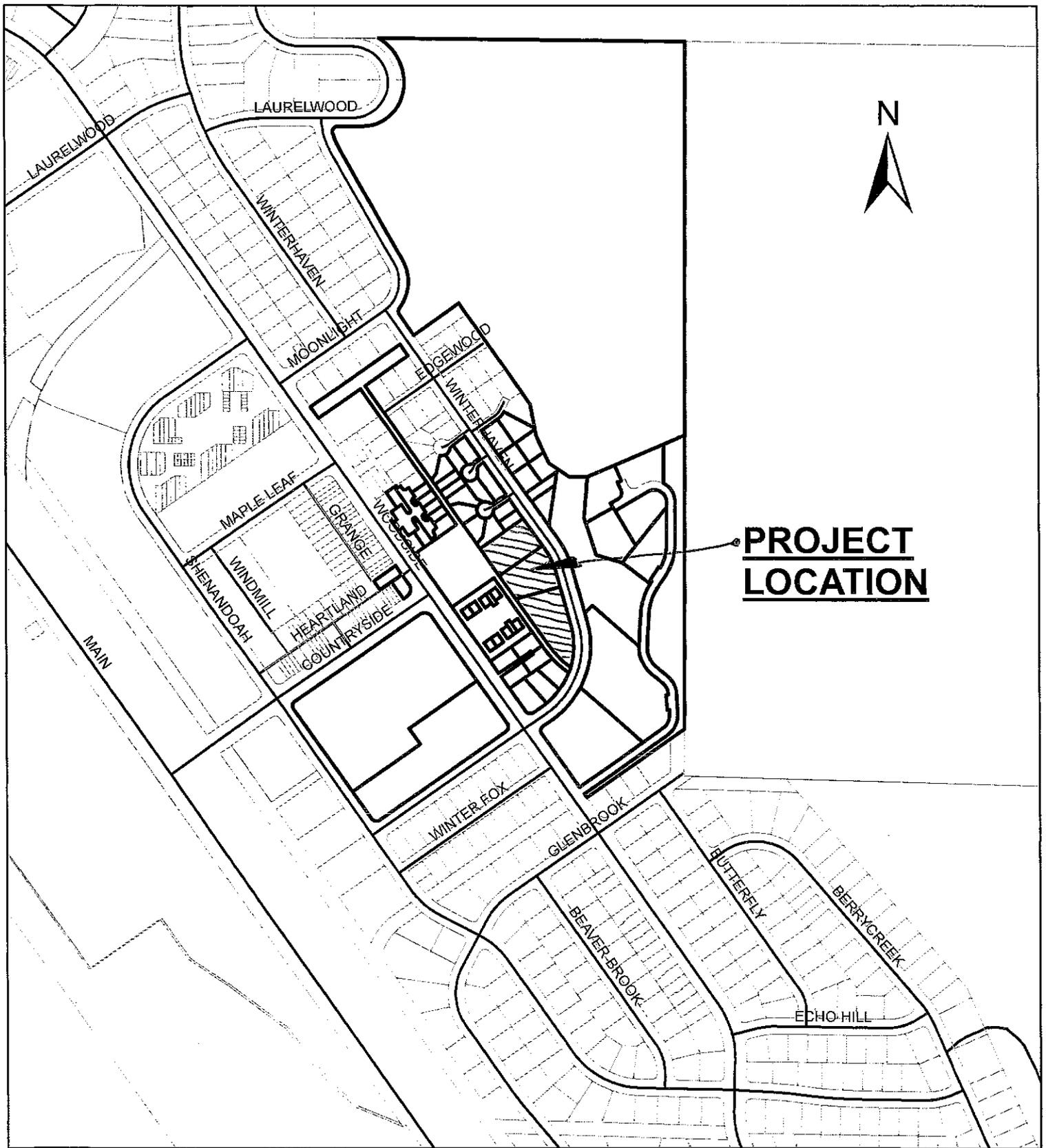
Name of individual to contact on behalf of Trust or LLC (if applicable): BRANT TANNER
Mailing Address: _____ City: _____ State: _____ Zip: _____
Phone: _____ Fax: _____ Cell: _____
Email Address: _____

Application Contact (if different than above): BRIAN YEAGER
**Application Contact will be the Planning Department's primary contact for questions related to the application.
Mailing Address: 317 N RIVER ST City: HAILEY State: ID Zip: 83333
Phone: 208-788-1705 Fax: 208-788-4612 Cell: 208-727-7614
Email Address: byeager@galena-engineering.com

See attached checklist(s) for items that must be submitted with this application in order for application to be considered complete. See the Hailey Subdivision Ordinance for explanation of the review process.

Fees: *Cost of additional noticing, recording fees, and other direct costs will also be assessed.*

	<u>FOR CITY USE ONLY</u>
<input checked="" type="checkbox"/> Regular Plat \$1,250 + \$55/lot, sub-lot, or unit.....	\$ <u>1,910.00</u>
<i>if w/ development agreement - fees for services rendered by City Attorney are billed @ \$125/hr</i>	
<input type="checkbox"/> OR Short Plat \$300 / lot, sub-lot, or unit (not to exceed \$1,200).....	\$ _____
<input type="checkbox"/> OR Lot Line Adjustment \$240.....	\$ _____
Publication: <input checked="" type="checkbox"/> Regular Plat \$40.00 x 4 OR	\$ <u>160.00</u>
<input type="checkbox"/> Short Plat \$40.00 x 2 OR <input type="checkbox"/> Lot Line Adjustment \$40	
Mailing: <input checked="" type="checkbox"/> Regular Plat: (# of addresses <u>56</u>) x 4 x (. <u>49</u> postage + .15 for paper, envelope & label)	\$ <u>35.84</u>
OR <input type="checkbox"/> Short Plat: (# of addresses _____) x 2 x (_____ postage + .15 for paper, envelope & label)	
OR <input type="checkbox"/> Lot Line Adjustment: (# of addresses _____) x (_____ postage + .15 for paper, envelope & label)	
DO NOT COUNT DUPLICATES OR CITY OF HAILEY	
Total Due.....	\$ <u>2,105.84</u>



Vicinity/Adjoiner Map for
SUNBURST HILLS

Scale: 1" = 1000'

Section 14, T.2 N., R.18 E., B.M., City of Hailey, Blaine County, Idaho

DANIELS GENE C 1617 S COLORADO AVE BOISE ID 83706-3837	TANGLEWOOD LLC 75 BUFFALO LN CARBONDALE CO 81623-0000	GUERBER PATRICK JASON PO BOX 5814 HAILEY ID 83333-0000
EHRMANTRAUT SANDRA P BOX 132 BELLEVUE ID 83313-0000	MILLER MAVIS L PO BOX 3626 HAILEY ID 83333-0000	REVELES CARLOS A REVELES ALEJANDRO PO BOX 4201 HAILEY ID 83333-0000
HAILEY SWEETWATER PARTNERS LLC 4401 N MESA ST EL PASO TX 79902-1107	BROWDER SHARON 2721 WINTERHAVEN DR HAILEY ID 83333-0000	RICE JOEL P PO BOX 5887 HAILEY ID 83333-0000
MATTHIES RONALD MATTHIES FAYE 3488 S OTIS CT LAKEWOOD CO 80227-0000	SPRENGER GRUBB & ASSOCIATES C/O DAVID ANDERSON 7103 N PENNCROSS WAY MERIDIAN ID 83646-0000	TANNER INVESTMENTS LLC PO BOX 353 HAILEY ID 83333-0000
PIERCE MICHAEL J EAKIN PAMELA A PO BOX 4723 KETCHUM ID 83340-0000	EVANS MELVIN BRYAN EVANS JANET L 1142 BUTTERCUP RD HAILEY ID 83333-0000	MC LAUGHLIN AMBROSE P MC LAUGHLIN CAROL D'ATRI BOX 4837 KETCHUM ID 83340-0000
MOUNTAIN WEST IRA INC FBO DARYL FAUTH IRA 10096 W FAIRVIEW AVE # 160 BOISE ID 83704-0000	FARRELL ERIC D PO BOX 3807 KETCHUM ID 83340-0000	HIRSCH TYLER D HIRSCH KATYA M 14527 RAINBOW DR LAKE OSWEGO OR 97035-2656
FEDERALHOME LOANMORTGAGE CORPORATION 5000 PLANO PARKWAY CARROLLTON TX 75010-4902	PINA APRIL PO BOX 2628 HAILEY ID 83333-0000	KIMBIRD PARTNERSHIP BOX 3369 HAILEY ID 83333-0000
MONTES FELIPE 2630 WOODSIDE BLVD D2 HAILEY ID 83333-0000	BUNKHOUSE ASSOCIATION 10096 W FAIRVIEW AVE # 160 BOISE ID 83704-0000	FUGER LYNN A PO BOX 2706 HAILEY ID 83333-0000
HARVEY WILLIAM S PO BOX 594 HAILEY ID 83333-0000	HURWITZ ROBERT HURWITZ CAROL 3715 OCEANFRONT WALK MARINA DEL REY CA 90292-0000	GRAY CHRISTOPHER D GRAY KIRA J PO BOX 1034 SUN VALLEY ID 83353-0000
MOUNTAIN SAGE SUB H.O. ASSOC PO BOX 2182 HAILEY ID 83333-0000	ALLRED DAVID R ALLRED RACHAEL R 2528 WOODSIDE BLVD HAILEY ID 83333-0000	LIND CATHY JO PO BOX 87 HAILEY ID 83333-0000

MICKELSON JOANN K
PO BOX 1336
KAMUELA HI 96743-1336

BRENNAN DORA MAE
2526 WINTERHAVEN DR
HAILEY ID 83333-8799

LALANNE MICHEL R
PO BOX 1597
SUN VALLEY ID 83353-0000

BLAINE COUNTY SCHOOLS
118 W BULLION ST
HAILEY ID 83333-0000

JORGENSEN LAURA B
PO BOX 4758
HAILEY ID 83333-0000

SANGHA KEN
C/O CHATEAU DRUG
PO BOX 9200
KETCHUM ID 83340-0000
AREVALO JOSE F AREVALO ALONDRA
PO BOX 487
HAILEY ID 83333-0000

MARQUEZ JUAN A MARQUEZ MARIA R
PO BOX 227
BELLEVUE ID 83313-0000

ALLEY BRIAN
BOX 446
HAILEY ID 83333-0000

REVELES ARTURO AGUAYO-REVELES
JERONIMA
BOX 4201
HAILEY ID 83333-0000

ANDAZOLA JESUS ANDAZOLA
MARRCELA
BOX 1957
HAILEY ID 83333-0000

WARD FRANCIS E WARD SUSAN W
BOX 5713
KETCHUM ID 83340-0000

NEWTN ROBERT SCOTT TRUSTEE
NEWTN DANIELA TRUSTEE
340 FARMER BROTHERS DR
SEDONA AZ 86336-0000

BRADSHAW MICHAEL BRADSHAW
KATHERINE
4851 W RIVERCHASE RD
HERRIMAN UT 84096-0000

MONJARAS ARACELI MONJARAS
FREDDY
PO BOX 1825
HAILEY ID 83333-0000

JUAREZ MARIO TELLEZ CARMEN
REGALDO
BOX 2470
HAILEY ID 83333-0000

DRIEMEYER ROB ROARK
BOX 2217
HAILEY ID 83333-0000

GREENBERG JEREMY GREENBERG
ERIKA M
PO BOX 835
HAILEY ID 83333-0000

BROWN STEWART K
BOX 1265
HAILEY ID 83333-0000

AMBRIZ ARTURO AMBRIZ ELOISA
BOX 3765
HAILEY ID 83333-0000

LEVITT ZANE W LEVITT ROWENA
3422 ROWENA AVE
LOS ANGELES CA 90027-2209

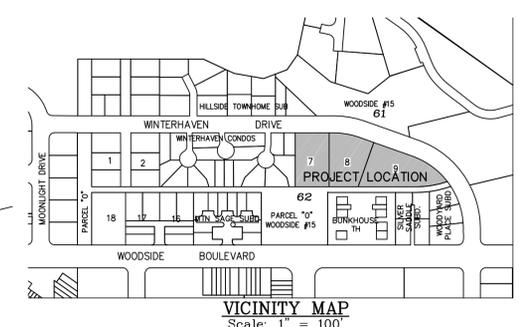
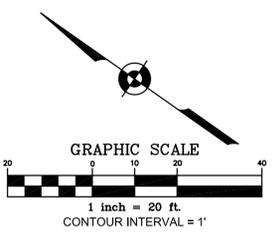
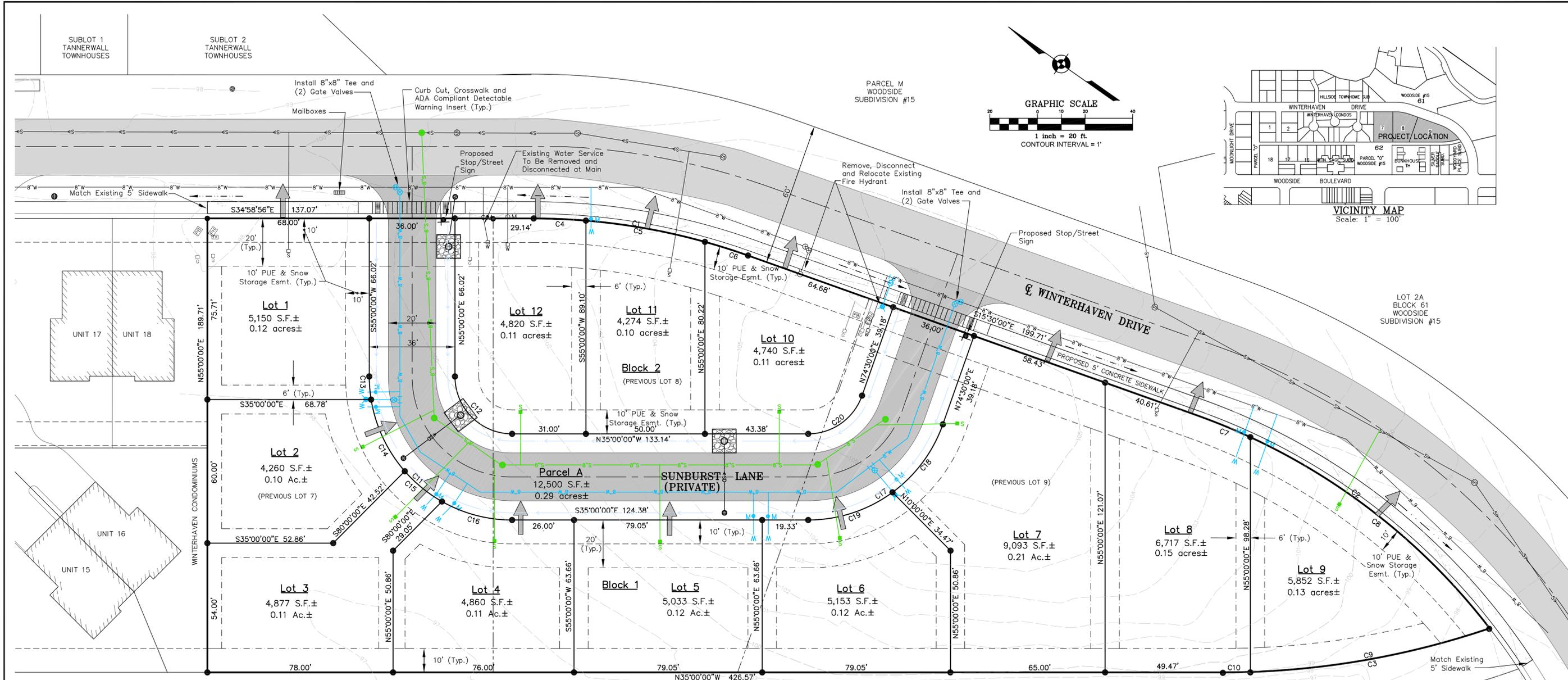
BEACHAM JEFFREY S BEACHAM
KATHRYN G
PO BOX 210
HAILEY ID 83333-0000

HURTADO JOSE E HURTADO BLANCA E
PO BOX 3574
HAILEY ID 83333-0000

RUGIMBANA RONALD B
8914 W LANDMARK CT
BOISE ID 83704-0000

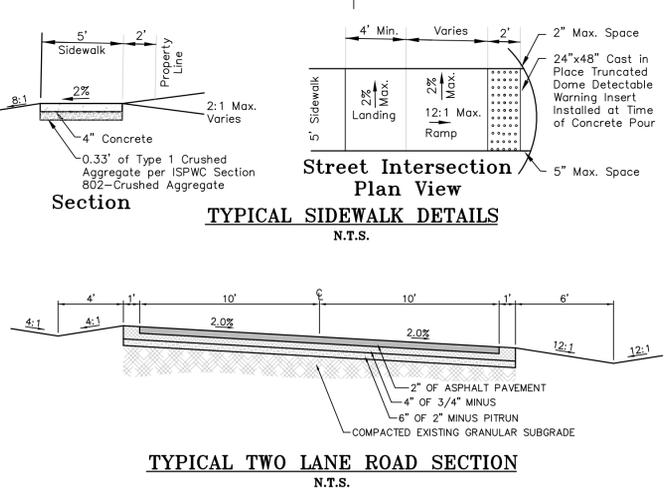
THOMPSON LACIE R
170 LABRADOR LN
BELLEVUE ID 83313-5203

CANCINO SERGIO CANCINO MARIA
PO BOX 2475
HAILEY ID 83333-0000



LEGEND

- Property Boundary
- - - Proposed Lot Lines
- - - Previous Lot Lines
- - - Easement Line, As Shown
- - - Setback Line
- - - Proposed Building Envelope
- - - Centerline of Right-of-way
- - - Adjoining Lot Line
- - - 5' Contour Interval
- - - 1' Contour Interval
- 8" S Proposed Sewer Main
- SD Proposed Storm Drain
- 8" W Proposed 8" Water Main
- Drainage Arrows
- Proposed Detectable Warning Insert
- Existing Asphalt
- Proposed Asphalt
- Proposed 5' Wide Concrete Sidewalk
- Proposed Access
- Found 5/8" Rebar
- Found 1/2" Rebar
- Proposed Catch Basin
- Proposed Drywell
- Existing Gas Service
- Existing Telephone Riser
- Existing Power Transformer
- Existing Sewer Manhole
- Existing Water Service
- Existing Fire Hydrant
- Proposed Sewer Manhole
- Proposed Sewer Service
- Proposed Water Service
- Proposed Water Meter
- Proposed Water Valve
- Proposed Fire Hydrant



NOTES

- 1) Refer to the Plat Notes, Conditions, Covenants, and Restrictions on Original Plat of Woodside #15, Inst. No. 152510, records of Blaine County, Idaho.
- 2) Existing Utility locations are approximate and locations should be verified before any excavation. All utilities shall be installed underground.
- 3) Current Zoning is GR (General Residential District), with a minimum Lot size of 6000 sq. ft., except for Cottage sub-lots which shall have an aggregate density of no more than twelve lots per acre, per Hailey Subdivision Ordinance 821, Section 8.8. Existing parcel area is 1.78 acres. Allowable density is 12 Cottage lots per acre, or 21 lots, proposed density is 12 Cottage lots. Currently, setbacks for detached townhomes (cottages) are 20' front, 6' side and 10' rear. Should setbacks be revised in the future, unbuilt townhomes will be subject to ordinances in place at the time of construction.
- 4) Interior roadway is designated as a fire lane and shall be posted for no parking.
- 5) Parcel A is reserved for Common Access, Public Utilities and Snow Storage to benefit and be maintained by Lots within this subdivision. This area is unbuildable except for ingress/egress or utilities. Costs for utilities, snow removal, onsite street maintenance, and maintenance of Parcel A shall be shared on a pro rata basis between the cottage lots.
- 6) Lots 1 and 7-12 shall access directly onto Winterhaven Drive. All other lots shall access onto Parcel A.
- 7) The City of Hailey reserves the right to clean and maintain sewer mains within this subdivision.
- 8) All driveways, except Lot 3, shall be located 10 feet from adjacent property lines.
- 9) A bus stop shall be constructed by the developer at a location deemed appropriate by Mountain Rides and the City of Hailey.

CURVE TABLE

CURVE	LENGTH	RADIUS	TANGENT	DELTA	CHORD	CHORD DIRECTION
C1	91.81'	270.00'	46.35'	19°28'56"	91.37'	N 25°14'28" W
C2	154.17'	270.00'	79.25'	32°42'55"	152.08'	N 00°51'28" E
C3	114.08'	350.00'	57.55'	18°40'28"	113.57'	S 44°20'14" E
C4	21.95'	270.00'	10.98'	4°39'28"	21.94'	N 32°39'12" W
C5	50.86'	270.00'	25.50'	10°47'32"	50.78'	N 24°55'41" W
C6	19.00'	270.00'	9.50'	4°01'55"	19.00'	N 17°30'58" W
C7	24.53'	270.00'	12.27'	5°12'19"	24.52'	N 12°53'50" W
C8	129.64'	270.00'	66.09'	27°30'36"	128.40'	N 03°27'37" E
C9	102.55'	350.00'	51.64'	16°47'13"	102.18'	S 45°16'51" E
C10	11.53'	350.00'	5.77'	1°53'14"	11.53'	S 35°56'37" E
C11	94.25'	60.00'	60.00'	90°00'00"	84.85'	S 10°00'00" W
C12	37.70'	24.00'	24.00'	90°00'01"	33.94'	S 10°00'00" W
C13	9.71'	60.00'	4.86'	9°16'09"	9.70'	S 50°21'56" W
C14	33.54'	60.00'	17.22'	32°01'46"	33.11'	S 29°42'58" W
C15	20.20'	60.00'	10.20'	19°17'34"	20.11'	S 04°03'18" W
C16	30.80'	60.00'	15.75'	29°24'31"	30.46'	S 20°17'45" E
C17	73.83'	60.00'	42.40'	70°30'00"	69.26'	S 70°15'00" E
C18	35.95'	60.00'	18.53'	34°19'44"	35.41'	S 88°20'08" E
C19	37.88'	60.00'	19.59'	36°10'16"	37.25'	S 53°05'08" E
C20	29.53'	24.00'	16.96'	70°30'00"	27.70'	S 70°15'00" E

SUNBURST HILLS
 A CONCEPTUAL DEVELOPMENT LAYOUT FOR
 A COTTAGE TOWNHOUSE SUBLOT DEVELOPMENT
 LOCATED WITHIN SECTION 14, T.2N., R.18E., B.M., CITY OF HAILEY, BLAINE COUNTY, IDAHO
 PREPARED FOR BRANT TANNER, TANNER INVESTMENTS L.L.C.

PROJECT INFORMATION
 P:\yds\proj\6550.01\dwg\Boundary-Plat-concept-05-06-14.dwg 6/23/2014 10:37:01 AM MDT

DESIGNED BY	DATE
DRAWN BY	DATE
CHECKED BY	DATE

REUSE OF DRAWINGS
 These drawings, or any portion thereof, shall not be used on any Project or extension of this Project except by permission of Galena Engineering, Inc.

GALENA ENGINEERING, INC.
 Civil Engineers & Land Surveyors
 5000 Avenue North
 P.O. Box 425
 Ketchum, Idaho 83340
 (208) 726-4729
 (208) 726-4783 fax
 email galena@galena-engineering.com

NO.	DATE	BY	REVISIONS

PRELIMINARY

1

7/07/2014



Sharon F. Browder
2721 Winterhaven Drive
Hailey, ID 83333

Community Development Department
115 South Main Street
Hailey, ID 83333

RE: Sunburst Hills Preliminary Plat Proposal

Dear Community Development Professionals:

I received a second notice in the mail of the preliminary plat proposal for Sunburst Hills, a reconfiguration of the existing Lots 7-9 within Woodside Subdivision Final Plat No. 15, on June 21, 2014. I am writing to further comment on this proposed reconfiguration. The letter asks for comments pursuant to Title 18 of the Hailey Municipal Code, rather than on the project as a whole. While some of my comments relate to this type of infrastructure, I have additional comments regarding the development that are more comprehensive. I am also enclosing my previous comment letter, dated May 3, 2014, as most of the specific points I made then still hold true.

Since my last letter, I have had an opportunity to review the City of Hailey Comprehensive Plan (Updated 2010), as well as portions of the Hailey Subdivision Ordinance and pertinent sections of the Hailey Municipal Code.

The proposed development seems inconsistent with some of the vision for the City contained in the Comprehensive Plan, including the following:

- *A compact community core retaining the character of Old Hailey (p. 15).*
- *The livability and quality of life in Hailey cannot be maintained and enhanced without residential neighborhoods that are supported, protected, and connected (p. 19).*
- *Impacts resulting from growth pressure, such as environmental degradation, inadequate social and infrastructure services, and loss of small town character are concerns associated with unrestricted growth of the community; therefore it is the responsibility of the city to plan for potential future population growth (p. 26).*
- *Within the context of Hailey's existing overall land use patterns and allowed density, 4 to 5 units per acre is a reasonable target for development to balance expansion and infill. A target density of 4 to 5 units per acre would translate into developments with a mix of lot sizes; some similar to*

the larger 12,000 square foot lots found in the Limited Residential zoning district and some similar to 6,000 square foot lots found in the General Residential zoning district (p. 26).

- *Goal 5.1 (d). High Density Residential – high density residential infill is encouraged in the area along Main Street and River Street between Downtown and the north and south ends of Main Street (p. 29).*
- *Goal 5.1 (f). Traditional Residential – Density varies depending on the qualities of different neighborhoods, generally density is higher within a ¼ mile of Downtown, Community Activity Areas or Neighborhood Service Centers and connected by transit service (p. 29).*
- *Goal 5.5. Lessen dependency on the automobile (p. 30).*
- *Goal 11.1. Establish built environment that maintains a human scale, retains interest, aesthetics, encourages various levels of interaction among all members of the community, and enhances the character of different neighborhoods (p. 44).*
- *Goal 11.2. Ensure building height and mass respects the scale of the traditional and historic built environment (p. 44).*
- *The Comprehensive Plan is only as effective as the commitment to adopt and implement policies and standards and allocate funding in a manner that is consistent with the direction provided by the goals of the plan (p. 48).*

My most global comments are that this type of development is not consistent with the neighboring development that includes six single story homes and will unjustly impact the private property rights and home values of those landowners who made their investment based on the current approved Woodside Subdivision Final Plat No. 15. Four of these homes are owner occupied and three of them, including mine, look directly on to the proposed development. The proposed density of Sunburst Hills is approximately 6.7 units/acre, rather than the City's goal of 4-5 units/acre and 10 of the 12 lots are less than the minimum lot size of 6,000 square feet identified for the General Residential District; 9 of them significantly less. My 1,300 foot house takes up most of my 6,000 square foot lot. Unless the "cottage townhomes" are indeed single-story cottages, this density would seem to encourage or nearly guarantee that the townhouses will be two stories in order to have a marketable square footage. The natural grades of the Sunburst Hills lots are up to 8 feet higher than the highest points on the lots of the neighboring homes. This creates a situation where anything higher than a very modest single story will create a looming presence and shading of the existing neighborhood. This could be partially mitigated by requiring larger lot sizes and deed restrictions with a reasonable height requirements, such as 22 feet.

I have mentioned traffic concerns in my May 3, 2014 correspondence. The traffic safety concern from so many extra driveways remains. There is a Mountain Rides transit stop nearby, but only a small handful of residents use that at any given time. You can count on dozens of more daily car trips and their resulting emissions. This neighborhood far enough from schools and any

commercial enterprise that, beyond an occasional bicycle trip, virtually every trip will be by automobile.

The latest Sunburst Hills preliminary plat includes further considerations for drainage since my May 3, 2014 comments. However, proposed Lots 6, 7, 8, and 9 slope in the opposite direction from the nearest proposed storm drain, which is located near proposed Lot 10. These 4 lots drain towards those in the Woodward Place and Silver Saddle subdivisions. It appears that the proposed drainage infrastructure may be inadequate to address this.

Finally, there is the issue of weeds, both noxious and merely invasive. Disturbance of the existing native vegetation wrought by building the proposed driveway and then waiting for one lot or the other to sell will create opportunities for a huge influx of weeds from existing seed sources unless the areas are immediately and successfully re-vegetated, a feat that would require irrigation in combination with weed control. Maintaining desirable vegetative cover should be an ongoing requirement of the developer until the last lot has been landscaped.

The Comprehensive Plan identifies (p.48) that it is only as effective as the commitment to adopt and implement its policies. Nearly all of the higher density development in Hailey seems aimed at re-platting central Woodside, one of the farthest areas of town from any services, while lots near the city's core remain undeveloped. The Sunburst Hills preliminary plat flies in the face of some of the aspirations of the Comprehensive Plan, such as respecting the traditional built environment, maintaining a human scale, interest, and aesthetics as well as livability and quality of life, and protecting our neighborhood.

I have been informed by the Community Development Department that the Sunburst Hills development is to be a Planned Unit Development, as defined by Article 10 of the Hailey Zoning Ordinance.

There is a requirement that the Planned Unit Development comply with the lighting Ordinance 8B (Ordinance Number 812). I applaud this requirement and request that there not be street lighting or other exterior lighting associated with this development that will shine in our windows or obscure the night sky, such as the free-standing streetlamps installed nearby at the Sweetwater development.

Article 10 of the Hailey Zoning Ordinance regarding Planned Unit Development requires the developer to provide one or more of a list of amenities in the Ordinance listed under 10.3.8. Subdivision Note 9 of the Sunburst Hills proposed plat indicates that the developer has offered to construct a bus stop at a location deemed appropriate by Mountain Rides in order to fulfill this requirement. This seems inconsequential to the neighborhood of impact, as a covered bus

stop already exists within a few hundred feet of Sunburst Hills. In order to help offset the substantial impact of this development on the quality of life and property values of the existing neighbors, a much more meaningful public benefit would be the conservation of green space as provided for in this Ordinance.

10.3.8 Each PUD shall provide one (1) or more of the following amenities, commensurate with the size and density of the development, and commensurate with the modifications requested by the applicant, to ensure a public benefit:

a. Green Space. All Green Space shall be granted in perpetuity and the PUD agreement shall contain restrictions against any encroachment into the Green Space. Where a subdivision is involved as part of the PUD approval process, Green Space shall be identified as such on the plat. A long-term maintenance plan shall be provided. Unless otherwise agreed to by the City, the PUD agreement shall contain provisions requiring that property owners within the PUD shall be responsible for maintaining the Green Space for the benefit of the residents or employees of the PUD and/or by the public. Green space shall be set aside in accordance with the following formulas: 1. For residential PUD's: a minimum of .05 acres per residential unit.

According to my calculations, if the Sunburst Hills development consisted of 9 or 10 units, rather than 12, the formula in the Ordinance would allow for green space totaling between 0.45 and 0.50 acre. According to the Sunburst Hills preliminary plat, the combined acreage of Lots 7, 8, and 9 is 0.49 acre. Leaving these three lots, or Lots 8 and 9 and a portion of Lot 7, in their current condition of nearly pristine native vegetation (see photo below) as green space would alleviate the most direct impacts of shading and blocked views to the existing homeowners while using no extra irrigation water. Nine units, rather than 12, would also bring the density to 5 units per acre, which is in line with the City's goal for infill, mentioned above. In its current state, the green space would cost the developer and future residents little or nothing, other than protecting it from disturbance and degradation, while alleviating the drainage concerns associated with these lots if they were developed. This seems like a win-win situation for all concerned. This solution has the power to make the difference between the neighborhood, if not embracing the development, at least accepting it, versus creating an adversarial situation where the owners of existing modest homes feel abused by the system. If there is interest from either the city or the developer, I would be happy to contribute my professional skills to develop an educational interpretive sign for the site that discusses the benefits of native vegetation, water conservation through landscaping, or both.

Once again, thank you for the opportunity to share my observations and recommendations. Please feel free to contact me at my daytime number (cell 208-727-7388, or work 208-727-5005) for clarification on any of these points.

Sincerely,

Sharon F. Browder

Homeowner

2721 Winterhaven Drive; Lot 1, Block 1, Woodward Place Subdivision



Proposed green space area under Ordinance 10.3.8. View looking east across Lot 9 of Sunburst Hills preliminary plat from Woodward Place Subdivision.

5/3/2014



Sharon F. Browder
2721 Winterhavn Drive
Hailey, ID 83333

Community Development Department
115 South Main Street
Hailey, ID 83333

RE: Sunburst Hills Preliminary Plat Proposal

Dear Community Development Professionals:

I received notice in the mail of the preliminary plat proposal for Sunburst Hills, a reconfiguration of the existing Lots 7-9 within Woodside Subdivision Final Plat No. 15, on April 22, 2014. I am writing to comment on this proposed reconfiguration.

I purchased my single-story home on an nearby parcel, Lot 1, Block 1 of Woodward Place Subdivision, also known as 2721 Winterhaven Drive, in May, 2010. This home represents a substantial proportion of my net worth and was purchased only after careful considerations of the conditions, and potential future conditions, of the surrounding area. This is my only residence. I live here year round, along with my two teenaged children, who attend school here. I work full time as a biological scientist in the community.

One of the main reasons that I purchased this home, other than its energy efficiency, was its access and views of public lands (managed by the Bureau of Land Management and the City of Hailey) directly to the east. Our windows face east across what is currently **existing Lot 9** within the Woodside Subdivision Final Plat No. 15, towards these public lands. I am an avid gardener and former landscape designer, and have made substantial investments in my yard. I have altered the irrigation system and converted the entire yard, which was formerly all lawn, to water-wise landscaping, certified as "trout friendly – gold standard" by the Wood River Land Trust (see enclosed photos # 1 and 2). I have recently installed a raspberry patch, strawberry bed, raised vegetable beds, and a grape arbor on the east side of my home. The success of these installations depends on receiving morning sunlight, which is the only sunlight available on the east side of the house. **Existing Lots 7-9** are several feet higher in elevation than my lot (see enclosed photos #3-6). Development directly to the east of my home would block both the views and the morning sunlight. Multi-story development in this location would

likely cast my entire yard in shadow for most, if not all, of the morning from late spring through early fall.

Of course, I realized at the time I purchased my home that it was likely that the **existing Lot 9** would eventually be developed. However, the configuration of the **existing Lot 9** is such that the relatively thin triangular wedge at the south end of the lot would not be a likely location for a home. There are better siting choices for a home on this 0.702 acre lot, and the minimum 20-foot front yard setback would also limit a building location, so it would be unlikely that our open views to the east would be blocked when a single-family home was finally constructed on this lot.

A second issue is drainage. Currently, the native vegetation on **existing Lots 7-9** absorbs the snowmelt quickly. The soils here are heavy clay. When the native vegetation is replaced by pavement, roofs, and irrigated landscaping, there will be a great deal more runoff. The clay soils absorb water quite slowly, and also dries very slowly once saturated, particularly when compacted by construction activities. As I mentioned previously, the elevation of **existing Lots 7-9** are several feet higher than mine. The proposed development density of Sunburst Hills would mean that a large percentage of the ground throughout the development would become impervious to water, and the snowmelt and irrigation water would run downhill.

A third issue is traffic safety. Currently, I must be very cautious when backing out of my driveway due to both the rise and curve of Winterhaven Drive at this location. The multi-family dwellings on Winterhaven Drive to the north generate a good deal of traffic, which is quite difficult to see coming over the rise at this juncture. Dwellings or other structures on the **proposed Lots 8 and 9** will make this more difficult still (see enclosed photo #6). The addition of twelve dwellings, and however many trips a day are expected from these residences, will quadruple this hazard over what is currently planned. My oldest child is just finishing drivers education, and will be backing out of the driveway on his own in a few short months.

A fourth item of concern is outdoor lighting. In our neighborhood residents can sit out and enjoy the stars or meteor showers at night because there are no street lights. When someone accidentally leaves a porch light on all night, it's pretty unusual and quite annoying. I've noticed that on some of the higher density developments, there are street lights and "security" lights that are left on all night. It would negatively impact our quality of life to have light pollution blotting out the stars and shining in our windows at night.

I understand that residential development is the intended use of Woodside Subdivision Final Plat No. 15 parcels. When I purchased my home, it was my expectation that someday there would be three additional single-family homes constructed on the **existing Lots 7, 8, and 9**.

The Sunburst Hills proposal is inconsistent with the existing residential density of single story detached homes and with the lot dimensions in the Hailey Zoning Ordinance. Drastic changes to existing plats are unfair to homeowners like me who have already made their investment in a single-family home. While I appreciate the need for high density development, too much of it negatively impacts neighborhoods. This type of higher density development would forever change the character of my home and neighborhood. Where is the conceptual community plan that says this neighborhood should be filled with high-density housing? And if that is the plan, why were single-family, single-story homes like mine allowed to be part of the mix, overshadowed and crowded by looming larger developments? A plan for mixed housing needs to be carefully crafted, with community input, by the people who live in the neighborhood, rather than routinely approving proposals in a piecemeal fashion to satisfy developers' desires for profit. Higher density housing types and multi-story developments should be buffered from single-family and single-story developments by open space.

My home is paid for. Because it is single-story, energy efficient, and the yard is now low-water and low-maintenance, I had planned to retire in it. My home has lost \$52,000 in assessed value due to the recession since I purchased it. Sunburst Hills, as platted, is likely to lower the value further. Quite frankly, if Sunburst Hills is approved as currently platted, I will be looking to leave Hailey after my children are finished with high school. If I can sell my home for a reasonable price.

My request is that development take place without substantially reducing the quality of life, and reducing the property values, of existing homeowners. The proposed density of Sunburst Hills subdivision is too high; every one of the proposed twelve lots is below the minimum lot size standard of 6,000 square feet currently in Hailey's Zoning Ordinance for general residential development. Some appear to be below the 50 foot minimum lot width. It is unclear if the proposed "cottage townhouse development" consists of multi-story buildings. Whatever is developed there should meet the minimum requirements already in place for residential development and should be single-story to avoid more severe impacts to the existing single-story developments nearby.

I recommend that the proposal be denied, based on the reasons provided above. I hope that the City of Hailey will do what is best for and fair to its residents, not necessarily what will generate the most profits for developers or the greatest tax revenues. If changes are made to the existing plat, I recommend the following mitigations:

- **Open space on proposed Lots 8 and 9.** Eliminate construction on **proposed Lot 8 and Lot 9**. The preliminary plat identifies **proposed Lots 8 and 9** as two 0.13 acre building sites, both of which are below the minimum lot size of 6,000 square feet for the General

Residential District. This will create a buffer between the development and the single-story homes nearby. The lots currently consist of intact native vegetation, which, if left undisturbed, will require no additional irrigation water and currently provide a defense against noxious weed invasion that will rapidly ensue if this vegetation is disturbed by construction activities. This sagebrush- dominated vegetation is low enough that the views to the east, and views of approaching traffic, are not blocked. Provisions for snow storage should be made on other parts of the development, to prevent the destruction of this vegetation and prevent drainage from snow storage and other runoff from the development from either subbing or flowing into my property. Require an engineering study of drainage systems, and adoption of the recommendations, as a requirement for any building permit.

- **Reduced density.** Uphold the minimum lot dimensions of 6,000 square feet and a width of 50 feet for all lots. Require the minimum setbacks of 20 feet for the front yard and 10 feet for the side and back yards. My 6,000 square foot lot is *very small* and the 10 foot setback to my neighbor to the west is *very close*. These are very minimum setbacks. Uphold the 40% maximum total lot coverage value.
- **Single family detached homes.** Maintain consistency with the original plat and developments. The character of a neighborhood becomes increasingly urban and impersonal as the number of higher density developments rises. Home ownership is likely to decrease and the incidence of renting increase. This is a trend that destabilizes neighborhoods with frequent turnover of the residents, less investment in the community, and neighbors that don't know each other. Additionally, the substantial increase in traffic will increase traffic hazards and decrease the quality of life for existing residents.
- **Single story development.** Allow only single story development. Multistory development on these lots will dwarf several existing single story homes to the west, blocking both sunlight and views. Consideration of existing property investments should be taken into account.
- **Dark skies.** Eliminate outdoor lighting, other than individual porch lights. To minimize light pollution, individual porch fixtures should be designed or shielded so emitted light rays are projected below a horizontal plane running through the lowest point on the fixture. Fixtures should use bulbs of no more than 150 watts.

Thank you for the opportunity to share my observations and recommendations. Please feel free to contact me at my daytime number (208-727-5005) for clarification on any of these points.

Sincerely,

Sharon F. Browder

Homeowner

2721 Winterhaven Drive; Lot 1, Block 1, Woodward Place Subdivision



Photo 1. Front yard, 2721 Winterhaven Drive, showing water-wise landscaping.



Photo 2. Example of community investment by a homeowner. Water-wise, no chemical landscaping that incorporates native plants for pollinators and wildlife.



Photo 3. Approximate locations of my lot corners at 2721 Winterhaven (Lot 1, Block 1, Woodward Place Subdivision) relative to the existing Woodside Subdivision Final Plat No. 15.



Photo 4. Approximate locations of my lot corners at 2721 Winterhaven (Lot 1, Block 1, Woodward Place Subdivision) relative to the Sunburst Hills preliminary plat.



Photo 5. Relative elevation difference between Existing Lot 9 and my lot. The natural grade is likely 2 to 3 feet higher on the south portion of Existing Lot 9 than at 2721 Winterhaven.



Photo 6. View looking northeast along Winterhaven Drive from the bottom of my driveway towards the proposed development. Both the curve and the hill reduce traffic visibility.

From: Sharon Browder [<mailto:sfbrowder@gmail.com>]
Sent: Tuesday, July 08, 2014 3:22 PM
To: Kristine Hilt
Subject: Sunburst Hills Preliminary Plat Comments

Dear Community Development Professionals:

A few additional thoughts on the Sunburst Hills preliminary plat proposal from a more global perspective. The approximately 800-acre canyon directly to the east of the proposed subdivision appears to have, at some time in the past, washed out onto the valley floor, creating an alluvial fan of debris approximately 20 feet thick at its outer edge (see attached photo). The outer edge of this fan, directly across Winterhaven Drive from the proposed subdivision, may or may not have been cut off by the action of the Wood River crossing its floodplain. In the future when the canyon burns, debris flows out of the canyon from a heavy precipitation event following the fire, such as have been recently observed in Deer Creek and Greenhorn drainages, are likely.

The proposed development is situated directly at the mouth of the canyon and very close to the wildland urban interface and what appears to be an historic debris flow. Public safety concerns for evacuation on short notice should be at the forefront of consideration for where higher density developments are allowed. How fast could all of these higher density on Winterhaven Drive and the central Woodside Area be evacuated in case of these types of eventualities?

The Comprehensive Plan seems to have been on target with encouraging higher density near the Hailey downtown core for yet another good reason.

Thanks again,

Sharon Browder

STAFF REPORT

TO: Hailey Planning and Zoning Commission

FROM: Micah Austin, Community Development Director

RE: City of Hailey initiated text amendment to Ordinance 532, the Zoning Ordinance, by amending section 8.1.1 and 8.1.2 to clarify the General Provisions and Specific Standards for regulating fences, by amending 8.2.2 to revise the definition of Animated Sign and add a definition for Electronic Message Display, by amending 8.2.6 to prohibit Electronic Message Display Signs, by amending 8.2.7 to revise Design Guidelines and Standards for all signs, and by amending 8.2.8 with the addition of a Sign Matrix.

HEARING: Planning and Zoning: June 9, 2014
Planning and Zoning: July 7, 2014
City Council: TBD

Notice

Notice for the public hearing before the Planning and Zoning Commission on June 9, 2014 was published in the Idaho Mountain Express on May 21, 2014 and mailed to public agencies and area media on May 21, 2014.

Proposal

The proposed amendment to Article 8 of the Zoning Ordinance would revise sections of code regulating both fences and signs within the City of Hailey. Section 8.1 would be amended to clarify the general provisions and standards for siting fences and regulating the location and maintenance of fences. The amendments to Section 8.2 would add a definition of Electronic Message Display and would also prohibit Electronic Message Displays. In addition, the Design Guidelines and Standards in 8.2 would be revised to clarify standards, correct errors in the text, and by adding a Sign Matrix to assist in correctly siting signs within zoning districts.

Background

In late 2012, the City of Hailey began a community discussion of whether Electronic Message Displays were appropriate for the community and proposed a text amendment to allow the signs. The Planning and Zoning Commission voted unanimously to recommend denial of the ordinance on two occasions and the City Council voted once on the proposal which was denied by the City Council. During the that process, staff did extensive research into sign ordinances all over the state and country as the related to Hailey's sign ordinance. As a result, errors were discovered in Hailey sign ordinance as well as inconsistencies in sign standards, area calculation standards, and other areas of sign regulations. As a result of the negative vote from the City Council on Electronic Message Displays, staff was directed to draft an ordinance strictly prohibiting EMDs in the City of Hailey. The proposed ordinance fulfills this directive as well as amends sections of Article 8 that were determined to be inconsistent or unclear.

Procedural History

The text amendment was considered by the Hailey Planning and Zoning Commission on June 9, 2014 during a regularly scheduled and noticed public hearing. After the public hearing, the Commission chose to table a decision on the text amendment the next regularly scheduled meeting. On July 7, 2014, the Commission will hold a public hearing on this text amendment.

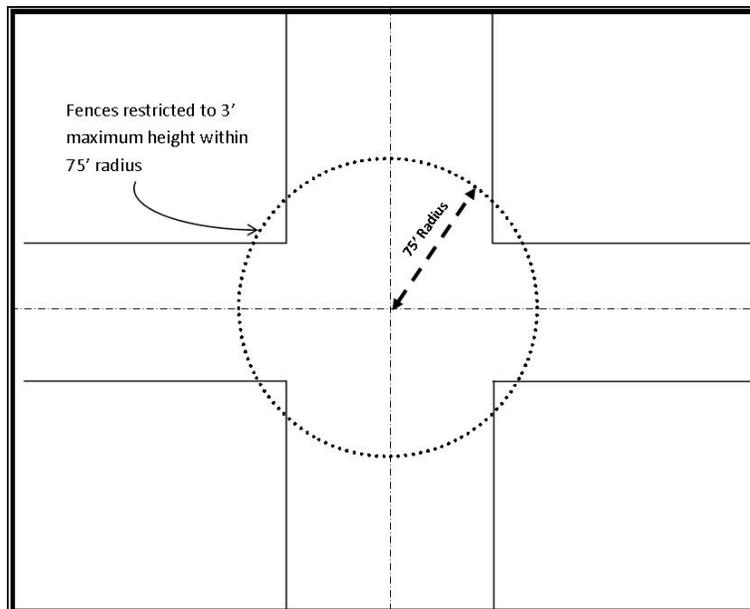
Department Comments

The proposed ordinance addresses both fences and signs. The following is a summary of those amendments and implications, or options for amendments, to each section.

Fences

Option 1: 75' Radius

The proposed ordinance addresses the fence siting requirements when a fence is located within 75' of the centerline of an intersection. While it has always been the City's policy to deny fences within 75' of an intersection, this policy has not always been enforced and the numerous fences throughout the city that are within this radius shows how prevalent these fences have become over the years. While no fences have been knowingly permitted within this radius, Staff questioned the public safety value of this requirement and found that the Idaho Transportation Department standards are to allow fences and other objects within a 75' radius of an intersection, provided they are no taller than 36" from grade. It is staff's recommendation in this amendment to allow fences within the 75' radius, provided they are no taller than 36". An illustration in the proposed ordinance assists in clarifying this standard:

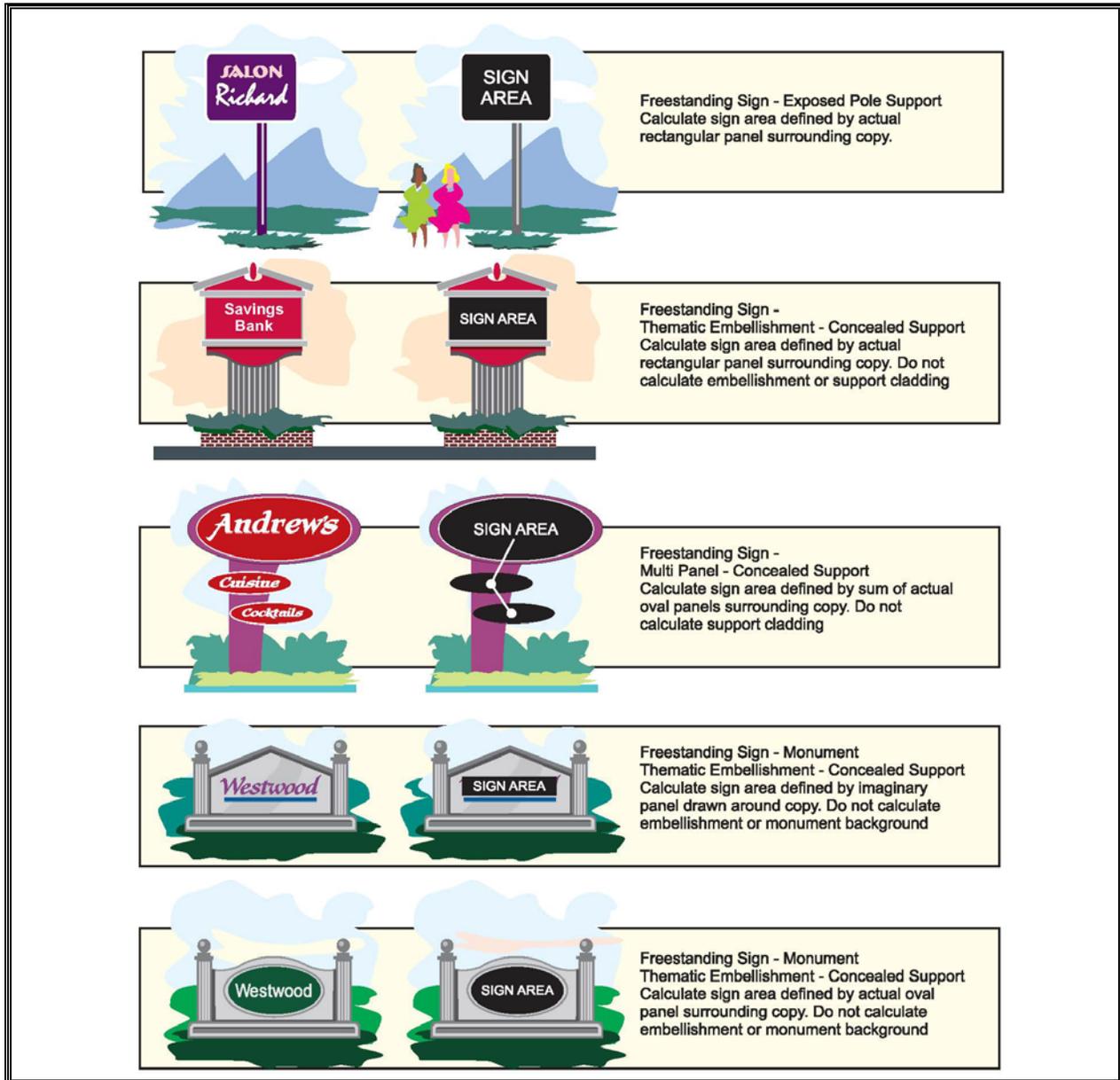


In addition to the siting standards for fences within intersections, the amendment also clarifies that a Fence Permit is not required for maintaining a fence. The proposed language reads, "Fence Permits are not required for maintenance and repairs to existing fences that do not

and other structure have been built within the 75' radius and is allowed according to setbacks. It seems unusual that a home can be built within this radius but not a fence. This amendment would address this inconsistency as well.

Signs

The initial motivation for this amendment was to strictly and clearly prohibit Electronic Message Displays in the City of Hailey. However, during the text amendment process that ultimately resulted in denial of the EMD ordinance, staff found several sections of 8.2 (Signs) that needed clarification or had errors. The proposed ordinance adds and definition for Electronic Message Displays, strictly prohibits them, and addresses the standards for evaluating signs, including how to correctly calculate a sign area. Illustrations have been provided in the proposed ordinance that clarifies sign area calculation standards. The following illustration is taken from the proposed ordinance and gives several examples of how to correctly calculate a sign area. This will be an excellent tool for the public and for staff in evaluation sign permits.



The amendment also addresses some errors that were discovered the currently adopted sign ordinance including a section that prohibits Awning Signs in all business districts. Clearly, this was a mistake and has been addressed in the text amendment. Other revisions include reformatting the sections and subsections to be consistent throughout Section 8.2, removing all references to lit signs, and listing which zones are appropriate for specific signs. All lighting and zoning standards have been moved to the Sign Matrix, which is similar to the District Use Matrix adopted in 2013 for efficiently organizing zone regulations. The Sign Matrix addresses which types of signs are allowed in each zone:

Sign Description or Category	Zoning Districts
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	RGB	LR	GR	NB	LB	TN	B	LI	TI	A	SCI-SO	SCI-I
Awning and Marquee	N	N	N	P	P	P	P	P	P	P	P	P
Changeable Copy	N	N	N	P	P	P	P	P	P	P	P	P
Externally Lit Sign	N	N	N	P	P	P	P	P	P	P	P	P
Freestanding	P	P	P	P	P	P	P	P	P	P	P	P
Internally Lit and Neon Signs	N	N	N	N	N	N	P	P	P	P	P	P
Portable	N	P	P	P	P	P	P	P	P	P	P	P
Projecting	N	N	N	P	P	P	P	P	P	P	P	P
Temporary Signs	N	N	N	N	P	P	P	P	P	P	P	P
Wall Signs	P	N	P	P	P	P	P	P	P	P	P	P
Window Signs	N	N	N	P	P	P	P	P	P	P	P	P

Except for prohibiting Electronic Message Displays, the proposed amendment does not introduce any new regulations but adds language to clarify and streamline the decision-making process for both the public and staff for current regulations.

The proposed amendments are in compliance with the Section 5, Goal 5.1(b), and Section 11, Goal 11.1, of the Comprehensive Plan. The amendments are also in compliance with the current Zoning Ordinance and the Subdivision Ordinance.

Standards of Evaluation

Note: Staff analysis is in lighter type,
Italicized words are words or phrases added by staff for clarification purposes.

14.6 When evaluating any proposed amendment under this Article, the Commission and Council shall make findings of fact on the following criteria:

a. The proposed amendment is in accordance with the Comprehensive Plan;
The Council should consider how the proposed amendments relates to the various goals of the Comprehensive Plan (listed below for reference). Section 11, Community Design, has been addressed as being most applicable to this application as seen below.

Section 11: COMMUNITY DESIGN

Goal 11.1: Establish a built environment that maintains human scale, retains interest, aesthetics, encourages various levels of interaction among all members of the community, and enhances the character of different neighborhoods.

Fences and particularly Signs are often the first impression that a city gives to many visitors, residents, and businesses. Fences should be planned and constructed in a way that is functional but adds aesthetic value to a home and neighborhood. For better or for worse, signs

can define the character of a city by enhancing or detracting from the overall aesthetics. The proposed amendment supports the City Council and Planning and Zoning Commission’s clear direction to prohibit EMD signs because they did not feel these signs portrayed the appropriate image for Hailey. Other proposed amendments further clarify the code in a way that will continue to produce appealing signage through the Sign Permit Process. All of these amendments work towards establishing a built environment that is envisioned in the Comprehensive Plan and are compliant with the Comp Plan.

Section 5: Land Use, Population and Growth Management

Goals 5.1 (b): Downtown, the historic commercial center containing the greatest concentration of commercial, cultural and civic activity.

The downtown and historic commercial center has the highest density of signage in the City of Hailey, which demands a carefully thought out Sign Permit process that relies on clear standards and regulations. The proposed amendment clarifies these standards and attempts to keep the built environment of Hailey an aesthetically appealing and inviting place for business, residents, and visitors. The heart of Hailey is the downtown area and without proper and complimentary signage, it risks losing the vibrancy and appeal of a historic and exciting downtown area.

Comp Plan Goals (2010)	
1.1	Preserve, protect and restore natural resources including waterways, floodplains, wetlands, soil, community forest, native vegetation, green space and wildlife habitat and migration corridors for the benefit of the City and its residents.
1.2	Efficiently use and conserve resources.
1.3	Promote renewable energy production
1.4	Promote energy conservation
1.5	Promote air quality protection
2.1	Reduce the potential threat to loss of life, limb or property and minimize public expenditures due to natural and man-made hazards.
3.1	Assure the protection and preservation of Special Sites, Areas and Features to maintain a strong community identity for future generations
3.2	Protect the residential character of the original Townsite.
4.1	Create and maintain an interconnected system of parks, recreational facilities, trails, green spaces and natural lands in order to provide diverse recreation opportunities for Hailey residents within ¼ mile to ½ mile of the greatest number of residents.
5.1	Retain a compact City comprised a central downtown with surrounding diverse neighborhoods, areas and characteristics as depicted in the Land Use Map:
a.	Main Street Corridor – area of high density commercial, mixed use and residential development.
b.	Downtown - the historic commercial center containing the greatest concentration of commercial, cultural and civic activity. Downtown is the priority area for encouraging higher density commercial and mixed use (commercial and residential) development.
c.	Community Activity Areas – located at the north and south ends of the Main Street Corridor. High density residential is encouraged. Commercial and mixed use (commercial and residential) development is appropriate, but should be subordinate and secondary to the infill

of Downtown.
d. High Density Residential – high density residential infill is encouraged in the area along Main Street and River Street between Downtown and the north and south ends of Main Street.
e. Residential Buffer – medium density residential, providing a buffer between lower density residential neighborhoods to the east and west and the Main Street District.
f. Traditional Residential – Density varies depending on the qualities of different neighborhoods, generally density is higher within a ¼ mile of Downtown, Community Activity Areas or Neighborhood Service Centers and connected by transit service.
g. Neighborhood Service Centers – Small commercial areas serving residents within walking distance (¼ to ½ mile) where commercial use is subordinate to residential uses and to Downtown or Community Activity Areas.
h. Light Industrial – Areas containing uses important to a variety of business sectors that focus on the production of products and services that are less compatible with, and do not compete with, uses in Downtown and the Community Activity Areas.
i. Airport Site Redevelopment – a diversity and integration of uses and community assets that complement and support Downtown and are connected within and to existing neighborhoods.
j. Community Gateways – areas where one has a sense of arrival or sense of being within a part of town distinguished from others providing opportunities for special design considerations.

5.2	Maintain Downtown as the area containing the greatest concentration of commercial, cultural and civic activity and as the priority area for encouraging higher density commercial and mixed use (commercial and residential) development.
5.3	Continue cooperation with the Blaine County and the Friedman Memorial Airport Authority in regional planning efforts to optimally relocate the airport and plan for the long term redevelopment of the site within the city limits to ensure that changes in land use are beneficial to the community of Hailey.
5.4	Protect open space within and surrounding Hailey, including visible ridgelines, undeveloped hillsides and agricultural areas which help define the unique character of Hailey.
5.5	Lessen dependency on the automobile.
5.6	Manage and accommodate population growth by infill development and, when appropriate, minimal expansion by annexation and/or density increases.
5.7	Encourage development at the densities allowed in the Zoning Code.
6.1	Encourage a diversity of economic development opportunities within Hailey
6.2	Encourage abundant, competitive and career-oriented opportunities for young workers.
7.1	Encourage a variety of projects and programs that meet the needs generated by various segments of the population, especially the needs of those who risk suffering effects of discrimination or are socially or economically disadvantaged.
7.2	Encourage projects and programs that seek to provide opportunities for cultural, cross-cultural and educational enrichment.
8.1	Encourage development that provides opportunities for home ownership and rental homes for individuals and families of all socio-economic levels.
9.1	Plan for the long-term utilities, service and facility needs of the City while minimizing impacts to the greatest extent possible.
10.1.	Create and maintain a pedestrian and bicycle-friendly community that provides a safe, convenient and efficient multi-modal transportation system for all Hailey residents.
11.1	Establish a built environment that maintains a human scale, retains interest, aesthetics, encourages various levels of interaction among all members of the community, and enhances the character of different neighborhoods.
12.1	Evaluate whether proposed regulatory or administrative actions may result in an unconstitutional taking of private property.
13.1	Encourage and facilitate the development of school facilities that are planned consistently with the city’s other land use policies.
13.2	Ensure the provision of safe, adequate, convenient multi-modal transportation access to all existing and future school sites.

- b. Essential public facilities and services are available to support the full range of proposed uses without creating excessive additional requirements at public cost for the public facilities and services;**

There are no additional costs or compromise anticipated to public facilities and services.

- c. The proposed uses are compatible with the surrounding area; and**

The proposed amendment would have no impact on surrounding a

- d. The proposed amendment will promote the public health, safety and general welfare.**

It is not anticipated that the proposed amendment will adversely affect the public health, safety and welfare of citizens.

Motion Language

Approval:

Motion to recommend to the City Council the proposed amendments to Article 8, Sections 8.1 and 8.2, finding that the amendments are in accordance with the Comprehensive Plan, that essential public facilities and services are available to support the full range of proposed uses without creating excessive additional requirements at public cost for the public facilities and services, that the proposed uses are compatible with the surrounding area, and that the proposed amendment will promote the public health, safety and general welfare.

Denial:

Motion to recommend to the City Council denial of the proposed amendments to Article 8, Sections 8.1 and 8.2, finding that _____ [the Council should cite which standards are not met and provided the reason why each identified standard is not met].

Continuation:

Motion to continue the public hearing upon the proposed amendment to Section 8.2 to _____ [the Commission should specify a date].

Table:

Motion to table the proposed amendment to Section 8.2

HAILEY ORDINANCE NO. ____

AN ORDINANCE OF THE CITY OF HAILEY, IDAHO, AMENDING HAILEY'S ZONING ORDINANCE, ORDINANCE NO. 532, BY AMENDING SECTION 8.1 TO CREATE STANDARDS FOR FENCES AND LANDSCAPING WITHIN A **SEVENTY FIVE FOOT RADIUS** OF THE CENTERLINE OF AN INTERSECTION; BY AMENDING SUBSECTION 8.2.2 TO AMEND THE DEFINITION OF ANIMATED SIGN, CHANGEABLE COPY SIGN AND WINDOW SIGN AND TO ADD THE DEFINITION OF ELECTRONIC MESSAGE DISPLAY; BY AMENDING SECTION 8.2.5 TO LIST WINDOW SIGNS AS AN EXEMPT SIGN; BY AMENDING SECTION 8.2.6 TO LIST AN ELECTRONIC MESSAGE DISPLAY SIGN AS A PROHIBITED SIGN; BY REPEALING SUBSECTIONS 8.2.7, 8.2.9, 8.2.10 AND 8.2.11 AND ADDING A NEW SUBSECTION 8.2.7 TO ESTABLISH DESIGN GUIDELINES AND STANDARDS; BY REPEALING SUBSECTION 8.2.8 AND REPLACING IT WITH A NEW SUBSECTION 8.2.8 TO ESTABLISH A SIGN MATRIX; BY RESERVING SECTIONS 8.2.9, 8.2.10 AND 8.2.11; BY PROVIDING FOR A SEVERABILITY CLAUSE; BY PROVIDING FOR A REPEALER CLAUSE; AND BY PROVIDING FOR THE EFFECTIVE DATE OF THIS ORDINANCE UPON PASSAGE, APPROVAL AND PUBLICATION ACCORDING TO LAW.

WHEREAS, the City of Hailey wishes to uniformly regulate fences at intersections to promote public safety;

WHEREAS, the City of Hailey wishes to expressly regulate the display of electronic message displays (EMD);

WHEREAS, the City of Hailey wishes to clarify provisions of its sign ordinance and to create a matrix for permitted and non-permitted signs;

WHEREAS, the proposed amendments are generally in accordance with the Comprehensive Plan;

WHEREAS, the proposed amendments will not create excessive additional requirements at public cost for public facilities and services; and

WHEREAS, the proposed amendments will be in accordance with the welfare of the general public.

BE IT THEREFORE ORDAINED BY THE MAYOR AND COUNCIL OF THE CITY OF HAILEY, IDAHO, AS FOLLOWS:

Section 1. Section 8.1 of the Hailey Municipal Code is amended by the deletion of the stricken language and addition of the underlined language, as follows:

8.1 Fences.

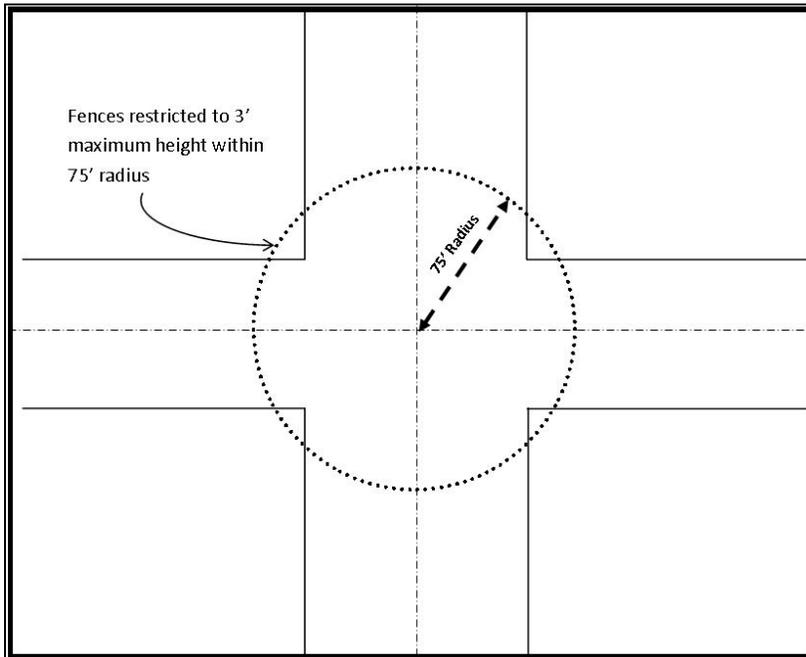
8.1.1 General Provisions. The following requirements shall apply in all districts:

a. No fence may be located, constructed, or maintained in such a way as to obstruct the view of intersections by motorists and pedestrians.

- b. ~~Fences shall not be located within seventy five (75) feet of the centerline intersection of two (2) streets.~~
- e. No barbed wire or other sharp pointed metal fence and no electrically charged fence shall be permitted, unless after consideration, the Commission makes a determination that such materials are necessary for security purposes.
- d.c. A fence permit, issued by the Building Official Community Development Department, shall be required for all fences in all districts.
- d. Fence Permits are not required for maintenance and repairs to existing fences that do not change the location, height, material, or structure of the fence.

8.1.2 Specific Standards. The following provisions shall apply in addition to those specified in Section 8.1.1.

- a. Height. Except as otherwise provided, ~~for all uses fences~~ in the LR, GR, LB and TN districts, ~~fences~~ shall not exceed four (4) feet in height when located within the required front yard setback and six (6) feet when located within the required side and rear yard setbacks-, fences in
- b. ~~For all uses in the LI, TI, SCI-SO and B D districts, fences shall not exceed six (6) feet in height-and fences in~~
- e. ~~For all uses in the SCI-I D district or for Public Uses or Public Utility Facilities in all zoning districts, fences shall not exceed eight (8) feet in height.~~ **Fences located within a seventy-five (75) foot radius of the centerline of any intersection of two (2) or more streets shall not exceed three (3) feet in height.** Refer to illustration below:



- d.b. Arbors, trellises, entry arches and similar yard or landscape features may be permitted within a required yard setback provided they are not more than eight (8) feet high, five (5) feet wide and three (3) feet deep.

- ec. Multiple features, excluding landscaping, shall not be placed in a linear fashion for the purpose of creating a fence-like barrier.
- fd. Chain link material is prohibited except for Public Uses or Public Utility Facilities with an approved Conditional Use Permit.
- ge. For the purpose of applying the above height standards, the average height of the fence along any unbroken run may be used, provided the height at any point is not more than 10% greater than the maximum height.

Section 2. Subsection 8.2.2 of the Hailey Municipal Code is amended by the deletion of the stricken language and addition of the underlined language and by the insertion of the definitions in alphabetical order, as follows:

Animated Sign. Any sign or part of a sign that changes physical position in any way, or that uses movement or change of lighting to depict action or create a special effect or scene or the illusion of movement ~~which gives the visual impression of movement or rotation.~~

Changeable Copy Sign. A sign or portion thereof with characters, letters or illustrations that can be changed or rearranged by manual means without altering the face of the sign.

Electronic Message Display (EMD) Sign. A sign or portion thereof using backlighting or internal lighting capable of displaying words, symbols, figures or images that can be electronically or mechanically changed by remote or automatic means. An Electronic Message Display Sign may also be known as an Electronic Message Center.

Window Sign – ~~Any sign installed upon or within three feet of a window for the purpose of viewing from outside the premises. This term does not include merchandise displayed.~~ A sign affixed or directly adjacent to the surface of a window with its message intended to be visible to the exterior environment.

Section 3. Subsection 8.2.5 of the Hailey Municipal Code is amended by the addition of the underlined language, as follows:

8.2.5 Exempt Signs.

A. The following Signs shall not be subject to the permit process as defined by Section 8.2.4 and are not included in the total aggregate sign area as defined in Section 8.2.9.

1. Flags, symbols, or insignias either historic or official of any state or nation, providing the Flag is no larger than sixty square feet and is flown from a pole the top of which is no higher than forty feet (40') from natural grade.

2. Signs posted by a government entity.

3. Two Temporary Signs per building or if a multi-unit building one per

Unit.

4. Signs with areas of four square feet or less.

5. Merchandise displayed in windows that does not involve Copy.

6. Pennants and wind socks, which in no way identify or advertise a person, product, service, or business.

7. Any sign inside a building not visible from the exterior of the building.
8. Art located on private property which in no way identify or advertise a person, product, service, or business.
9. Historic Signs designated by the Hailey City Council as having historical significance to the City or replicas of historic Signs as approved by the Hailey Council.
10. Building identification Signs which identify the name of the building only. These Signs are separate from Signs which identify, advertise, or promote any person, entity, product, or service.
11. Signs on licensed and registered vehicles that are used for normal day-to-day operations of a business, regardless of whether the business is located within Hailey.
12. Window Signs.

Section 4. Subsection 8.2.6 of the Hailey Municipal Code is amended by the addition of the underlined language, as follows:

8.2.6 Prohibited Signs.

A. No person shall erect, maintain, or relocate any of the following Signs within the City:

1. Signs creating traffic hazards. A sign at or near any public street, or at the intersection of any public streets, situated in such a manner as to create a traffic hazard by obstructing vision. Additionally, any sign at any location which would interfere with, obstruct the view of, or be confused with any authorized traffic sign.
2. Any sign which, due to structural weakness, design defect, or other reason, constitutes a threat to the health, safety, and welfare of any person or property.
3. Any sign which contains an intermittent light source, or which includes the illusion of intermittent or flashing light by means of animation, or an externally mounted intermittent light source.
4. Roof Signs, except mansard roof Signs provided that the highest portion of any sign attached to a mansard roof is no more than 2/3 the height of the mansard roof to which it is attached.
5. Animated Signs.
6. Any Pennant, propeller, or similar device which is designed to display movement under the influence of the wind and which contains a message, announcement, declaration, demonstration, display, illustration, or insignia used for promotion or advertisement of a person, product, service, or business.
7. Any Sign attached to or displayed on outdoor furniture.
8. Any Sign mounted on wheels.
9. Any inflatable object used for promotional or sign purposes, excluding standard size balloons.
10. Signs advertising a business that is located outside of the corporate limits of Hailey.
11. Signs using "day-glo," fluorescent, or brilliant luminescent colored or neon lit backgrounds.
12. Reflective colored material that gives the appearance of changing color.
13. Any Sign covering or obscuring windows, doors, storefronts, building entrances, eaves, cornices, columns, horizontal expression lines, or other architectural elements or details.
14. Electronic Message Display Sign (EMD)

Section 5. Subsections 8.2.7, 8.2.9, 8.2.10 and 8.2.11 of the Hailey Municipal Code are repealed and replaced by a new Subsection 8.2.7, as follows:

8.2.7 Design Guidelines and Standards.

8.2.7.1 General Guidelines. The following are suggested ways to increase the effectiveness of Signs placed within the City

a. Projecting Signs are preferred over Portable or sandwich board Signs. Projecting Signs generally are more effective for increasing visibility to both pedestrians and motorist.

b. Sign materials and colors should compliment the building façade. Basic and simple color applications are encouraged.

c. The color of letters and symbols should contrast with the base or background color of the sign to maximize readability.

d. In multi-unit buildings, a Directory Sign with the names and suite numbers of all Units without individual street level entrances are encouraged to be provided at the shared entrance to those Units.



Easy-to-read fonts should be used and hard to read fonts should be avoided.



8.2.7.2 Lighting Standards.

A. All internally and externally lighted Signs shall comply with lighting standards as set forth in Article VIII B of the Hailey Zoning Ordinance.

B. A sign lit by an external light source shall specifically illuminate the Sign.

C. Signs using backlighting or internal lighting shall only illuminate the Copy portion of the Signs. All other areas, including background, shall be constructed, treated and colored in a manner which makes those areas opaque.

D. A maximum of 2 neon Signs per Unit or building shall be allowed, regardless of whether the sign requires a permit.

E. Lightings Standards Chart:

Allowed	Prohibited
<p data-bbox="289 1430 678 1478">Externally illuminated sign (Allowed)</p> 	<p data-bbox="971 1409 1289 1541">Internally illuminated sign where the background is illuminated (Prohibited)</p> 

<p>Internal illumination of symbols and letters but not the sign background (Allowed)</p> 	
<p>Backlighting behind individually mounted letters (Allowed)</p> 	

8.2.7.3 Area Standards.

A. Except as otherwise provided herein, the total Sign area permitted for any building shall not exceed a total of two square feet of Sign area per linear foot of Building Frontage.

1. A building with only one Unit that meets or exceeds 75 feet of linear Building Frontage shall not exceed 150 square feet of total sign area.

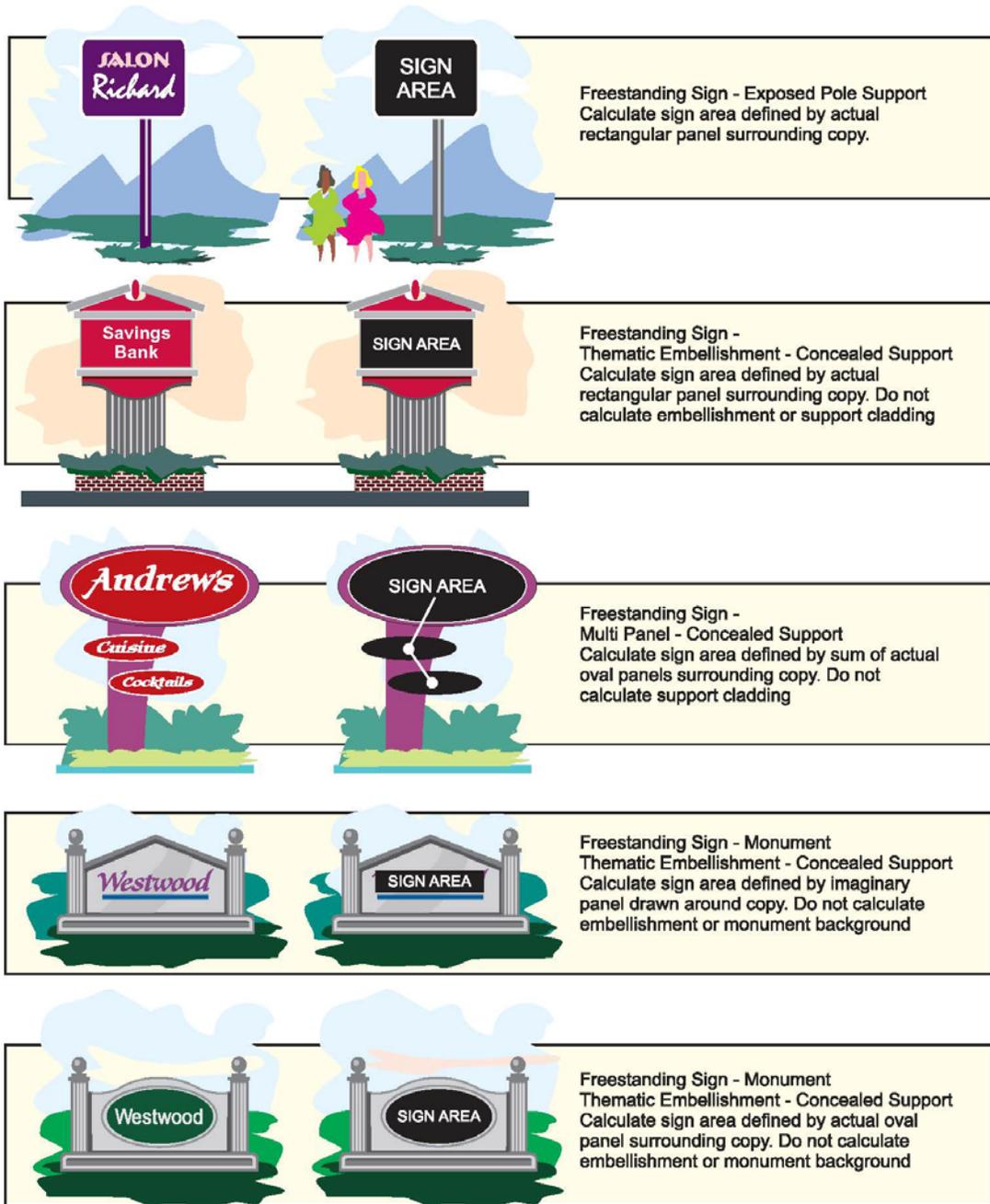
2. The total Sign area permitted for a multi-unit building shall be determined by the Master Sign Plan.

B. Signs on vacant properties are subject to Section 8.2.7.5(C), and are allowed only one Sign per lot.

C. The total aggregate area of all Signs for any building shall not exceed the total Sign area permitted. All Sign faces displayed that are over four (4) square feet shall be included in determining the total aggregate Sign area for a building.

D. The area of a Sign shall be computed using all faces of a Sign within a perimeter which forms the outside shape, excluding any necessary supports upon which the Sign may be placed. Where a Sign consists of more than one face, section, or module, all areas shall be totaled.

1. Sign Area Computational Methodology examples:



- E. Internally lighted Signs shall not exceed a total of 75 square feet for any building.
- F. The size standards in Section 8.2.7.5 shall apply and control the total sign area permitted for each type of sign.

8.2.7.4 Sign Standards for Multi-Unit Buildings.

- A. All Units with an individual street level entrance are allowed up to two Signs, with no more than one Sign on any one building facade.
- B. Each street level interior Units without an individual street level entrance and/or each Units located above the ground floor shall be limited to one Sign.
- C. The location and placement of all exterior Signs for all units within a multi-unit

Building shall be determined and shall be consistent with the design, scale and proportion of the Building and shall be mounted accordingly.

D. The Sign area available for any business within a multi-tenant or Mixed Use Building shall be limited to the amount allocated to the Unit the Business occupies in the approved Master Sign Plan.

E. The Master Sign Plan shall consider the number of units, the Building Façade area and configuration, existing Sign area if applicable and should reasonably provide signage for each Unit.

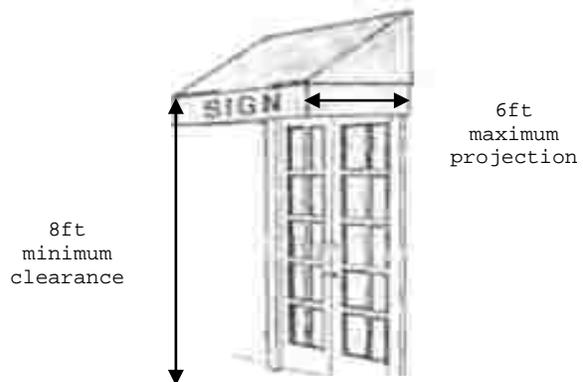
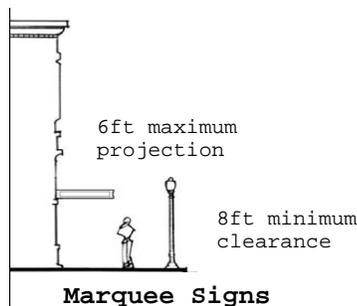
G. The size standards in Section 8.2.7.5 shall apply and control the total Sign area permitted for each type of Sign.

8.2.7.5 Standards for Categories of Signs.

A. Awning and Marquee Signs.

1. The Copy area is limited to the valances of the awnings.

2. Shall not project more than six feet (6') from the building wall and shall provide at least eight feet (8') of vertical clearance when projecting over a pedestrian access way, measured from the ground to the lowest part of the supports for the Awning or Marquee Sign.



B. Freestanding Signs.

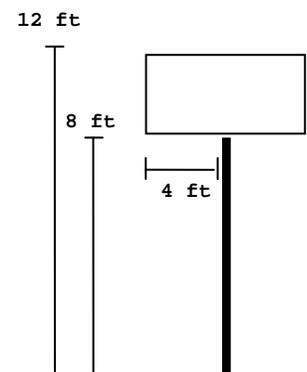
1. The height shall not be greater than twelve feet (12'), measured from natural grade to the top of the Sign.

2. Shall provide eight feet (8') of ground clearance if projecting over the public right-of-way.

3. There shall be only one Freestanding Sign per Building.

4. Freestanding Signs aligned perpendicular to the adjacent public right-of-way are allowed a maximum sign area of 48 square feet, or 24 square feet per side. Those aligned parallel to the adjacent public right-of-way are allowed a maximum sign area of 24 square feet.

5. Shall not extend, at any point, more than four feet (4') into the public right of way.

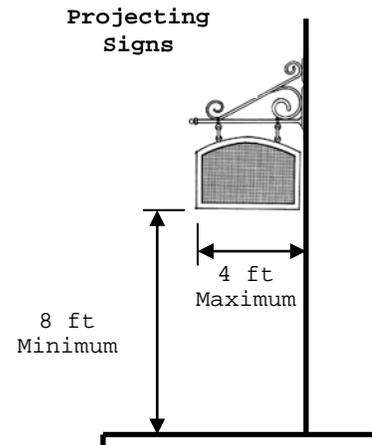


C. Portable Signs.

1. Any Portable Sign is limited to two (2) sign faces or two (2) sides per Portable Sign.
2. Maximum area allowed is six (6) square feet per side, and limited to three feet (3') in height. If there are supporting legs on a Portable Sign frame, they may be up to six inches (6") in height.
3. One Portable Sign is permitted per Unit.
4. Portable Signs shall be weighted or anchored in some manner to prevent them from being moved or blown over by the wind.
5. Portable Signs shall not be located so as to obstruct pedestrian or vehicular traffic, or obstruct sight lines at intersections.
6. No more than two (2) Portable Signs placed in the public right-of-way shall be permitted per corner.
7. Portable Signs must be located in a manner that that maintains thirty-six inches (36") of clear width along all public right-of-ways.

D. Projecting Signs.

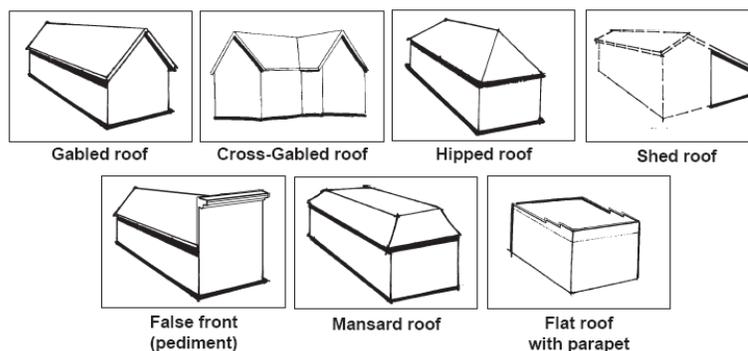
1. Projecting Signs may be placed on a building or underneath an approved canopy, awning or colonnade, but may not extend, at any point, more than four feet (4') from the surface to which it is attached.
2. Signs must have at least eight feet (8') of vertical clearance when projecting over a pedestrian access way, measured from natural grade to the bottom of the Sign.
3. No part of the Sign may extend higher than the lowest portion of a flat roof, the top of a parapet wall, the vertical portion of a mansard roof, the eave line or fascia of a gable, gambrel, or hipped roof.
4. Any portion of a Projecting Sign shall be located below the bottom of any second floor window of a multi-unit building.



E. Wall Signs.

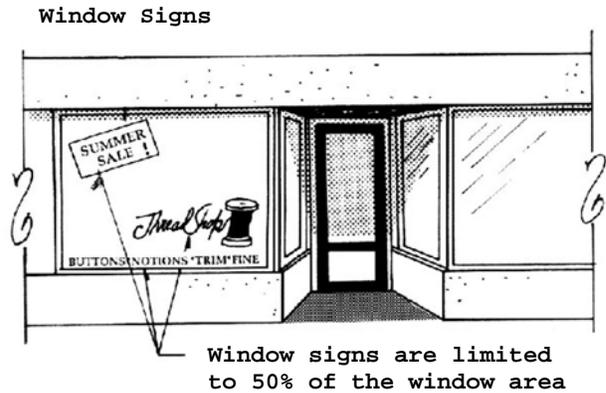
1. Wall Signs may be placed on a structure provided they do not exceed a total of ten percent (10%) of the facade to which they are attached.
2. No part of the Sign may extend higher than the lowest portion of a flat roof, the top of a parapet wall, the vertical portion of a mansard roof, the eave line or fascia of a gable, gambrel, or hipped roof.

Typical Roof Types



F. Window Signs.

1. Permanent Window Signs may be placed in or on any window provided that no more than 50% of the total transparent area of the window is obscured.



G. Changeable Copy Signs.

1. The Copy on any Changeable Copy Sign shall not be changed more than three times per day. Changeable Copy Signs shall be maintained in a legible and serviceable manner.

2.. The size of any Changeable Copy Sign shall be determined by the applicable size standards specified for an Awning and Marquee, Freestanding, Portable, Projecting, Wall, or Window Sign.

H. Temporary Signs.

1. A Temporary Sign shall be displayed for no more than ninety-six (96) hours and for no more than four (4) times per year with a minimum of a five (5) day interval between displays of the Temporary Sign.

2. No more than two (2) Temporary Signs are allowed for any building or for any Unit in a multi-unit building at any given time.

3. The total square footage of one or more Temporary Signs shall not exceed thirty-six feet (36').

4. The square footage of Temporary Signs is exempted from the total allotted sign square footage allowed for any building or for any Unit in a multi-unit building.

5. Temporary Signs shall not exceed the height of the roof lines of adjacent buildings or structures.

6. Temporary Signs shall not extend into a Street or Alley, unless specifically authorized in a sign permit.

Section 6. Subsection 8.2.8 of the Hailey Municipal Code is repealed and replaced by a new Subsection 8.2.8, as follows:

8.2.8 Sign Matrix. Signs are permitted or non-permitted in the zoning districts as follows:

Sign Description or Category	Zoning Districts											
	RG B	LR	GR	N B	LB	TN	B	LI	TI	A	SC I-SO	SC I-I
Awning and Marquee	N	N	N	P	P	P	P	P	P	P	P	P
Changeable Copy	N	N	N	P	P	P	P	P	P	P	P	P
Electronic Message Display	N	N	N	N	N	N	N	N	N	N	N	N

Sign Description or Category	Zoning Districts											
	RG B	LR	GR	N B	LB	TN	B	LI	TI	A	SC I- SO	SC I-I
Freestanding	P	P	P	P	P	P	P	P	P	P	P	P
Internally Lit and Neon Signs	N	N	N	N	N	N	P	P	P	P	P	P
Lit sign (Internal and External)	N	N	N	P	P	P	P	P	P	P	P	P
Portable	N	P	P	P	P	P	P	P	P	P	P	P
Projecting	N	N	N	P	P	P	P	P	P	P	P	P
Temporary Signs	N	N	N	N	P	P	P	P	P	P	P	P
Wall Signs	P	N	P	P	P	P	P	P	P	P	P	P
Window Signs	N	N	N	P	P	P	P	P	P	P	P	P

A “P” indicates that a sign is permitted in the respective zoning district, provided the sign conforms to the applicable requirements of the Land Use Ordinance. An “N” indicates that a sign is not allowed in the respective zoning district.

Section 7. Sections 8.2.9, 8.2.10 and 8.2.11 are hereby reserved.

Section 8. Severability Clause. If any section, paragraph, sentence or provision hereof or the application thereof to any particular circumstances shall ever be held invalid or unenforceable, such holding shall not affect the remainder hereof, which shall continue in full force and effect and applicable to all circumstances to which it may validly apply.

Section 9. Repealer Clause. All Ordinances or Resolutions or parts thereof in conflict herewith are hereby repealed and rescinded.

Section 10. Effective Date. This Ordinance shall be in full force and effect after its passage, approval and publication according to law.

PASSED AND ADOPTED BY THE HAILEY CITY COUNCIL AND APPROVED BY THE MAYOR THIS __ DAY OF _____, 2014.

FRITZ X. HAEMMERLE, Mayor

ATTEST:

MARY CONE, City Clerk

AGENDA ITEM SUMMARY

DATE: 7-7-2014 **DEPARTMENT:** CDD **DEPT. HEAD SIGNATURE:** MA

SUBJECT: Public Hearing on proposed Life Springs Subdivision which is within the Hailey Area of City Impact.

AUTHORITY: (IFAPPLICABLE) IAR _____ City Ordinance/Code Zoning Ordinance No. 532

BACKGROUND/SUMMARY OF ALTERNATIVES CONSIDERED:

Ordinance 649 established the Hailey Area of City Impact in 1994 with criteria for governing subdivisions within the Area of Impact. Since that time, the ordinance has been amended only once, Ordinance 721, and has not been revisited recently. Among other things, Ordinance 649 requires that all Subdivisions within the Area of City Impact conform to the "City of Hailey's Subdivision rules and regulations" and that the City Council holds the authority to "approve, conditionally approve, or deny the application." While this requirement may have conformed to state statutes at the time of adoption in 1994, a 2000 decision by the Idaho Supreme Court concluded that "the power to approve a subdivision application in the impact area resides exclusively with the County" (Blaha v. Board of Ada County Commissioners, 2000). This decision has rendered the validity of our ACI ordinance and agreement questionable.

In order to fulfill the intent of Ordinance 649, staff is recommending the City Council consider the attached recommendations for Life Springs Subdivision and hold a public hearing. The following recommendations are presented:

1. All public infrastructure shall be constructed in accordance with the City of Hailey Subdivision Ordinance, City Standards, and Title 18 of the Hailey Municipal Code. This includes, but is not limited to streets and all street improvements, sidewalks, drainage improvements, driveway approaches, street design, public utility easements, etc.
2. A Public Right of Way easement of 80' wide shall be dedicated for future expansion of River Street. The easement shall align with the current location of River Street and run to the northern boundary of the project.
3. 7' wide sidewalk along with curb and gutter shall be installed the length of the proposed Lot 1 to align with the existing curb, gutter, and sidewalk located on the corner of McKercher Boulevard and Highway 75. All improvements shall be built according to Title 18 and City Standards and all plans must be approved by the City of Hailey prior to construction.

The Planning and Zoning Commission will consider this application on July 14, 2014. Blaine County will be holding a public hearing on August 5, which is the reason the sequence is reversed for this recommendation. Kathy Grotto with Blaine County has requested a recommendation from the City of Hailey for the August 5 public hearing.

FISCAL IMPACT / PROJECT FINANCIAL ANALYSIS:

None

ACKNOWLEDGEMENT BY OTHER AFFECTED CITY DEPARTMENTS: (IFAPPLICABLE)

City Administrator City Attorney City Clerk

Building
 Engineer
 Fire Dept.

Library
 Mayor
 Planning
 Police
 Public Works,
Parks
 P & Z Commission

Benefits Committee
 Streets
 Treasurer
 Sustainability

RECOMMENDATION FROM APPLICABLE DEPARTMENT HEAD:

Discuss the ACI ordinance and the recommendation of approval for Life Spring Subdivision.

ACTION OF THE CITY COUNCIL:

Date : _____

City Clerk _____

FOLLOW-UP:

*Ord./Res./Agrmt./Order Originals: Record *Additional/Exceptional Originals to:

Copies (all info.):

Instrument # _____

Copies (AIS only)

City of Hailey

COMMUNITY DEVELOPMENT DEPARTMENT

115 MAIN STREET SOUTH
HAILEY, IDAHO 83333

Zoning, Subdivision, Building and Business Permitting and Community Planning Services

(208) 788-9815
Fax: (208) 788-2924

Date: June 11, 2014

To: Blaine County Commissioners
ATTN: Kathy Grotto, Senior Planner
Blaine County Land Use Services
208-788-5570
kgrotto@co.blaine.id.us

From: City of Hailey
Micah Austin, Community Development Director
City of Hailey
208-788-4221

Re: Life Springs Subdivision application with Hailey Area of City Impact

Dear Ms. Grotto,

The City of Hailey has reviewed the documents you mailed concerning the proposed Life Springs Subdivision wherein Tax Lot 8232 is subdivided into two lots adjacent to the Hailey City Limits. This proposed subdivision is located within the Area of City Impact, as adopted by Ordinance 649 in 1994 and as such, the City of Hailey is required to make recommendations on this project, as they may relate to the City of Hailey Subdivision Ordinance and land use regulations.

Pursuant to Hailey Ordinance No. 649 which governs subdivisions with the Hailey Area of City Impact, the Hailey City Council held a public hearing on this subdivision application on July 7, 2014 and the Hailey Planning and Zoning Commission held a public hearing on July 14. Based on the testimony received at those public hearings and the information presented, the Hailey City Council hereby makes the following recommendations on Life Springs Subdivision.

These recommendations are as follows:

1. All public infrastructure shall be constructed in accordance with the City of Hailey Subdivision Ordinance, City Standards, and Title 18 of the Hailey Municipal Code. This includes, but not limited to streets and all street improvements, sidewalks, drainage improvements, driveway approaches, street design, public utility easements, etc.
2. A Public Right of Way easement of 80' wide shall be dedicated for future expansion of River Street. The easement shall align with the current location of River Street and run to the northern boundary of the project.
3. 7' wide sidewalk along with curb and gutter shall be installed the length of the proposed Lot 1 to align with the existing curb, gutter, and sidewalk located on the corner of McKercher Boulevard and Highway 75. All improvements shall be built according to Title 18 and City Standards and all plans must be approved by the City of Hailey prior to construction.
4. The length McKercher Boulevard shall be improved and straightened to match the five lane configuration constructed on the east intersection to Highway 75.
5. The intersection with River Street shall match the five-lane design of the intersection at Highway 75.

As the application is seen by the Planning and Zoning Commission and the City Council, there may be other recommendations made at that time. For now, please let me know if you have any additional questions.

Micah

A handwritten signature in blue ink that reads "Micah Austin". The signature is fluid and cursive, with the first name "Micah" and last name "Austin" clearly legible.

Micah Austin
Community Development Director
City of Hailey
[Micah.austin@haileycityhall.org](mailto:micah.austin@haileycityhall.org)
208-788-9815 ext. 13

CC: Ned Williams, City Attorney

AGENDA ITEM SUMMARY

DATE: 7-7-2014 **DEPARTMENT:** CDD **DEPT. HEAD SIGNATURE:** MA

SUBJECT: Update on Beekeeping Ordinance and discussion of Urban Agriculture trends with City of Hailey

AUTHORITY: (IF APPLICABLE) IAR _____ City Ordinance/Code Zoning Ordinance No. 532

BACKGROUND/SUMMARY OF ALTERNATIVES CONSIDERED:

On August 19, 2013, the Hailey City Council held a public hearing on amending the Urban Agriculture use within the City of Hailey to include the production of honey (beekeeping) as an allowed use. Ordinance 1132 was adopted in August of 2013. It has now been a year and, as instructed by the Council, we are revisiting the discussion to determine how well the amendment has worked and if there is a need for further amendments.

Since adoption of the ordinance, an unknown number of individuals have placed hives on their properties and begun keeping bees for honey production. The police department has confirmed that there have been no complaints concerning beekeeping within the City of Hailey. Regarding other Urban Agriculture complaints, illegal roosters continue to be the only complaint received with chickens.

Staff has received questions from residents regarding expanding the definition of Urban Agriculture to include other forms of fowl and limited livestock grazing. Specifically, there is interest in ducks, goats, rabbits, and increasing the number of chickens allowed per home. The City of Hailey has successfully managed the Urban Agriculture uses since the original adoption in 2010 when a maximum of three laying hens were allowed per single family dwelling. In that time period, there have been very few complaints, primarily dealing with roosters. There is now interest in expanding Urban Agriculture use to other zones and increasing the limits on the number of animals allowed per lot. In addition, there is interest in using vacant lots for grazing livestock. Currently, this is not allowed under our Zoning Ordinance.

Options to consider:

1. **No Change.** Make no changes to existing ordinance and continue to enforce on a complaint driven process.
2. **List and Limit.** Amend the ordinance to establish a specific list of allowed "livestock" and setting a maximum limit per animal, per lot. This may include grazing on vacant lots.
3. **Animal Unit Equivalent (AUE).** Consider using Animal Units Equivalent per acre calculations that would apply to any "livestock," as would be defined. This would establish a certain AUE per animal and set limits based on the zoning and acreage. For example, 1.00 AUE may be allowed for a residential zone and a chicken may be .10 AUE, which would allow for ten chickens. A goat may be .50 AUE, meaning that two goats would be allowed, but no chickens. Under this metric, one goat and five chickens would be allowed to max out their AUE limit.
4. **Sub Uses of Urban Ag.** Create sub-uses of the Urban Agriculture use to specifically address unique Urban Ag request per zone. With the District Use Matrix, this can be easily done. For example, one use to be: Urban Ag: Grazing on Vacant Lots, etc.

Other options and variations may be discussed at the City Council Meeting.

FISCAL IMPACT / PROJECT FINANCIAL ANALYSIS:

None

ACKNOWLEDGEMENT BY OTHER AFFECTED CITY DEPARTMENTS: (IF APPLICABLE)

- | | | | | | |
|-------------------------------------|--------------------|-------------------------------------|------------------|-------------------------------------|--------------------|
| <input checked="" type="checkbox"/> | City Administrator | <input type="checkbox"/> | Library | <input type="checkbox"/> | Benefits Committee |
| <input checked="" type="checkbox"/> | City Attorney | <input type="checkbox"/> | Mayor | <input type="checkbox"/> | Streets |
| <input type="checkbox"/> | City Clerk | <input checked="" type="checkbox"/> | Planning | <input type="checkbox"/> | Treasurer |
| <input checked="" type="checkbox"/> | Building | <input type="checkbox"/> | Police | <input checked="" type="checkbox"/> | Sustainability |
| <input type="checkbox"/> | Engineer | <input type="checkbox"/> | Public Works, | <input type="checkbox"/> | _____ |
| <input type="checkbox"/> | Fire Dept. | | Parks | <input type="checkbox"/> | _____ |
| | | <input checked="" type="checkbox"/> | P & Z Commission | | |
-

RECOMMENDATION FROM APPLICABLE DEPARTMENT HEAD:

Discuss Urban Agriculture and if there should be any changes to the ordinance.

ACTION OF THE CITY COUNCIL:

Date : _____
City Clerk _____

FOLLOW-UP:

*Ord./Res./Agrmt./Order Originals: Record *Additional/Exceptional Originals to:

Copies (all info.):
Instrument # _____

Copies (AIS only)